

**REGULAR MEETING OF THE PLANNING BOARD  
WEDNESDAY – JUNE 10, 2015  
7:30 PM  
CITY COMMISSION ROOM  
151 MARTIN STREET, BIRMINGHAM**

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- A. Roll Call
- B. Review and Approval of the Minutes of the regular meeting of **May 13, 2015 and May 27, 2015**
- C. Chairpersons' Comments
- D. Review of the Agenda
- E. Community Impact Statement Review
  - 1. **2100 E. Maple (former Urgent Care)** –Request for approval of a Community Impact Study to review the construction of a new one story, 46,500 sq.ft. retail building for Whole Foods Market (**Postponed from May 27, 2015**).
- F. Preliminary Site Plan Review
  - 1. **2100 E. Maple (former Urgent Care)** – Request for approval of Preliminary Site Plan to allow the construction of a new one story, 46,500 sq.ft. retail building for Whole Foods Market (**Postponed from May 27, 2015, New request to postpone to July 8, 2015**).
- G. Rezoning Requests
  - 1. **555 S. Old Woodward, 555 Building** - Application for rezoning of the property from D-4 in the Downtown Overlay District to D-5, a new zoning classification proposed for the Downtown Overlay District, to allow the renovation and expansion of the existing mixed use building (**Postponed from May 27, 2015, New request to postpone to July 8, 2015**).
- H. Final Site Plan Review
  - 1. **1691/1693 Haynes** – Request for approval of two new garages on site.

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**Notice: Due to Building Security, public entrance during non-business hours is through the Police Department—Pierce St. Entrance only.** Individuals with disabilities requiring assistance to enter the building should request aid via the intercom system at the parking lot entrance gate on Henrietta St.

Persons with disabilities that may require assistance for effective participation in this public meeting should contact the City Clerk's Office at the number (248) 530-1880, or (248) 644-5115 (for the hearing impaired) at least one day before the meeting to request help in mobility, visual, hearing, or other assistance.

*Las personas con incapacidad que requieren algún tipo de ayuda para la participación en esta sesión pública deben ponerse en contacto con la oficina del escribano de la ciudad en el número (248) 530-1800 o al (248) 644-5115 (para las personas con incapacidad auditiva) por lo menos un día antes de la reunión para solicitar ayuda a la movilidad, visual, auditiva, o de otras asistencias. (Title VI of the Civil Rights Act of 1964).*

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## I. Study Session Items

Rules of Procedure for Study Sessions: Site Plan and Design Review, Special Land Use Permit Review and other review decisions will not be made during study sessions; Each person (member of the public) will be allowed to speak at the end of the study session; Each person will be allowed to speak only once; The length of time for each person to speak will be decided by the Chairman at the beginning of the meeting; Board members may seek information from the public at any time during the meeting.

1. **D5 – Proposed Gateway Zone in the Downtown Birmingham Overlay District.**

## J. Pre-Application Discussion

1. 856 N. Old Woodward

## K. Meeting Open to the Public for items not on the Agenda

## L. Miscellaneous Business and Communications:

- a. Communications
- b. **Administrative Approval** Correspondence –
  - (i) Citgo/Shell request for Administrative Approval at **33588 Woodward** (canopy material)
  - (ii) Triple Nickel request for Administrative Approval at **555 S. Old Woodward** (kitchen layout)
- c. Draft Agenda for the next Regular Planning Board Meeting (**June 24, 2015**)
- d. Other Business

## M. Planning Division Action Items

- a. Staff Report on Previous Requests
- b. Additional Items from tonight's meeting

## N. Adjournment

**CITY OF BIRMINGHAM  
PLANNING BOARD ACTION ITEMS  
OF WEDNESDAY, MAY 13, 2015**

Item	Page
<p><b>FINAL SITE PLAN REVIEW</b>  <b>2483 W. Maple Rd.</b>  <b>DFCU</b>  <b>Application for Final Site Plan and Design Review to construct a new one-story bank building with a drive-through facility on the east side of the building</b></p> <p><b>Motion by Mr. DeWeese</b>  <b>Seconded by Mr. Williams to recommend approval of the Final Site Plan and Design Review to the City Commission for 2483 W. Maple Rd. with the following condition:</b>  <b>1. Compliance with the requirements of the City Departments.</b></p> <p><b>Motion carried, 7-0.</b></p>	<p><b>2</b></p> <p><b>3</b></p> <p><b>3</b></p>

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**CITY OF BIRMINGHAM  
REGULAR MEETING OF THE PLANNING BOARD  
WEDNESDAY, MAY 13, 2015  
City Commission Room  
151 Martin Street, Birmingham, Michigan**

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Minutes of the regular meeting of the City of Birmingham Planning Board held on May 13, 2015. Chairman Scott Clein convened the meeting at 7:30 p.m.

**Present:** Chairman Scott Clein; Board Members Carroll DeWeese, Bert Koseck, Gillian Lazar, Janelle Whipple-Boyce, Bryan Williams; Alternate Board Member Daniel Share; Student Representative Andrea Laverty

**Absent:** Board Member Robin Boyle, Alternate Board Member Stuart Jeffares; Student Representative Scott Casperson

**Administration:** Matthew Baka, Senior Planner  
Jana Ecker, Planning Director  
Shalaka Puranik, Asst. City Planner  
Carole Salutes, Recording Secretary

**05-87-15**

**APPROVAL OF THE MINUTES OF THE REGULAR PLANNING BOARD MEETING  
HELD APRIL 8, 2015**

**Motion by Mr. DeWeese**

**Seconded by Ms. Lazar to approve the Minutes of the regular Planning Board meeting on April 22, 2015**

**Motion carried, 6-0.**

**VOICE VOTE**

Yeas: DeWeese, Lazar, Clein, Koseck, Whipple-Boyce, Williams

Nays: None

Abstain: Share

Absent: Boyle

**05-88-15**

**CHAIRPERSON'S COMMENTS (none)**



**05-89-15**

**APPROVAL OF THE AGENDA** (33877 Woodward Ave. is removed per applicant's request)

**05-90-15**

**SPECIAL LAND USE PERMIT ("SLUP")**

**FINAL SITE PLAN REVIEW**

**33877 Woodward Ave., Sav-On Drugs**

**Application for SLUP and Final Site Plan and Design Review to add a drive-through pharmacy on the south side of the building** (postponed from March 25, 2015 – request by applicant to postpone indefinitely)

**05-91-15**

**FINAL SITE PLAN REVIEW**

**2483 W. Maple Rd.**

**DFCU**

**Application for Final Site Plan and Design Review to construct a new one-story bank building with a drive-through facility on the east side of the building**

Mr. Baka advised the site at 2483 W. Maple Rd. is the current location of Cranbrook Auto Care. The petitioner intends to demolish the current building and construct a one-story bank with a drive-through.

The existing site is zoned B-1. The bank use is permitted; however the drive-in teller requires a SLUP. Should Preliminary and Final Site Plan approval be granted by the Planning Board, a public hearing will be held by the City Commission to consider granting the proposed SLUP.

On February 11, 2015 the applicant appeared before the Planning Board and presented revised plans with several changes aimed at addressing earlier concerns of the Planning Board. At that time, the Planning Board voted to grant the applicant Preliminary Site Plan and to recommend Special Land Use Permit approval with several conditions.

On April 14, 2015 the applicant appeared before the Board of Zoning Appeals ("BZA") and was granted a variance to provide less than 70% glazing on the first floor of the building as required by section 4.83 of the Zoning Ordinance. They increased the amount of glazing to mitigate their variance request. The remaining conditions of the Preliminary Site Plan approval have been addressed by the applicant.

*Design Review*

The applicant has submitted design plans and material usage for each façade. The building as proposed will be primarily constructed of red face brick with limestone head caps above the windows and almond color porcelain tile on the soffit. The roof is proposed to be black asphalt shingles. Information has been submitted verifying that the

screenwalls, size of parking spaces, photometric plan, and sign plan all conform to ordinance requirements. A materials board was passed around.

Mr. Vince Pangle, Strategic Property Services, spoke on behalf of DFCU requesting a Final Site Plan recommendation. They received unanimous approval from the BZA on April 14 for the glazing variance. All other conditions that would have required a variance have now been dealt with. He described the 8 in. raised speed table that tells drivers they are entering a pedestrian thoroughfare. They will be putting a full brick finish with a cap on the masonry brick wall that surrounds the entire rear of the site and backs up to residential. The two screenwalls that run north and south along Cranbrook and north and south along the entrance drive will be checked and deterioration will be replaced. All of the brick will match the proposed building. Two curb cuts have been removed.

Mr. Koseck asked why they are using tile on the facade. Ms. Shirley Gannon, Architect, explained it is porcelain exterior grade tile that has been incorporated as part of the branch standards since 2004. It adds contrast from the darker red brick and gives a classic alabaster look to the building. She has never had to make repairs on it.

There were no comments from the public at 7:50 p.m.

Mr. Williams said he is impressed that the 8 in. raised level for the cars will slow them down, so he will vote in favor of the proposal this time. They were not asked to make that change, but they did anyway.

**Motion by Mr. DeWeese**

**Seconded by Mr. Williams to recommend approval of the Final Site Plan and Design Review to the City Commission for 2483 W. Maple Rd. with the following condition:**

- 1. Compliance with the requirements of the City Departments.**

No one in the audience wished to comment on the motion at 7:51 p.m.

**Motion carried, 7-0.**

**VOICE VOTE**

Yeas: DeWeese, Williams, Clein, Koseck, Lazar, Share, Whipple-Boyce

Nays: None

Absent: Boyle

**05-92-15**

**STUDY SESSION**

**MX Ordinance Amendments and Medical Marijuana**

Ms. Puranik noted that since the reformatting of the Zoning Ordinance in 2005, several errors have been identified in the new graphic format of the Zoning Ordinance. These text amendment and land use matrix changes were inadvertently made while switching formats.

In addition, the City Attorney requested that the Planning Board consider amending the Zoning Ordinance to allow and control medical marijuana establishments in the City. Previously, the City prohibited medical marijuana establishments because they were prohibited by Federal law. However, as a result of recent court rulings, the City can no longer prohibit such establishments as they are expressly permitted in the State of Michigan.

Thus, the Planning Board was asked to consider allowing medical marijuana establishments in certain zone districts, and to consider any controls that may be needed. The Planning Board held a public hearing on May 14, 2014 recommending approval to the City Commission to allow medical marijuana facilities in the MX District. The issue was sent back to the Planning Board by the City Commission on February 25, 2015 and after much discussion the board recommended discussion of the subject with the City Attorney and Police Dept.

At the Planning Board's request, the City Attorney was present for the meeting on April 8, 2015 to answer questions/concerns related to the medical marijuana facilities. After further discussion, the Planning Board suggested changing the buffer maps to show 200 ft. and 300 ft. buffers from single-family and multi-family residential so that the allowable area for medical marijuana facilities is clearly identified. The board also requested staff to determine the height requirements for vertical ventilation. The Planning Division conducted research on the methods of regulating such establishments; other jurisdictions listing specific criteria for lighting; ventilation and the overall building.

Since the issue is complex, the Planning Division suggests two options for consideration. The first option includes detailed regulations for lighting, ventilation and the building, in addition to the buffer requirements. The second option considers the mechanical ventilation system requirements along with the buffer.

The board had no concerns with the text amendments and went on to consider the buffer maps and the proposed ordinance language.

Using the 300 ft. buffer from single and multi-family homes map, Mr. DeWeese recommended eliminating the two isolated parcels to the west and permitting the parcels defined at the east end.

Ms. Whipple-Boyce received confirmation there has been no consideration for 1,000 ft. drug free school zones, which is Federal law. That would eliminate all of the parcels. Ms. Ecker replied the City Attorney and deputy police chief have both weighed in on the maps and determined this is the best location.

In response to a question by Mr. Share, Ms. Puranik confirmed she got the option standards from the International Mechanical Code. She saw no mention of what height the exhaust stack should be set. Ms. Ecker thought that is covered, without dictating exact height. It would depend on the size of the operation. Odors should not be detected.

Chairman Clein was uncomfortable with seeing what he considers to be very specific performance specs in a land use ordinance. He did not think this topic is ready for a public hearing. Ms. Ecker said it will be sent to the other City Departments, as well as the City Attorney and Police Dept., asking for their comments/objections.

Ms. Whipple-Boyce thought the question of drug-free school zones has to be addressed. Secondly, she can't foresee ever supporting medical marijuana in this district, so she will very likely always be a "no" vote on that. This district is our most exciting area right now and we are seeing some of the best projects coming in. People are excited to do them and it has just exploded. There is so much going on, so many great buildings, so much interest, tons of people and new residential projects. So, as far as she is concerned this location for medical marijuana facilities is not even close to being ready for a public hearing because it is not the right place.

Mr. Koseck noted this use is legal and it has a purpose. The MX is Birmingham's edgiest district. He wonders how these facilities will affect nearby property values. It is about perception. If the use by law has to have a place in the community, then put it right Downtown within 300 ft. of the Police Dept. which may discourage providers because of the price of land.

Mr. Williams said he lives in the MX District and has never seen a Birmingham Police car on Eton. He agreed the facilities should be placed Downtown. Make them pay quadruple what they would otherwise pay; they won't come.

Chairman Clein suggested that staff come back next time with information on where similar communities have allowed these facilities. Ms. Ecker recalled that the Deputy Police Chief and City Attorney did not agree with placing these facilities in Downtown Birmingham. She will take it to them again.

Board members agreed with keeping Option 1 but listing general guidelines without specific technical requirements.

Chairman Clein noted the Planning Board is charged with providing recommendations to the City Commission as to where they think these establishments should go. It was decided to put this matter on the agenda for the joint City Commission/Planning Board meeting on June 15 and see what the commissioners say.

Mr. Koseck thought that maps drawing a circle 200 and 300 ft. from the Police Dept. would be good to have for the joint meeting.

**05-93-15**

## **STUDY SESSION**

### **Proposal to add D-5: Downtown Gateway Over Five Stories to the Downtown Birmingham Overlay District**

Ms. Ecker advised that the Planning Division has received an application from the owners of the 555 S. Old Woodward building to request an amendment to the Zoning

Ordinance to create a new D-5 zoning classification to the Downtown Birmingham Overlay District.

The building owners are interested in renovating the existing buildings and adding new residential units along S. Old Woodward Ave., as well as adding an addition to the south of the existing residential tower for new retail space and residential units. The building official previously ruled that any changes to the existing legal non-conforming building would increase the non-conformity, and thus be prohibited unless numerous variances were approved. Therefore, the petitioner feels their hands have been tied in terms of making exterior and structural improvements to the building.

Accordingly, the applicant is requesting a Zoning Ordinance amendment to create a new D-5: Downtown Gateway Over Five Stories zoning classification in the Downtown Birmingham Overlay District. Over the past several months, the applicant has reviewed several drafts of the proposed ordinance language with City staff.

Proposed ordinance language to amend Article 3, section 3.01, 3.02 and 3.04 of the Birmingham Zoning Ordinance was presented for the Planning Board to review and consider.

Mr. Rick Rattner, 380 N. Old Woodward Ave., Attorney for the petitioner, was present with a representative of the owner, Mr. Jerry Reinhart; the architect, Mr. Bob Ziegelman; and a landscaper from his office. Mr. Rattner gave a presentation aimed at convincing the Planning Board why the petitioner would like to see the changes made and why it would work in this particular location. Their primary goal is to get the building zoned so that it comes into compliance. They want to do a building that is an icon in the City of Birmingham and a great gateway to the City, along with being completely in line with the 2016 Plan. Included in the presentation was a video depicting Andres Duany's comments when he came to the City in 2014. He stated it is a special building that requires special treatment and it could become incredibly exciting and really cool.

Mr. Koseck said they have not seen a site plan showing the footprint relative to property lines, along with the expansion opportunity. The building needs to be seen in its context. He received confirmation that the tall building is apartments and the other building contains office space. Ms. Ecker said the way this ordinance is written the commercial side could potentially go up an equivalent height to the apartment side.

Mr. DeWeese thought it would be appropriate for the board to think through, if they were going to allow a building of that scale, what they would want there that fits the spirit and essence of the rest of Downtown. He knows that the back side is not inviting at all from the Woodward Ave. side and the front side is not pedestrian oriented the way it is set up. The lower levels could be made more friendly and the parking garage covered up.

Chairman Clein felt the board should look at the proposed ordinance and decide whether creating a D-5 Zone makes sense. Mr. Williams considered this an iconic structure that is long overdue for attention. The Planning Board has almost totally ignored the south end of town, so let's start with this.

Mr. Koseck noted there are buildings being built today that look a lot like this. They have beautiful high tech glass and he knows what Duany is talking about in terms of lighting it so that it glows. Mr. Williams thought the only practical way to proceed with this study is to set up a sub-committee of this board to work with staff.

Chairman Clein suggested the next step would be to come back to a study session to allow the board to review and provide their input. It was discussed that the board should not create the language of the district around a specific project. Everyone agreed that another study session is in order so that the board can look at all of the implications of the request. June 10 would be the earliest.

Mr. Rattner said it is important to him to put together a package for Ms. Ecker as quickly as they can. Chairman Clein asked for a graphic of an existing site plan so the board knows what parcels are included and what are not. Context should be shown so it is clear what is around the site and how that plays into it. Mr. Koseck added it is about the existing footprint, the applicant's ownership limits, and context within 200 ft.

Mr. Williams stated this is an important building and the board will treat it accordingly.

**05-94-15**

**PRE-APPLICATION DISCUSSION** (no applicant came forth)  
**856 N. Old Woodward Ave.**  
**Vacant property**

**05-95-15**

**MEETING OPEN TO THE PUBLIC FOR ITEMS NOT ON THE AGENDA** (no discussion)

**05-96-15**

## **MISCELLANEOUS BUSINESS AND COMMUNICATIONS**

a. Communications (none)

b. Administrative Approvals

Ø 266 Elm St. - Construction of deck and pergola.

Ø 34901 and 34953 Woodward Ave. – Minor design changes.

Ø Mr. Baka advised the Shell Gas Station (formerly known as Citgo) has asked to remove a 6 ft. screenwall between two parking areas on the southern boundary; change the underside of the canopy from Azak to a different material; paint the columns of the canopy rather than cladding them in Azak; change the body of the building from black to green. The board determined these are lower quality changes and the applicant should come back to the board for a full design review.

c. Draft Agenda for the Regular Planning Board Meeting on May 27, 2015

- Ø Transitional Zoning public hearing;
- Ø 2100 E. Maple Rd. rezoning;
- Ø 2100 E. Maple Rd. Site Plan and CIS review;
- Ø 555 S. Old Woodward Ave. application for rezoning (to be postponed to June 10, 2015)

d. Other Business

- Ø It was observed there are cars stored in the parking lot at Adams Square and the construction site is in disarray. Mr. Baka noted the Fred Lavery service facility is not allowed to store cars outside. Ms. Ecker said they will pass that along to Code Enforcement.

**05-97-15**

**PLANNING DIVISION ACTION ITEMS**

- a. Staff report on previous requests (none)
- b. Additional items from tonight's meeting (none)

**05-98-15**

**ADJOURNMENT**

No further business being evident, board members motioned to adjourn at 9:47 p.m.

Jana Ecker  
Planning Director

**CITY OF BIRMINGHAM  
PLANNING BOARD ACTION ITEMS  
OF WEDNESDAY, MAY 27, 2015**

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<p><b>PUBLIC HEARING</b></p> <p>1. An ordinance to amend Chapter 126, Zoning, of the Birmingham City Code as follows:</p> <p><b>TO ADD ARTICLE 02 DISTRICT INTENT, PERMITTED USES, AND SPECIAL USES, SECTION 2.41, TZ1 (TRANSITION ZONE) DISTRICT TO CREATE A DISTRICT INTENT AND LIST PERMITTED AND SPECIAL USES IN THIS ZONE DISTRICT;</b></p> <p><b>TO ADD ARTICLE 02 DEVELOPMENT STANDARDS, SECTION 2.42, TZ1 (TRANSITION ZONE) DISTRICT TO CREATE DEVELOPMENT STANDARDS IN THIS ZONE DISTRICT;</b></p> <p><b>TO ADD ARTICLE 02 DISTRICT INTENT, PERMITTED USES, AND SPECIAL USES, SECTION 2.43, TZ2 (TRANSITION ZONE) DISTRICT TO CREATE A DISTRICT INTENT AND LIST PERMITTED AND SPECIAL USES IN THIS ZONE DISTRICT;</b></p> <p><b>TO ADD ARTICLE 02 DEVELOPMENT STANDARDS, SECTION 2.44, TZ2 (TRANSITION ZONE) DISTRICT TO CREATE DEVELOPMENT STANDARDS IN THIS ZONE DISTRICT;</b></p> <p><b>TO ADD ARTICLE 02 DISTRICT INTENT, PERMITTED USES, AND SPECIAL USES, SECTION 2.45, TZ3 (TRANSITION ZONE) DISTRICT TO CREATE A DISTRICT INTENT AND LIST PERMITTED AND SPECIAL USES IN THIS ZONE DISTRICT;</b></p> <p><b>TO ADD ARTICLE 02 DEVELOPMENT STANDARDS, SECTION 2.46, TZ3 (TRANSITION ZONE) DISTRICT TO CREATE DEVELOPMENT STANDARDS IN THIS ZONE DISTRICT;</b></p> <p><b>TO ADD ARTICLE 4, SECTION 4.53, PARKING STANDARDS, PK-09, TO CREATE PARKING STANDARDS FOR TZ1, TZ2 AND TZ3 ZONE DISTRICTS;</b></p> <p><b>TO ADD ARTICLE 4, SECTION 4.58, SCREENING STANDARDS, SC-06, TO CREATE SCREENING STANDARDS FOR TZ1, TZ2 AND TZ3 ZONE DISTRICTS;</b></p> <p><b>TO ADD ARTICLE 4, SECTION 4.62, SETBACK STANDARDS, SB-05, TO CREATE SETBACK STANDARDS FOR TZ1 ZONE DISTRICTS;</b></p> <p><b>TO ADD ARTICLE 4, SECTION 4.63, SETBACK STANDARDS, SB-06, TO CREATE SETBACK STANDARDS FOR TZ2 AND TZ3 ZONE DISTRICTS;</b></p> <p><b>TO ADD ARTICLE 4, SECTION 4.69, STREETSCAPE STANDARDS, ST-01, TO CREATE STREETSCAPE STANDARDS FOR TZ1, TZ2 AND TZ3 ZONE DISTRICTS;</b></p> <p><b>TO ADD ARTICLE 4, SECTION 4.77, STRUCTURE STANDARDS, SS – 09, TO CREATE STRUCTURE STANDARDS FOR THE TZ1 ZONE DISTRICT;</b></p> <p><b>TO ADD ARTICLE 4, SECTION 4.78, STRUCTURE STANDARDS, SS – 10,</b></p>	1



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<p>TO CREATE STRUCTURE STANDARDS FOR TZ2 AND TZ3 ZONE DISTRICTS; TO ADD <b>ARTICLE 5, SECTION 5.14, TRANSITION ZONE 1</b>, TO CREATE USE SPECIFIC STANDARDS FOR THE TZ1 ZONE DISTRICT; TO ADD <b>ARTICLE 5, SECTION 5.15, TRANSITION ZONES 2 AND 3</b>, TO CREATE USE SPECIFIC STANDARDS FOR THE TZ2 AND TZ3 ZONE DISTRICTS; <b>AND</b> TO AMEND CHAPTER 126, ZONING, OF THE CODE OF THE CITY OF BIRMINGHAM, <b>ARTICLE 4, ALL SECTIONS NOTED BELOW</b>, TO APPLY EACH SECTION TO THE NEWLY CREATED TZ1, TZ2 AND/OR TZ3 ZONE DISTRICTS AS INDICATED: Ordinance Section Name Section Number Applicable Zone to be Added Accessory Structures Standards (AS) <b>4.2</b> <b>4.3</b> <b>4.4</b> TZ1, TZ2, TZ3 TZ1 TZ1, TZ2, TZ3 Essential Services Standards (ES) <b>4.09</b> TZ1, TZ2, TZ3 Fence Standards (FN) <b>4.10</b> <b>4.11</b> TZ1, TZ2, TZ3 TZ1 Floodplain Standards (FP) <b>4.13</b> TZ1, TZ2, TZ3 Height Standards (HT) <b>4.16</b> <b>4.18</b> TZ1, TZ2, TZ3 TZ1, TZ2, TZ3 Landscaping Standards (LA) <b>4.20</b> TZ1, TZ2, TZ3 Lighting Standards (LT) <b>4.21</b> <b>4.22</b> TZ1, TZ2, TZ3 TZ1, TZ2, TZ3 Loading Standards (LD) <b>4.24</b> TZ1, TZ2, TZ3 Open Space Standards <b>4.30</b> TZ1, TZ2, TZ3 (OS) Outdoor Dining Standards (OD) <b>4.44</b> TZ2, TZ3 Parking Standards (PK) <b>4.45</b> <b>4.46</b> <b>4.47</b></p>	<p><b>1</b></p>

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<p>TZ1, TZ2, TZ3 TZ1, TZ2, TZ3 TZ1, TZ2, TZ3 Screening Standards (SC) <b>4.53</b> TZ1, TZ2, TZ3 Setback Standards (SB) <b>4.58</b> TZ1, TZ2, TZ3 Structure Standards (SS) <b>4.69</b> TZ1, TZ2, TZ3 Temporary Use Standards (TU) <b>4.77</b> TZ1, TZ2, TZ3 Utility Standards (UT) <b>4.81</b> TZ2, TZ3 Vision Clearance Standards (VC) <b>4.82</b> TZ1, TZ2, TZ3 Window Standards (WN) <b>4.83</b> TZ2, TZ3 <b>AND</b> TO AMEND <b>ARTICLE 9, DEFINITIONS, SECTION 9.02</b> TO ADD DEFINITIONS FOR BOUTIQUE, PARKING, SOCIAL CLUB, TOBACCONIST, INDOOR RECREATION FACILITY AND SPECIALTY FOOD STORE. 3. To consider a proposal to rezone the following transitional parcels that are adjacent to residential zones throughout the City as follows: <b>300 Ferndale, 233, 247, 267 &amp; 287 Oakland, 416 &amp; 424 Park, Parcel # 1925451021, Birmingham, MI.</b> Rezoning from R-2 Single-Family Residential to TZ1 - Attached Single-Family to allow attached Single-Family and Multi-Family Residential which are compatible with adjacent Single-Family Residential uses. <b>191 N. Chester Rd. Birmingham, MI.</b> Rezoning from R-2 Single-Family Residential to TZ1 - Attached Single-Family to allow Attached Single-Family and Multi-Family Residential uses which are compatible with adjacent Single-Family Residential uses. <b>400 W. Maple Birmingham, MI.</b> - O1 Office to TZ3 Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses. <b>564, 588, Purdy, 115, 123, 195 W. Brown, 122, 178 E. Brown Birmingham, MI.</b> Rezoning from O2 Office to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses. <b>1221 Bowers &amp; 1225 Bowers Birmingham, MI.</b> Rezoning from O1- Office/ P - Parking to TZ1 - Attached Single-Family to allow Attached Single-Family, Multi-Family Residential uses which are compatible with adjacent Single-Family Residential uses. <b>1111 &amp; 1137 Holland; 801, 887, 999, 1035 &amp; 1105 S. Adams Rd.; 1108, 1132 &amp; 1140 Webster; 1137 &amp; 1143 Cole St.; 1101 &amp; 1120 E. Lincoln Birmingham, MI.</b> Rezoning from O2 Office to TZ2 - Mixed-Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses. <b>500, 522 &amp; 576 E. Lincoln; 1148 &amp; 1160 Grant; 1193 Floyd; Parcel</b></p>	<p><b>1</b></p>

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<p><b>#1936403030, Birmingham, MI.</b> Rezoning from B-1 Neighborhood Business to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.</p> <p><b>36801, 36823 &amp; 36877 Woodward, Parcel #'s 1925101001, 1925101006, 1925101007, 1925101008, 1925101009, Birmingham MI.</b> Rezoning from O1- Office &amp; P-Parking to TZ3 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.</p> <p><b>1775, 1803, 1915, 1971, 1999, 2055, 2075 &amp; 2151 Fourteen Mile Rd., Parcel # 2031455006, Birmingham, MI.</b> Rezoning from O1- Office to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.</p> <p><b>100, 124, 130 &amp; 152, W. Fourteen Mile Rd. &amp; 101 E. Fourteen Mile Rd. Parcel #1936379020, Birmingham, MI.</b> Rezoning from B1-Neighborhood Business, P-Parking, R5-Multi-Family Residential to TZ2 - Mixed-Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.</p> <p><b>880 W. Fourteen Mile Rd., 1875, 1890 &amp; 1950 Southfield Rd. Birmingham, MI.</b> Rezoning from B1-Neighborhood Business, O1-Office to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.</p> <p><b>1712, 1728, 1732, 1740, 1744, 1794 &amp; 1821 W. Maple Rd. Birmingham, MI.</b> Rezoning from B1-Neighborhood Business, P-Parking, O1-Office to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.</p> <p><b>2483 W. Maple Rd. Birmingham MI.</b> Rezoning from B1-Neighborhood Business to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.</p> <p><b>151 N. Eton, Birmingham MI.</b> Rezoning from B-1 Neighborhood Business to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.</p> <p><b>412 &amp; 420 E. Frank, Parcel # 1936253003, Birmingham MI.</b> Rezoning from B1-Neighborhood Business, B2B-General Business, R3-Single-Family Residential to TZ1 – Attached Single-Family Residential to allow Attached Single-Family and Multi-Family Residential uses which are compatible with adjacent Single-Family Residential uses.</p>	<p><b>1</b></p>
<p><b>Motion by Mr. DeWeese</b> <b>Seconded by Mr. Williams to continue this public hearing to June 24, 2015 in order to provide more detailed information.</b></p>	<p><b>8</b></p>
<p><b>Motion carried, 7-0.</b></p>	<p><b>8</b></p>

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<p><b>COMMUNITY IMPACT STATEMENT REVIEW ("CIS")</b>  <b>PRELIMINARY SITE PLAN REVIEW</b>  <b>2100 E. Maple Rd.</b>  <b>Request for approval of a CIS and Preliminary Site Plan Review to review the construction of a new one-story, 46,000 sq. ft. retail building for Whole Foods Market</b></p>	8
<p><b>Motion by Ms. Whipple-Boyce</b>  <b>Seconded by Mr. Williams to postpone the action on the CIS as provided by the applicant for the proposed development at 2100 E. Maple Rd., Whole Foods, to June 10, 2015, allowing the applicant the opportunity to address the issues that were raised in the previous motion.</b></p>	10
<p><b>Motion carried, 7-0.</b></p>	11
<p><b>Motion by Mr. DeWeese</b>  <b>Seconded by Mr. Koseck to move up the rezoning request for 2100 E. Maple Rd., Whole Foods, so that it is next on the agenda.</b></p>	11
<p><b>Motion carried, 7-0.</b></p>	11
<p><b>REZONING REQUEST</b>  <b>2100 E. Maple Rd. (former Urgent Care)</b>  <b>Application for rezoning from O-1 Office to B-2 General Business to allow retail and commercial uses on the site;</b></p> <p><b>Or, in the alternative,</b></p> <p><b>Application for rezoning from O-1 Office to B-2B General Business to allow retail and commercial uses on the site.</b></p>	11
<p><b>Motion by Mr. DeWeese</b>  <b>Seconded by Mr. Jeffares to recommend to the City Commission rezoning of 2100 E. Maple Rd. from O1 Office to B2 General Business to allow retail and commercial uses on the site.</b></p>	12
<p><b>Motion carried, 6-1.</b></p>	12
<p><b>Motion by Mr. DeWeese</b>  <b>Seconded by Mr. Koseck to postpone the Preliminary Site Plan for 2100 E. Maple Rd., Whole Foods, to June 10, 2015.</b></p>	14

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<b>Motion carried, 6-0.</b>	<b>15</b>

DRAFT

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**CITY OF BIRMINGHAM  
REGULAR MEETING OF THE PLANNING BOARD  
WEDNESDAY, MAY 27, 2015  
City Commission Room  
151 Martin Street, Birmingham, Michigan**

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Minutes of the regular meeting of the City of Birmingham Planning Board held on May 27, 2015. Chairman Scott Clein convened the meeting at 7:30 p.m.

**Present:** Chairman Scott Clein; Board Members Carroll DeWeese, Bert Koseck, Gillian Lazar, Janelle Whipple-Boyce, Bryan Williams (left at 11:30); Alternate Board Member Stuart Jeffares

**Absent:** Board Member Robin Boyle, Alternate Board Member Daniel Share; Student Representatives Scott Casperson, Andrea Laverty

**Administration:** Matthew Baka, Senior Planner  
Jana Ecker, Planning Director  
Carole Salutes, Recording Secretary

**05-87-15**

**APPROVAL OF THE MINUTES OF THE REGULAR PLANNING BOARD MEETING  
HELD MAY 13, 2015**

Ms. Whipple-Boyce asked that approval of the minutes be postponed so that more of her comments could be included regarding why she does not feel medical marijuana facilities are appropriate in the MX District.

**05-99-15**

**CHAIRPERSON'S COMMENTS** (none)

**05-100-15**

**APPROVAL OF THE AGENDA** (no change to the posted agenda)

**05-101-15**

**PUBLIC HEARING**

1. An ordinance to amend Chapter 126, Zoning, of the Birmingham City Code as follows:

**TO ADD ARTICLE 02 DISTRICT INTENT, PERMITTED USES, AND SPECIAL USES, SECTION 2.41, TZ1 (TRANSITION ZONE) DISTRICT TO CREATE A DISTRICT INTENT AND LIST PERMITTED AND SPECIAL USES IN THIS ZONE DISTRICT;**

TO ADD **ARTICLE 02 DEVELOPMENT STANDARDS, SECTION 2.42, TZ1 (TRANSITION ZONE) DISTRICT** TO CREATE DEVELOPMENT STANDARDS IN THIS ZONE DISTRICT;

TO ADD **ARTICLE 02 DISTRICT INTENT, PERMITTED USES, AND SPECIAL USES, SECTION 2.43, TZ2 (TRANSITION ZONE) DISTRICT** TO CREATE A DISTRICT INTENT AND LIST PERMITTED AND SPECIAL USES IN THIS ZONE DISTRICT;

TO ADD **ARTICLE 02 DEVELOPMENT STANDARDS, SECTION 2.44, TZ2 (TRANSITION ZONE) DISTRICT** TO CREATE DEVELOPMENT STANDARDS IN THIS ZONE DISTRICT;

TO ADD **ARTICLE 02 DISTRICT INTENT, PERMITTED USES, AND SPECIAL USES, SECTION 2.45, TZ3 (TRANSITION ZONE) DISTRICT** TO CREATE A DISTRICT INTENT AND LIST PERMITTED AND SPECIAL USES IN THIS ZONE DISTRICT;

TO ADD **ARTICLE 02 DEVELOPMENT STANDARDS, SECTION 2.46, TZ3 (TRANSITION ZONE) DISTRICT** TO CREATE DEVELOPMENT STANDARDS IN THIS ZONE DISTRICT;

TO ADD **ARTICLE 4, SECTION 4.53, PARKING STANDARDS, PK-09**, TO CREATE PARKING STANDARDS FOR TZ1, TZ2 AND TZ3 ZONE DISTRICTS;

TO ADD **ARTICLE 4, SECTION 4.58, SCREENING STANDARDS, SC-06**, TO CREATE SCREENING STANDARDS FOR TZ1, TZ2 AND TZ3 ZONE DISTRICTS;

TO ADD **ARTICLE 4, SECTION 4.62, SETBACK STANDARDS, SB-05**, TO CREATE SETBACK STANDARDS FOR TZ1 ZONE DISTRICTS;

TO ADD **ARTICLE 4, SECTION 4.63, SETBACK STANDARDS, SB-06**, TO CREATE SETBACK STANDARDS FOR TZ2 AND TZ3 ZONE DISTRICTS;

TO ADD **ARTICLE 4, SECTION 4.69, STREETSCAPE STANDARDS, ST-01**, TO CREATE STREETSCAPE STANDARDS FOR TZ1, TZ2 AND TZ3 ZONE DISTRICTS;

TO ADD **ARTICLE 4, SECTION 4.77, STRUCTURE STANDARDS, SS – 09**, TO CREATE STRUCTURE STANDARDS FOR THE TZ1 ZONE DISTRICT;

TO ADD **ARTICLE 4, SECTION 4.78, STRUCTURE STANDARDS, SS – 10**, TO CREATE STRUCTURE STANDARDS FOR TZ2 AND TZ3 ZONE DISTRICTS;

TO ADD **ARTICLE 5, SECTION 5.14, TRANSITION ZONE 1**, TO CREATE USE SPECIFIC STANDARDS FOR THE TZ1 ZONE DISTRICT;

TO ADD **ARTICLE 5, SECTION 5.15, TRANSITION ZONES 2 AND 3**, TO CREATE USE SPECIFIC STANDARDS FOR THE TZ2 AND TZ3 ZONE DISTRICTS;

**AND**

TO AMEND CHAPTER 126, ZONING, OF THE CODE OF THE CITY OF BIRMINGHAM, **ARTICLE 4, ALL SECTIONS NOTED BELOW**,

TO APPLY EACH SECTION TO THE NEWLY CREATED TZ1, TZ2 AND/OR TZ3 ZONE DISTRICTS AS INDICATED:

Ordinance Section Name Section Number Applicable Zone to be Added Accessory Structures

Standards (AS)

**4.2**

**4.3**

**4.4**

TZ1, TZ2, TZ3

TZ1

TZ1, TZ2, TZ3

Essential Services Standards (ES)

**4.09** TZ1, TZ2, TZ3  
 Fence Standards (FN) **4.10**  
**4.11**  
 TZ1, TZ2, TZ3  
 TZ1  
 Floodplain Standards (FP) **4.13** TZ1, TZ2, TZ3  
 Height Standards (HT) **4.16**  
**4.18**  
 TZ1, TZ2, TZ3  
 TZ1, TZ2, TZ3  
 Landscaping Standards (LA)  
**4.20** TZ1, TZ2, TZ3  
 Lighting Standards (LT) **4.21**  
**4.22**  
 TZ1, TZ2, TZ3  
 TZ1, TZ2, TZ3  
 Loading Standards (LD) **4.24** TZ1, TZ2, TZ3  
 Open Space Standards **4.30** TZ1, TZ2, TZ3 (OS)  
 Outdoor Dining Standards  
 (OD)  
**4.44** TZ2, TZ3  
 Parking Standards (PK) **4.45**  
**4.46**  
**4.47**  
 TZ1, TZ2, TZ3  
 TZ1, TZ2, TZ3  
 TZ1, TZ2, TZ3  
 Screening Standards (SC) **4.53** TZ1, TZ2, TZ3  
 Setback Standards (SB) **4.58** TZ1, TZ2, TZ3  
 Structure Standards (SS) **4.69** TZ1, TZ2, TZ3  
 Temporary Use Standards  
 (TU)  
**4.77** TZ1, TZ2, TZ3  
 Utility Standards (UT) **4.81** TZ2, TZ3  
 Vision Clearance Standards  
 (VC)  
**4.82** TZ1, TZ2, TZ3  
 Window Standards (WN) **4.83** TZ2, TZ3

**AND**

TO AMEND **ARTICLE 9, DEFINITIONS, SECTION 9.02** TO ADD DEFINITIONS FOR BOUTIQUE, PARKING, SOCIAL CLUB, TOBACCONIST, INDOOR RECREATION FACILITY AND SPECIALTY FOOD STORE.

3. To consider a proposal to rezone the following transitional parcels that are adjacent to residential zones throughout the City as follows:

**300 Ferndale, 233, 247, 267 & 287 Oakland, 416 & 424 Park, Parcel # 1925451021, Birmingham, MI.**

Rezoning from R-2 Single-Family Residential to TZ1 - Attached Single-Family to allow attached Single-Family and Multi-Family Residential which are compatible with adjacent Single-Family Residential uses.



**191 N. Chester Rd. Birmingham, MI.**

Rezoning from R-2 Single-Family Residential to TZ1 - Attached Single-Family to allow Attached Single-Family and Multi-Family Residential uses which are compatible with adjacent Single-Family Residential uses.

**400 W. Maple Birmingham, MI.** - O1 Office to TZ3 Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**564, 588, Purdy, 115, 123, 195 W. Brown, 122, 178 E. Brown Birmingham, MI.**

Rezoning from O2 Office to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**1221 Bowers & 1225 Bowers Birmingham, MI.**

Rezoning from O1- Office/ P - Parking to TZ1 - Attached Single-Family to allow Attached Single-Family, Multi-Family Residential uses which are compatible with adjacent Single-Family Residential uses.

**1111 & 1137 Holland; 801, 887, 999, 1035 & 1105 S. Adams Rd.; 1108, 1132 & 1140 Webster; 1137 & 1143 Cole St.; 1101 & 1120 E. Lincoln Birmingham, MI.**

Rezoning from O2 Office to TZ2 - Mixed-Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**500, 522 & 576 E. Lincoln; 1148 & 1160 Grant; 1193 Floyd; Parcel #1936403030, Birmingham, MI.**

Rezoning from B-1 Neighborhood Business to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**36801, 36823 & 36877 Woodward, Parcel #'s 1925101001,**

**1925101006, 1925101007, 1925101008, 1925101009, Birmingham MI.**

Rezoning from O1- Office & P-Parking to TZ3 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**1775, 1803, 1915, 1971, 1999, 2055, 2075 & 2151 Fourteen Mile Rd., Parcel # 2031455006, Birmingham, MI.**

Rezoning from O1- Office to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**100, 124, 130 & 152, W. Fourteen Mile Rd. & 101 E. Fourteen Mile Rd.**

**Parcel #1936379020, Birmingham, MI.**

Rezoning from B1-Neighborhood Business, P-Parking, R5-Multi-Family Residential to TZ2 - Mixed-Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**880 W. Fourteen Mile Rd., 1875, 1890 & 1950 Southfield Rd. Birmingham, MI.**

Rezoning from B1-Neighborhood Business, O1-Office to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**1712, 1728, 1732, 1740, 1744, 1794 & 1821 W. Maple Rd. Birmingham, MI.**

Rezoning from B1-Neighborhood Business, P-Parking, O1-Office to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**2483 W. Maple Rd. Birmingham MI.**

Rezoning from B1-Neighborhood Business to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**151 N. Eton, Birmingham MI.**

Rezoning from B-1 Neighborhood Business to TZ2 - Mixed Use to allow Commercial and Residential uses which are compatible with adjacent Single-Family Residential uses.

**412 & 420 E. Frank, Parcel # 1936253003, Birmingham MI.**

Rezoning from B1-Neighborhood Business, B2B-General Business, R3-Single-Family Residential to TZ1 – Attached Single-Family Residential to allow Attached Single-Family and Multi-Family Residential uses which are compatible with adjacent Single-Family Residential uses.

Mr. Baka recalled the Planning Board has held several study sessions over the past several years in order to develop a Transition Zoning classification that could be applied to areas of the City that abut single-family residential zones and are adjacent to commercial zones and/or located on major thoroughfares. The goal of these study sessions was to identify and revise the zoning classifications of these properties to provide a transition/buffer to the single-family neighborhoods through the use of screenwalls and landscaping.

Additionally, the new zones were crafted to incorporate small scale, neighborhood friendly uses that are likely to be patronized by residents of the immediate area. There are several restrictions proposed to control the new uses that would ensure that new development would be in keeping with the scale and standards that are expected in the City of Birmingham.

The Planning Board selected fourteen (14) locations throughout the City where these zones are proposed to be implemented. On some existing residential parcels this is proposed to be accomplished through attached single-family or multi-family housing. On commercial parcels, it is proposed to be accomplished through a mixed-use zone that permits residential and commercial uses.

On April 8, 2015 the Planning Board reviewed draft ordinance language for three new zoning classifications, TZ1, TZ2, and TZ3. At that time the Planning Board set a public hearing for May 27, 2015. The following outlines the proposal to be considered.

#### *Article 04*

In addition to the regulations provided in Article 02 of the Zoning Ordinance, the Planning Dept. identified many additional development standards contained in Article 04, Development Standards, that should be applied to the new transition zones. The Planning Department is now providing draft ordinance language for those development standards in a format that would allow for integration into Article 04 of the Zoning Ordinance.

#### *Article 05*

The creation of the new zoning classifications would also require additions to Article 05, Use Specific Standards, for any permitted uses allowed in the TZ zones. Draft ordinance language to add to Article 05 has been proposed for review.

#### *Single-family dwellings in Transition Zones*

Throughout the course of the study sessions it has been consistently maintained that single-family residential should be a permitted use in each zone. As discussed at the last study session, the standards that have been applied are R3, which is consistent with the rest of the Zoning Ordinance.

Mr. Baka discussed the permitted uses and development standards for each of the three zones, TZ1, TZ2, and TZ3. TZ1 is strictly residential and TZ2 and TZ3 are mixed-use or commercial zones. The only difference between TZ2 and TZ3 is that the maximum height is higher on TZ3 which allows three stories (minimum of two stories) and 42 ft.; whereas TZ2 permits a maximum of two stories.

Mr. Jeffares received clarification that E.F.I.S. is permitted as a building material for TZ1. For TZ2 and TZ3 it is allowed but not on the first floor.

Ms. Ecker spoke about why the City is taking this initiative. There are multiple parcels throughout the City that are in a difficult situation because they are either on a major road, adjacent to commercial uses, and/or abutting up against single-family neighborhoods. These parcels have not been dealt with by either the Zoning Ordinance or the Master Plan over the last several decades. The Planning Board is attempting to create a Transitional Zone to show the unique circumstances in each of the cases and to clearly delineate which uses are appropriate for those locations. Some protection for the nearby residents has been put into place and the size of any commercial proposal has been limited. Mr. Koseck hoped this would get better tenants, better buffers and respect the neighborhoods.

At 8:08 p.m., Chairman Clein called for comments from the public related to dimensional standards or the creation of transitional zoning in general.

Ms. Patricia Shane who lives on Purdy spoke against the rezoning. She doesn't want commercial coming into her neighborhood.

Ms. Catherine Gains, 343 Ferndale, believed the rezoning will increase on-street parking and traffic which is already getting crazy in her neighborhood. Consider not passing the rezoning.

Mr. Larry Bertolini thought off-street parking for outside dining should be incorporated. He wanted to see a comparison of what was to what can be as far as change in density and change in parking. He hopes the area will not become over commercialized by developers.

Ms. Schuger, who owns property at 467 Park and 1823 Bradford, questioned what the City will be bringing to the residents of the community other than assisting developers. She thinks graphics would be very helpful.

Ms. Jean Rizzo, 431 Park, received confirmation that the rear setback for a TZ1 property is 20 ft. and the side setback is 10 ft. No one in her neighborhood wants the rezoning.

Mr. Steve Rockoff who lives on Webster asked if environmental or traffic impact studies have been done with the parcels as to how the residents could be affected by the rezoning. Chairman Clein answered that without the specifics of a development proposal the details of what the impacts would be could be very far flung. Mr. Rockoff stated everyone he has talked to about the rezoning is against it. Mr. Baka noted that in the TZ2 and TZ3 zones the density will not change.

Ms. Cathleen Schwartz, 582 Henrietta, noted the residents moved in with what is there now. Change is always hard and some of the changes proposed could be very different from what currently exists. She would like to see the parcels in the context of the whole City in order to get a sense of the scope of change.

Mr. Joe Murphy, 751 Ann, said the rezoning appears to him to be a commercial undertaking. He urged the board to consider another way to raise money for the City.

Mr. Jim Partridge, owner of property at the SE corner of Webster and Adams, observed there are four parcels along Adams Rd. that do not meet the criteria and are therefore unbuildable because they are 120 ft. x 40 ft. His is 120 ft. x 42.3 ft. There is no parking. That needs to be looked at. Further there will be disagreements about whether the City is complying with the Uniform Energy Code.

Mr. Will Huffacre, 532 Pierce, agreed that parking could become an issue. He is opposed to the Transition Zones. He hasn't heard why it would really benefit him as a resident. There don't seem to be any provisions to protect residents. He asked if the proposed ordinance amendments would be retroactive. Chairman Clein responded there are code compliance officers who have the ability to issue violations for anything related to the ordinance. Ms. Ecker explained if the ordinance were to go through, an existing building is grandfathered in by legal non-conforming status. However, if a new use comes in or the building is expanded it would be subject to the new rules.

Mr. David Bloom who lives on Stanley stated the residents in this community have made it clear that they do not want to see this kind of development. He doesn't know why it is needed right now when there is so much other expansion going on in the City.

Mr. Paul Regan who lives on Purdy said that staff has done a yeoman's job on determining dimensionality, the height and the setbacks. However, the essence of zoning is usage and what is being considered now is not relief. Therefore, he is not in support. Separate the dimensionality from the uses and you would have a winner.

Mr. Koseck emphasized this proposal is not commercially driven in an effort to achieve more taxes for the City. It is not about putting more on a piece of property than can currently occur, because they all have to provide for their own parking.

Mr. Williams noted the board should focus on density in TZ1. Dimensions are not changing in TZ2 and TZ3 so focus on uses there.

Mr. Baka started a PowerPoint showing existing and proposed zoning for the 14 areas that are under consideration. Initial discussion centered around property at Park and Oakland which is a density issue because single-family is changing to multi-family. It may be the only one of the 14 that truly has density changes proposed. The post office is proposed to go to TZ1 if it is ever sold by the Federal Government.

Mr. Williams wanted to see a graphic depicting for each parcel what exists now and what could exist under current zoning; and what the proposed changes are with respect to uses. Other board members agreed the presentation needs to be a little simpler so that it is easier to understand.

**Motion by Mr. DeWeese**

**Seconded by Mr. Williams to continue this public hearing to June 24, 2015 in order to provide more detailed information.**

The chairman took discussion to the public for comments on the motion at 9:25 p.m.

Mr. Larry Bertolini noted additional items that might be reviewed at the next meeting:

- Ø Clarification as to what happens if the existing church and the existing post office decide to vacate;
- Ø Show graphically that there will be no increase in density;
- Ø Review of parking for outside dining establishments.

Mr. Michael Poris, 36801 Woodward Ave. did not support the motion. He wanted to see the rest of staff's presentation.

Mr. Paul Regan noted that some of the uses come with cars and parking more so than others.

**Motion carried, 7-0.**

ROLLCALL VOTE

Yeas: DeWeese, Williams, Clein, Jeffares, Koseck, Lazar, Whipple-Boyce

Nays: None

Absent: Boyle

The board took a short break at 9:30 p.m.

**05-102-15**

## **COMMUNITY IMPACT STATEMENT REVIEW ("CIS")**

### **PRELIMINARY SITE PLAN REVIEW**

**2100 E. Maple Rd.**

**Request for approval of a CIS and Preliminary Site Plan Review to review the construction of a new one-story, 46,000 sq. ft. retail building for Whole Foods Market**

*CIS*

Ms. Ecker advised the subject site, 2100 East Maple Rd., is currently vacant, but was previously an office building, and then an urgent care medical clinic. At this time, the applicant is proposing a new single-story 46,500 sq. ft. structure. The subject site is located on the south side of E. Maple Rd., west of the existing LA Fitness facility and east of the railroad tracks. The proposed new building will house a Whole Foods grocery store selling natural and organic foods. The site occupies a total of 4.62 acres.

The applicant was required to prepare a Community Impact Study in accordance with Article 7, section 7.27(E) of the Zoning Ordinance as they are proposing a new building containing more than 20,000 sq. ft. of gross floor area.

The applicant has submitted a Phase 1 Update, Environmental Site Assessment ("Phase 1") conducted on the property most recently by AKT Environmental Consultants. The Phase 1 Update has revealed no evidence of recognized environmental conditions except for documented historical industrial use of the property



and the previous presence of one 2,500 gallon fuel oil and one 2,500 gallon gasoline UST.

Several complex changes have been proposed by the applicant for the intersection of E. Maple Rd. and N. Eton which already doesn't function well. The main entrance to the Whole Foods will be the eastern drive. A deceleration lane is proposed to allow the turning movement into the property. The City's traffic study has found traffic will get a little better due to signal timing improvements and the pedestrian crossing system proposed to be put in place.

Mr. Rick Rattner, Attorney for Whole Foods, introduced Mr. James Butler from Professional Engineering Associates, Inc.; Mr. Joseph Marson, Traffic Engineer from Parsons; Mr. Michael Fitzgerald, Architect; Ms. Thea Hiak from Whole Foods; and Mr. Linden Nelson, the owner of the site. Mr. Rattner stated the traffic consultants are working very hard to make that intersection better.

Mr. Butler said they will provide a map for the haul route. They propose to put a trash compactor inside the truck dock, outside of the building. They are aware that a sewer runs beneath the building and that issue will be resolved. Replying to Chairman Clein, Mr. Butler explained their need for parking in excess of one hundred spaces above ordinance requirements is based on Whole Foods standards. Mr. Rattner added it is a very safe and modern parking lot located in the back of the store. Mr. Butler indicated they have prepared a revised geometry plan to address specific issues about how the intersection relates to N. Eton. Their traffic study relates directly to that geometry.

Ms. Thea Hiak said that Whole Foods is more than just a grocery store. She described the many innovative venues that are planned. Mr. Koseck was disappointed not to have a map showing how the store fits within its context.

**Motion by Mr. DeWeese**

**Seconded by Ms. Whipple-Boyce to accept the CIS for 2100 E. Maple Rd., Whole Foods with the following conditions:**

- (1) The applicant will be required to obtain approval to rezone the property to permit the proposed use as a grocery store;**
- (2) Applicant must submit a map showing proposed haul routes during construction;**
- (3) Applicant must provide information on all life safety issues to the Fire Dept. for approval;**
- (4) Applicant must provide information on the details of on-site trash storage and the collection of trash and recycled materials;**
- (5) Applicant must provide information on all utility easements;**
- (6) Applicant must provide information on the proposed security system for approval by the Police Department; and**
- (7) The applicant complies with the recommendations of the City's transportation consultant and all City Departments.**

Mr. Koseck thought it would be more logical to focus on the rezoning before moving into the more detailed information. Chairman Clein said he will not support the motion at this

point because he thinks it is premature to accept a CIS when they still don't have final agreement from the traffic consultant.

The chairman asked for comments on the motion from the audience at 10:45 p.m.

Mr. Larry Bertolini received clarification that the traffic volume count included LA Fitness and the All Seasons development.

Mr. Michael Poris thought the board should support the motion and move the CIS forward. The CIS is, in fact, a study.

Ms. Dorothy Conrad, 2252 Yorkshire, was concerned about pedestrian access.

**Motion failed, 1-6.**

**ROLLCALL VOTE**

Yeas: DeWeese

Nays: Clein, Jeffares, Koseck, Lazar, Whipple-Boyce, Williams

Absent: Boyle

**Motion by Ms. Whipple-Boyce**

**Seconded by Mr. Williams to postpone the action on the CIS as provided by the applicant for the proposed development at 2100 E. Maple Rd., Whole Foods, to June 10, 2015, allowing the applicant the opportunity to address the issues that were raised in the previous motion.**

There was no further discussion on the motion from the public at 10:54 p.m.

**Motion carried, 7-0.**

**VOICE VOTE**

Yeas: Whipple-Boyce, Williams, Clein, DeWeese, Jeffares, Koseck, Lazar

Nays: None

Absent: Boyle

**Motion by Mr. DeWeese**

**Seconded by Mr. Koseck to move up the rezoning request for 2100 E. Maple Rd., Whole Foods, so that it is next on the agenda.**

**Motion carried, 7-0.**

**VOICE VOTE**

Yeas: DeWeese, Koseck, Clein, Jeffares, Lazar, Whipple-Boyce, Williams

Nays: None

Absent: Boyle

**Motion by Mr. DeWeese**

**Seconded by Ms. Lazar to extend the meeting 30 minutes to 11:30 p.m.**

**Motion carried, 6-1. (Mr. Williams opposed)**

**05-103-15**

**REZONING REQUEST**

**2100 E. Maple Rd. (former Urgent Care)**

**Application for rezoning from O-1 Office to B-2 General Business to allow retail and commercial uses on the site;**

**Or, in the alternative,**

**Application for rezoning from O-1 Office to B-2B General Business to allow retail and commercial uses on the site.** (continued from the meeting of April 22, 2015)

Ms. Ecker recalled that at the last meeting the Planning Board wanted to see more detail on how the site could work and to have sealed plans by the architect. She provided the history of the property. The site is the only property on the south side of Maple Rd. east of the railroad tracks that is within the City of Birmingham. In 1989 the entire site was recommended for rezoning to all industrial, based on the Master Plan. However, residents were opposed and the whole property was rezoned to O1 Office which is what it is today.

The applicant believes O1 is not a suitable zoning for this district because on all sides properties are allowed to go higher. Also, the Birmingham MX District and the Troy side allow a greater range of uses. The development trend in the area as a whole is to go more towards mixed use.

B-2 permits a maximum height of 40 ft. and 3 stories; whereas B2B permits a maximum height of 30 ft. and 2 stories. Both of those zoning classifications allow a grocery store use.

Mr. DeWeese thought the applicant's request is reasonable in terms of height, spacing, setbacks, and uses.

Mr. Rick Rattner incorporated everything that was said at the last meeting. Further, everything that Mr. DeWeese has said is correct. This site being rezoned solves a lot of problems from a zoning perspective.

Chairman Clein asked for comments on the rezoning from members of the public.

Mr. Larry Bertolini was comfortable with rezoning to B2 as long as the Planning Board is comfortable they can control traffic adequately through the site plan process.

Ms. Dorothy Conrad said that her homeowners association supports the rezoning for Whole Foods. They do not support the rezoning for a whole number of other items that are on the list for that zoning category.



Ms. Ecker read a letter from a representative of Pembroke Manor Homeowners Association saying they enthusiastically support the property owner's request for rezoning; and further the site plan for Whole Foods Market.

Mr. Williams observed the board started by saying this is an appropriate case for contract zoning, and the neighborhood agrees.

**Motion by Mr. DeWeese**

**Seconded by Mr. Jeffares to recommend to the City Commission rezoning of 2100 E. Maple Rd. from O1 Office to B2 General Business to allow retail and commercial uses on the site.**

Ms. Lazar received clarification that parking requirements would not change for the site.

There were no comments from the public on the motion at 11:30 p.m.

**Motion carried, 6-1.**

**VOICE VOTE**

Yeas: DeWeese, Jeffares, Clein, Koseck, Whipple-Boyce, Williams

Nays: Lazar

Absent: Boyle

*Preliminary Site Plan*

Ms. Ecker explained the existing land uses on the site include a vacant office building, a surface parking lot and a cell tower with associated equipment enclosure. Both the office building and the large parking lot are proposed to be demolished to allow construction of the proposed grocery store, and a new adjacent surface parking lot. The existing cell tower and equipment enclosure at the far south end of the site is proposed to remain.

**The proposed use is permitted in the B2 District which has just been recommended for approval.** The applicant meets the bulk, area, height, and placement standards for the proposed building if the B2 zoning is approved for this site.

*Design Review*

There is an entry in the front NE corner for folks entering from the sidewalk. The building is moved right up to the street and parking is in the back. The applicant is proposing to utilize the following materials for the construction of the proposed grocery store building:

- ☐ Cast stone;
- ☐ Brick;
- ☐ Ceramic tile "wood" siding;
- ☐ Stone for the base of the building;
- ☐ Aluminum and glass storefront with clear glazing;
- ☐ Pre-finished metal coping along the parapet;
- ☐ Fritted glass for upper windows along the west, north and east elevations;

**However, fritted glass is not permitted. The applicant must remove the proposed fritted glass.**

- Metal canopies and column covers along the north and south elevations; and
- Metal frame sunshades surrounding windows along the east and west elevations.

**No material samples or colors have been provided at this time, but will be required at the time of Final Site Plan Review. The plans do not indicate the percentage of glazing provided; however it does not appear that the building as proposed meets the 70% glazing requirement as listed in section 4.83 of the Zoning Ordinance.**

**Motion by Ms. Whipple-Boyce**

**Seconded by Mr. DeWeese to extend the meeting 15 minutes to 11:45 p.m.**

**Motion carried, 6-0. (Mr. Williams absent)**

Mr. Linden Nelson introduced Mr. Michael Fitzgerald from OKW Architects. Mr. Fitzgerald explained that each Whole Foods store is unique. They are not a prototype by any means. Each interior decor and exterior architecture is designed to fit in with the context. He described the preliminary layout of the building and noted the synergy with LA Fitness. They are looking for a clear interior height of 18 or 19 ft. A parapet about 3.5 ft. above the roof structure is proposed to screen rooftop mechanical equipment. The depressed loading area will have a screenwall. There will be outdoor seating at the SW corner of the building. Two elements of the building along E. Maple Rd. are 30 ft. high, and the only piece of the building above that is in the back SE corner, and it is 35 ft. high. Along E. Maple Rd. display windows showing what is going on in the store and in the community sit beneath the clearstory windows in the upper portion of the building.

**Motion by Mr. DeWeese**

**Seconded by Ms. Lazar to extend the meeting 15 minutes to midnight.**

**Motion carried, 6-0. (Mr. Williams absent)**

Mr. Jeffares wanted to see glass and seating on E. Maple Rd. and the offices in back by the parking lot.

Ms. Whipple-Boyce asked for more information on the following:

- Ø How the entries and exits work with the circulation;
- Ø Seating and outdoor dining areas;
- Ø Outdoor seating;
- Ø Seeded glass;
- Ø Materials on the elevations;
- Ø Display windows may not work so well in this application because the pedestrians are below them.

Mr. Koseck suggested some things to look at:

- Ø A sidewalk that links the Maple Rd. side on the west to the store - so don't just treat that as a service entrance;
- Ø The grade change needs to be resolved;
- Ø Offices that have windows;

- Ø Windows should offer a view of what is going on inside along the Maple elevation.

Ms. Lazar thought there is room for change:

- Ø Display windows need some attention;
- Ø Place cash registers near the E. Maple Rd. entrance so it can be an exit as well;
- Ø How trucks get out without interfering with pedestrian or vehicular circulation.

Ms. Hiak advised the store hours will be from 8 a.m. to 10 p.m. seven days a week.

Chairman Clein wanted to see for next time the new configuration of the site as it hits E. Maple Rd.

There were no comments from the public at 11:58 p.m.

Mr. Linden Nelson offered an alternative layout for the store.

**Motion by Mr. DeWeese**

**Seconded by Mr. Koseck to postpone the Preliminary Site Plan for 2100 E. Maple Rd., Whole Foods, to June 10, 2015.**

There were no comments from the public on the motion at 12 a.m.

**Motion carried, 6-0.**

VOICE VOTE

Yeas: DeWeese Koseck, Clein, Jeffares, Lazar Whipple-Boyce

Nays: None

Absent: Boyle, Williams

**Motion by Ms Whipple-Boyce**

**Seconded by Mr. DeWeese to extend the meeting 5 minutes to 12:05 a.m.**

**Motion carried, 6-0. (Mr. Williams absent)**

**05-104-15**

**555 S. Old Woodward Ave., 555 Building**

**Application for rezoning of the property from D-4 in the Downtown Overlay District to D-5, a new zoning classification proposed for the Downtown Overlay District, to allow the renovation and expansion of the existing mixed-use building (Request to postpone to June 10, 2015)**

**05-105-15**

**MEETING OPEN TO THE PUBLIC FOR ITEMS NOT ON THE AGENDA (no discussion)**

**05-106-15**

## MISCELLANEOUS BUSINESS AND COMMUNICATIONS

- a. Communications (none)
- b. Administrative Approvals (none)
- c. Draft Agenda for the Regular Planning Board Meeting on June 10, 2015
  - Ø 2100 E. Maple Rd. CIS, Preliminary Site Plan;
  - Ø D5 ordinance study session;
  - Ø 555 S. Old Woodward Ave. application for rezoning study session;
  - Ø Property on Haynes site plan requesting addition of two garages.

**Motion by Ms Whipple-Boyce**

**Seconded by Mr. DeWeese to extend the meeting 5 minutes to 12:10 a.m.**

**Motion carried, 6-0. (Mr. Williams absent)**

- d. Other Business

**05-107-15**

## PLANNING DIVISION ACTION ITEMS

- a. Staff report on previous requests (none)
- b. Additional items from tonight's meeting (none)

**05-108-15**

## ADJOURNMENT

No further business being evident, board members motioned to adjourn at 12:10 a.m.

Jana Ecker  
Planning Director



## MEMORANDUM

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### Community Development Department

**Date:** June 5, 2015

**To:** Planning Board

**From:** Jana L. Ecker, Planning Director

**Re:** 2100 E. Maple - Whole Foods Market  
Community Impact Study & Preliminary Site Plan Review  
(Changes noted in blue type)

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## Community Impact Study

### I. INTRODUCTION

The subject site, 2100 East Maple is currently vacant, but was previously an office building, and then an urgent care medical clinic. At this time, the applicant is proposing a new a single story 46,500 square foot structure. The subject site is located on the south side of East Maple Road, west of the existing LA Fitness facility and east of the railroad tracks. The proposed new building will house a Whole Foods grocery store, selling natural and organic foods. The site occupies a total of 4.62 acres.

The applicant was required to prepare a Community Impact Study in accordance with Article 7, section 7.27(E) of the Zoning Ordinance as they are proposing a new building containing more than 20,000 square feet of gross floor area.

### II. COMMUNITY IMPACT STUDY

As stated above, the applicant was required to prepare a Community Impact Study ("CIS") given the size of the proposed development. The Zoning Ordinance recognizes that buildings of a certain size may affect community services, the environment, and neighboring properties. The CIS acts as a foundation for discussion between the Planning Board and the applicant, beyond the normal scope of information addressed in the preliminary site plan review application. The Planning Board "accepts" the CIS prior to taking action on a Preliminary Site Plan.

## **A. Planning & Zoning Issues:**

### **Use**

The property is currently zoned O-1 (Office) and the applicant has submitted a rezoning request to rezone the property from O-1 (Office) to B-2 (General Business). **The current O-1 classification does not allow a grocery store and places height restrictions on the new structure.** The B-2 classification would allow the market as a commercially permitted use with a maximum height of 40 feet and 3 stories. **On May 27, 2015 the Planning Board voted to recommend approval of the rezoning of this property from O-1 to B-2. A public hearing on this rezoning request will be held by the City Commission on June 29, 2015.**

### **Master Plan Compliance: Downtown Birmingham 2016 Plan**

The proposed development is not included in the Downtown Birmingham 2016 Plan. The Birmingham Future Land Use Plan (1980) included the property and zoned it as Industrial. The Eton Road Corridor Plan (1990) which includes the property adjacent to the west, envisions the area developing as a mixed use district with a variety of uses compatible with the surroundings. The vision statement can be stated as: "The Eton Road Corridor will be a mixed use corridor with a range of commercial, service, light industrial and residential uses that serve the needs of the residents of Birmingham. Creative site planning will be encouraged to promote high quality, cohesive development that is compatible with the existing uses in the corridor and the adjacent single family residential neighborhood." The Eton Road Corridor Plan ("ERCP") also states that the impacts of traffic on the existing and future residential developments should be minimized.

The applicant has advised that the proposed structure will be built in accordance with LEED principles. Although the subject site is not located within the area studied in the ERCP, it is immediately adjacent to the east. Chapter 5 of the ERCP details specific site and building design guidelines, including the use of high quality materials, the creation of a pedestrian friendly environment with entrances facing the street, street trees and streetscape elements, continuous sidewalks, and effective screening of parking and loading areas. **The Planning Division will review the project against these guidelines during Final Site Plan review. However, at this time it should be noted that the north elevation does not have an activated façade and/or main entry for pedestrians walking along E. Maple. The applicant has now added a full entry at the northeast corner of the proposed store on E. Maple.** The Planning Board may also wish to recommend the addition of benches and/or trash receptacles along the public right-of-way. **The applicant has now added a public plaza space**

with seating along E. Maple at the northwest corner of the site. A revised site plan has been submitted at this time (see attached), and full plans will be provided and reviewed during the Preliminary and Final Site Plan Review.

## **B. Land Development Issues:**

The applicant has provided a topographic survey of existing site conditions, indicating existing sewer, utilities and water lines.

The applicant has submitted a Phase I Update, Environmental Site Assessment ("Phase 1"), dated September 25, 2007, prepared by 21<sup>st</sup> Century Property Group, Inc.. This Phase I was based in part on the use of prior Environmental Site Assessments and subsequent subsurface work conducted on the property most recently by AKT Environmental Consultants, Inc. in March 1999. The Phase I ESA update has revealed no evidence of recognized environmental conditions (REC) at the property except for the following:

- Documented historical industrial use of the subject property and surrounding properties; and
- Previous presence of one 2,500-gallon fuel oil and one 2,500 gallon gasoline UST at the subject property.

The Phase I states that an appropriate level of inquiry has been made into the previous ownership and uses of the property consistent with good commercial and customary practice in accordance with generally accepted and customary professional practices. It is understood that Nelson Ventures, LLC and Fifth Third Bank may rely on the contents and conclusions contained in this report. The report further states that there no pollutants were detected on the property that exceed commercial criteria levels. However, arsenic was detected above residential criteria levels, but the levels of arsenic detected are within what is typically determined to be naturally occurring in metro Detroit. The Phase I concludes that no further work is warranted at this time to determine environmental liability concerns.

The applicant has also submitted a soil report dated April 17, 2015, prepared by G2 Consulting Group, Inc. A total of fifteen soil borings were performed; the first six were within the footprint of the proposed building and the remaining nine were performed within the proposed pavement areas. The soil conditions indicated approximately 3 to 4 inches of bituminous concrete underlain by 4 to 4 1/2 inches of aggregate base for borings B-1 to B-3 and B-6 to B-15. Approximately 6 to 8 inches of topsoil were found at the ground surface of borings B-4 and B-5. Fill, consisting of sand, silty clay, and clayey sand was found below the aggregate base within borings B-3, B-6 and B-10 through B-15 and the topsoil within boring B-4. Buried topsoil was found

below the sand fill within boring B-3 and native sandy silt was below the aggregate base within boring B-7. In general, native silty clay was below the aggregate base, fill soils, buried topsoil, and sand silt and extended to the explored depths ranging from 10 to 20 feet.

The soil report indicated soil strength of 1,000 to 9,000 psf. Fill soils with an organic matter content ranging from 4.2 to 6.9 percent were encountered within the proposed building footprint at boring locations B-4 and B-6. In addition, buried topsoil was encountered within boring B-3. These soils are not suitable for support of footings or floor slabs and were suggested to be completely removed and replaced with engineered fill.

The applicant is proposing to demolish the existing structure, remove existing foundations and utilities from within the footprint of the proposed building and pavement areas and backfill foundations and utility excavations with engineered fill. All unsuitable fill soils and buried topsoil will be removed, along with the existing pavement, vegetation, and topsoil. The entire site will be excavated to the exposed subgrade, and compacted engineered fill will be used to achieve finished grades and prepare the site for floor slab and pavement support. The foundations will bear within the native stiff to hard silty clay and/or engineered fill. The proposed building will be supported on conventional shallow spread and/or continuous footing foundations and be designed based on a net allowable soil bearing capacity of 3,000 psf.

The CIS states that approximately 1000 cubic yards of soil will be removed from the site due to the construction of building footings and parking areas. Approximately 3000 cubic yards of sand will be delivered to the site for the construction of the building pad and loading area. **The haul route for said materials will be determined with consultation and approval of the city. A map detailing the haul routes has not been provided for review. The applicant has now provided a letter indicating that the anticipated haul route during construction would be to enter the site at the west drive, and exit from the existing access drive located just east of the subject site on the LA Fitness property.**

The CIS states that the development will not cause any potential hazards or nuisances. **However, vibration and dust issues may be present during construction. The applicant has not provided any mitigation strategies at this time to address these issues. The applicant has now provided a letter indicating that the general contractor for the project will implement appropriate measures throughout construction to mitigate any vibration and dust issues.**

### **C. Utilities, Noise and Air Issues:**

In accordance with the City's design vision, all utilities on the site should be



buried to visually enhance the site. Thus, the applicant will be required to bury all utilities on the site. The CIS states that the electric service will be provided by DTE Energy. Natural gas service will be provided by CMS Energy. Telephone service will be provided by AT& T Communications. The route of the private utilities and their associated easements are now being determined with the applicable private utility companies.

As noted in the CIS, the applicant states that current ambient noise levels at the site are compatible with the proposed project. A noise study was prepared by Professional Engineering Associates, Inc. dated May 8, 2015. Noise readings were taken at four locations around the perimeter of the site on May 8, 2015. Readings were lowest at the southeast corner of the property at which point they ranged between 45.9 to 65.1 dBA. The northwest corner of the site provided the highest readings between 60.7 and 92.2 dBA, with the highest reading recorded when a train passed by the site on the adjacent railroad tracks.

The noise study provides that the project site will likely comply with the City's noise limits for commercial property immediately adjoining commercial of 90 dBA (daytime) and 75 dBA (nighttime). The CIS report states that use of high efficiency, low noise generating HVAC units is planned. **Specification sheets for all mechanical equipment will be reviewed at Final Site Plan Review for noise output to ensure that the City's noise limits for commercial property will be met. The applicant has now provided a letter indicating that they will provide all mechanical specifications, including noise output, at Final Design Review to ensure all City standards will be met.**

The CIS states that the site is located in the Southeast Michigan Air Quality District, with monitoring stations in Pontiac, Rochester, and Oak Park, as well as other stations within the district. This district has attained and regularly surpassed the National Ambient Air Quality standards for Carbon Monoxide, Nitrogen Dioxide, Ozone, Sulfur Dioxide and particulate matter less than 10 microns, and has attained the standard for annual and 24-hour fine particulates.

The CIS also states that the air quality in the area surpasses the current EPA standards, and is continually improving. In addition, each HVAC unit will be equipped with filtration panels. The report further states that the impact on the air quality due to the proposed use will be significantly less than that of an industrial or manufacturing site. The mechanical equipment for the proposed development will meet or exceed all requirements for air quality and noise control.

#### **D. Environmental Design and Historic Values:**

The applicant has indicated that the existing office building now on the site

has been vacant for many years, and thus the applicant believes that the demolition and removal of this obsolete building from the site will be a benefit to the area. The CIS states that on site shrubs and landscaping will be removed, as well as existing sidewalks which will be replaced to meet current standards. **A complete design review, including streetscape elements, will be conducted as a part of the Final Site Plan review process.** The height of the proposed building is not taller than surrounding developments on the south side of E. Maple Road.

The CIS states that the following elements of the building will be available for LEED credits:

- The near full length glass walls located on all four walls of the proposed building will provide customers and workers a connection to the outdoors through the introduction of daylight into most occupied areas of the building.
- Separate HVAC controls for many of the public areas within the retail store will provide a high degree of thermal comfort to the customers and employees of Whole Foods.
- The building will be non-smoking in keeping with State law, and the HVAC, ventilation, and filtration systems will aid in removing any undesirable airborne particles.
- The project is within 0.25 miles of more than one bus stop, providing another method of transportation to and from the site.
- The development of this project avoids the development of a project more industrial in nature that could be less desirable environmentally.
- The site development plans will reduce pollution from soil erosion by incorporating Best Management Practices for soil erosion and sedimentation control.

The site is not listed on the National Register of Historic Places, nor is it on the City's list of historic sites. Review by the SHPO and HDC is not required. The CIS states that there are no properties or elements within the site plan boundaries that are historic. However, the Big Rock Chophouse building on the other side of the railroad tracks to the west of the subject site is located in a local historic district. No negative impacts on this historic building are expected from the proposed Whole Foods as a large railroad embankment and tracks separates the historic site from the subject site.

#### **E. Refuse, Sewer and Water:**

The proposed facility will utilize a 10 cubic yard trash receptacle and assorted recycling bins. **No details have been provided at this time as to the location and/or screening of the trash receptacles. No details on this have been provided at this time to indicate how recycling will be collected within the building.** **The applicant has advised that a trash compactor will be used and housed in the depressed loading area to the west of the proposed building. The applicant has further advised that**

the recycling will be conducted within the building, and they do not have additional details at this time.

The report mentions that the local waste management services have been approached and have indicated their ability to serve the solid waste and recycling needs of the proposed development. However, no details have been provided at this point. Sanitary sewer service will be provided by connection to the existing 12 inch sewer line flowing to the east. The CIS states that the area's existing sanitary and combined sewer systems have adequate capacity to serve the proposed development.

The existing storm sewer systems have adequate capacity to serve the proposed development.

As per the site plan, water service will be provided by an existing 12" diameter water main located within E. Maple Road. The report states that the water service will be designed in accordance with City Engineering standards and will be compatible with the location and elevation of the existing water main.

#### **F. Public Safety:**

The CIS states that the proposed development has frontage on E. Maple road, which provides adequate access to the property for emergency vehicles and for public safety purposes. The CIS states that the site has two drive approaches to E. Maple road, and provides adequate vertical clearance and adequate turning radii for emergency vehicles. **The proposed building will be equipped with a monitored security system. Specifications and model of the system will be provided to the city as soon as it is available.** The applicant has now provided a letter indicating that these details will be provided at the time of Final Site Plan Review. The proposed development shall conform to all applicable fire codes for site layout, access, hydrant coverage and water connections. Since the structure is only a single story there is no requirement for an elevator. The pavement specifications for the site will be designed to accommodate the weight of fire and emergency vehicles. The fire suppression system for the development has not yet been designed which will be designed in accordance with the City and National Fire codes.

**The Fire Department will require further information to ensure that all life safety issues have been addressed, including details on the fire suppression system, fire access and the Knox Box location. This was not provided in the CIS and will be required at the time of Final Site Plan review.** The applicant has now provided a letter indicating that these details will be provided at the time of Final Site Plan Review.

## G. Transportation Issues:

The applicant has provided a Traffic Impact Study ("TIS") prepared by Parsons dated May 10, 2015. The development, as proposed, will use the existing driveway (which forms the fourth leg to the signalized N. Eton/E. Maple intersection) and the existing driveway about 400 feet to the east of the N. Eton intersection. Currently, westbound Maple left turns to the west drive are not permitted. Under the proposed plan, this movement would be permitted primarily to allow truck access to the loading docks on the west side of the building. Also under the proposed plan, there would not be vehicles permitted to enter from either eastbound Maple Road or southbound Eton; traffic from those directions would enter via the east (main) drive. Furthermore, the outbound west driveway would consist of a left turn lane and a through/right lane. The east site driveway will allow for all movements except outbound left turns. The left turns will be facilitated through use of the signal at the west driveway. The cross access on the south end of the site connecting parking lots with the existing LA Fitness to the east are proposed to be maintained. The peak hour traffic volumes were provided for existing (2015) and future (2016) conditions. The proposed Whole Foods is expected to generate 158 trips during the A.M peak hour and 442 trips during the P.M peak hour. A total of 250 on-site parking spaces are proposed for vehicles.

Based on the analysis conducted, the applicant's traffic study found the following:

1. The **existing intersections are operating at overall acceptable levels during both peak hours except for the two intersections of Maple Road and Eton Street which currently operate at overall LOS E and F during AM and PM peak hour, respectively.**
2. Under background conditions, these same intersections are anticipated to operate at similar levels of service, with some minor variations. Some improvements will be realized due to improved signal timing that could be made.
3. Under future conditions with the site traffic, all intersections are expected to continue to operate at or near the same levels of service. **With improvements proposed for the Maple/Eton intersection, the proposed development is expected to have minimal impact on the surrounding network.**

Based on these findings, the TIS recommends the following improvements:

1. **Modify the southbound approach of North Eton Street to restrict movement to the right and left turns; no southbound through movement would be permitted.** This will require traffic signal, pavement marking and traffic sign changes.
2. **Remove the westbound Maple Road left-turn restriction and**

**modify the roadway striping, signing, and signalization to permit that movement into the site. The west bound left turn would be controlled by a left turn green arrow** that would run simultaneously with the westbound through signal phase. The left turn lane should be modified so that it is 10 feet wide.

3. **Prohibit eastbound Maple Road traffic from making right turns into the west drive.** This would be done by modifying the design of the west driveway to discourage this movement as well as southbound Eton Street through movement.
4. **Install pushbuttons for the northbound and southbound pedestrian movement at both Maple Road and Eton Street intersections.** This will allow more green time to be assigned to east and west bound Maple Road traffic when the pushbutton is not activated.

The CIS report also addresses alternative modes of transportation for patron wishing to access the proposed Whole Foods. The CIS states that access to public transportation is provided within 0.25 miles of the site, which is less than a 5 minute walk. There are SMART bus stops on both eastbound and westbound Maple Road, and there is also access to SMART busses, taxis and the Troy Amtrak train station just south of the subject property. In addition, the CIS states that bike parking facilities are also proposed on site for cyclists, and the site is on an existing regional bicycle route. Finally, the proposed site plan includes on site walkways that connect to the public sidewalk system to allow for safe and efficient pedestrian travel to and from the site and thus providing a full array of transportation options for shoppers.

The City's transportation consultant will review the transportation study and provide comments and concerns to the Planning Board prior to the meeting on May 27, 2015.

**On May 27, 2015, Fleis and Vandenbrink, the City's transportation consultants, provided the following comments:**

- The study was completed consistent with current traffic engineering practice.
- Currently the traffic delays the intersections of Maple & N. Eton and Maple & Coolidge Highway are LOS E and F for AM and PM peak respectively.
- Improved traffic signal timing will mitigate background to LOS D except for PM peak at Maple & N. Eton which will continue to operate at LOS F.
- Eastbound right and southbound through moments will need to be restricted at the west site drive due to geometry limitations and resulting traffic backups affecting intersections to the west.

- Westbound left turns permitted at the west site drive with the addition of a westbound left turn lane.
- With the geometry changes noted above, the addition of push buttons for pedestrian movements and the addition of the proposed development traffic, the intersection of N. Eton & Maple will operate with reduced delay during both peak traffic periods.
- We recommend that the store monument sign be moved near the east site drive.
- Parsons provide the warrant analysis necessary to support the right turn lane proposed at east site drive.
- Parsons prepare a discussion regarding the impact the proposed project will have on multi-modal improvements in the area identified in the City's Multi-Modal Transportation Plan.

The applicant has now provided a response to the City's transportation consultant's including the elimination of the previously proposed monument sign, and the provision of all data necessary to document the warrants for the proposed right turn lane into the proposed eastern access drive. Additionally, the applicant has advised that they propose to implement the following improvements to improve the City's multi-modal infrastructure system:

1. Install an 8' wide sidewalk along E. Maple with connections to the internal sidewalk network on the Whole Foods site;
2. Install a pedestrian plaza/node along E. Maple;
3. Install bicycle racks at the access point to the Whole Foods store to accommodate patrons accessing the site by bicycle;
4. Install an improved cross walk along the west entrance drive to improve pedestrian access;
5. Install pedestrian countdown signals at the E. Maple and N. Eton intersection with the proposed western access drive.

The City's transportation consultant has now determined that all traffic study requirements have been met, and has made the following findings:

- Based on the criteria used by the Road Commission for Oakland County, a right turn lane is warranted at the east site driveway;
- With or without the right turn lane added to the east driveway the level of service at the driveway is acceptable during both peak traffic periods;
- The traffic simulation with and without the right turn lane at the east driveway does not exhibit any significant differences in traffic flow or queues;

- Based on the results of the traffic analysis, the City can consider not requiring the proposed right turn lane at the east driveway; and
- The project is providing improvements to the pedestrian facilities and is installing bike rack systems at the access points to the store.

#### H. Parking Issues:

The applicant indicates that a total of 250 parking spaces are proposed, with all spaces located in the surface parking lot. A total of 155 parking spaces are required (46,500sq.ft. / 300). A thorough discussion of the parking requirements is contained in the attached Preliminary Site Plan report.

#### I. Natural Features:

There are no ponds, streams, wetlands, floodplains or any other significant natural features on or adjacent to the site. There are some existing trees and shrubs on site that will be removed and replaced as a part of the site redevelopment.

The proposed development will not affect surface flows or the water levels of any water bodies. The proposed development will slightly decrease the storm water runoff of the site and decrease the run off rate into the sewer system compared to existing conditions today. Existing site trees and shrubs will be removed, but some new landscaping will be provided.

#### J. Departmental Reports

1. **Engineering Division** – The Engineering Division will provide their comments prior to the Planning Board meeting on May 27, 2015. **The Engineering Division has no comments on the CIS portion of this review, but will have comments once the site plan layout is finalized at Preliminary Site Plan Review.**
2. **Department of Public Services** – The Department of Public Services will provide their comments prior to the Planning Board meeting on May 27, 2015. **The Department of Public Services has stated that it is the applicant's responsibility to ensure the trees are watered to ensure their survival**
3. **Fire Department** – The Fire Department will provide their comments prior to the Planning Board meeting on May 27, 2015.
4. **Police Department** – The Police Department has no concerns with the development as proposed.



5. **Building Division** – The Building Division will provide their comments prior to the Planning Board meeting on May 27, 2015. **The Building Division has no comments on the CIS portion of this review, but will have comments once the site plan layout is finalized at Preliminary Site Plan Review.**

**K. Summary of CIS:**

The following issues remain outstanding with regards to the CIS:

- (1) The applicant will be required to obtain approval to rezone the property to permit the proposed use as a grocery store;
- (2) Applicant must provide information on all life safety issues to the Fire Dept. for approval;
- (3) Applicant must provide information on the details of the collection of trash and recycled materials;
- (4) Applicant must provide information on all utility easements;
- (5) Applicant must provide information on the proposed security system for approval by the Police Department; and
- (6) The applicant complies with the recommendations of the City's transportation consultant and all City Departments.

**L. Suggested Action:**

To **accept** the Community Impact Study as provided by the applicant for the proposed development at 2100 East Maple with the following conditions:

1. The applicant will be required to obtain approval to rezone the property to permit the proposed use as a grocery store;
2. Applicant must provide information on all life safety issues to the Fire Dept. for approval;
3. Applicant must provide information on the details of the collection of trash and recycled materials;
4. Applicant must provide information on all utility easements;
5. Applicant must provide information on the proposed security system for approval by the Police Department; and
6. The applicant complies with the recommendations of the City's transportation consultant and all City Departments.

Or

To **postpone** action on the Community Impact Study as provided by the applicant for the proposed development at 2100 East Maple, allowing the applicant the opportunity to address the issues raised above.



Or

3. To **decline** the Community Impact Study as provided by the applicant for the proposed development at 2100 East Maple for the following reasons:

a. \_\_\_\_\_

b. \_\_\_\_\_

c. \_\_\_\_\_

**III. Preliminary Site Plan Review** (Changes have not been finalized, site plan review requested on July 8, 2015.)

**1.0 Land Use and Zoning**

- 1.1. Existing Land Use – The existing land uses on the site include a vacant office building, a surface parking lot and a cell tower and associated equipment enclosure. Both the office building and the large parking lot are proposed to be demolished to allow construction of the proposed one story grocery store, and a new adjacent surface parking lot. The existing cell tower and associated equipment enclosure at the far south end of the site is proposed to remain.
- 1.2 Zoning – The property is zoned O1 Office, and is outside of, but adjacent to the Rail District. **The proposed retail grocery store use does not conform to the permitted uses of the existing zoning district. However, the applicant is proposing rezoning the property to B2 General Business. The proposed use is permitted in the B2 District.** The applicant has submitted a rezoning application. The Planning Board is considering approval of the rezoning application along with the review of the Community Impact Study and Preliminary Site Plan.
- 1.3 Summary of Adjacent Land Use and Zoning - The following chart summarizes existing land use and zoning adjacent to and/or in the vicinity of the subject site

	North	South	East	West
<b>Existing Land Use</b>	Residential and Commercial	Railroad/ Industrial	Retail/ Commercial	Railroad/ Industrial
<b>Existing Zoning District</b>	R6 Multiple-Family Residential and B1 Neighborhood Business	PP Public Property (Grand Trunk Railroad)	M-1 Light Industrial (City of Troy)	PP Public Property (Grand Trunk Railroad)
<b>Overlay Zoning District</b>	N/A	N/A	N/A	N/A

## 2.0 Setback and Height Requirements

Please see the attached Zoning Compliance Summary Sheet for detailed zoning compliance information. **The proposed height, scale and mass of the building do not meet all of the required development standards for the existing zoning of O1 (Office District).** The proposed building height is 30'; the maximum allowed height is 20' in the O1 zone district. **However, the applicant has submitted a rezoning application and is proposing to rezone the property to B2 (General Business). The proposed building does meet all of the required bulk, area, height and placement standards for the proposed B2 (General Business) zone district.**

## 3.0 Screening and Landscaping

- 3.1 Dumpster Screening – The applicant has provided no dumpster or dumpster screening information at this time. **Details regarding dumpster locations and associated screening must be provided at the time of Final Site Plan and Design Review.**
- 3.2 Parking Lot Screening – The applicant is proposing 250 parking spaces, including 8 barrier-free spaces, on the western, eastern and southern portions of the property. Portions of the parking lot along E. Maple Road are not screened by the proposed building and thus need to be screened. **The applicant will need to provide the required screening and details regarding the dimensions, materials and colors of the proposed screen wall or obtain a variance from the Board of Zoning Appeals.**
- 3.3 Mechanical Equipment Screening – No specifications have been submitted at this time on any proposed rooftop or ground-mounted mechanical equipment or screening. **Details regarding the type and placement of all rooftop mechanical equipment and associated screening must be provided at the time of Final Site Plan review.**
- 3.4 Landscaping – The applicant is proposing 67 large shrubs or trees for the entirety of the property. The trees and shrubs are located around the outer edges of the property and the interior parking lot area. The species and sizes have been provided; they include: Red Sunset Maples, European Hornbeams, Skyline Honeylocusts, Cleveland Select Pears, Greenspire Lindens, Frontier Elms, and Blue Muffin Viburnums.

Article 4, section 4.20 (LA) of the Zoning Ordinance requires that one street tree be provided in the right-of-way for every 40' of

street frontage. Thus, 14 trees are required along the E. Maple frontage (550 / 40). The applicant is proposing to remove 2 existing street trees, and to plant 4 Skyline Honeylocust trees. The applicant has stated that more trees cannot be provided due to conflicts with easements and utility lines. **The applicant will be required to obtain a waiver from the Arborist, or obtain a variance from the Board of Zoning Appeals or provide the required total of 14 street trees.** The applicant is proposing 5 new Columnar European Hornbeam trees along the proposed storefront, but these do not count as street trees as they are set back on private property up against the building. One Greenspire Linden tree is also proposed on private property just east of the main entry drive.

In accordance with Article 4, section 4.45 (PK) of the Zoning Ordinance, 5% of interior parking lots are required to have interior landscaping areas. The proposed parking area is 92,770 sq.ft. in size, thus requiring a total of 4639 sq.ft. of landscaped area, with one canopy tree required for each 150 sq.ft. of required landscaped area. Thus, the applicant is required to provide a total of 31 canopy trees. The applicant is proposing a total of 6366 sq.ft. of landscaped area throughout the parking lot, and a total of 31 canopy trees (29 new, and 2 existing).

#### **4.0 Parking, Loading and Circulation**

- 4.1 Parking – In accordance with Article 4, section 4.45 (PK) of the Zoning Ordinance, a total of 155 parking spaces are required for the area of the building (46,500 sf / 300), if the applicant's rezoning application is granted for the commercial use as a grocery store. The applicant is proposing 250 parking spaces on site, including 8 wheelchair accessible spaces. All parking spaces meet the minimum size requirement of 180 square feet.
- 4.2 Loading – In accordance with Article 4, section 4.22 of the Zoning Ordinance, two loading spaces are required for the proposed development, and two loading spaces are proposed to be screened by the building itself at the northwest corner of the new building.
- 4.3 Vehicular Circulation and Access – The applicant is proposing to move the existing curb approximately 14' closer to the property line to create a new right turn lane entering the property. Vehicles entering the site from the western Maple entrance do so via a 16' wide entrance and may park in the surface parking lot located to the west, south, or east of the proposed building. Vehicles entering the site from the eastern Maple entrance do so via a 19' wide

entrance and may park in the surface parking lot located to the west, south, or east of the proposed building. With regards to internal circulation on the site, 22' and 24' wide two way drives have been provided. Vehicles exiting the site from the western Maple exit can turn left onto westbound Maple via a 12' wide lane or turn right onto eastbound Maple via a 14' wide lane. Vehicles exiting the site from the eastern Maple exit can turn left onto westbound Maple via a 12' wide lane. The proposed drive widths on the interior of the site are adequate for proper maneuvering within the site. **The applicant needs to clearly indicate which curbs and curb cuts are proposed and which are existing. Also, the applicant needs to indicate whether or not the proposed changes will affect circulation in the right-of-way.**

- 4.4 Pedestrian Circulation and Access – The pedestrian circulation and access is not clearly specified on any of the submitted plans, although it does appear that the required public sidewalks are proposed. The sidewalk near the east entry drive is right up against the right turn lane, which is not a comfortable pedestrian environment. In addition, no pedestrian crossing or pathways are indicated throughout the expansive surface parking lot. **The applicant must submit a plan detailing pedestrian circulation and access to and within the site.**

## 5.0 Lighting

The applicant has provided a photometric plan and specification sheets on the proposed lighting. Ten wall sconce fixtures are proposed on the south elevation of the building. Half of the sconces are in the loading/unloading area; the others border the parking area to the rear of the building. The applicant is also proposing 26 pole mounted luminaires in the parking lot. **A detailed lighting review will be conducted at Final Site Plan and Design Review.**

## 6.0 Departmental Reports

1. Engineering Division – The Engineering Division will provide their comments prior to the Planning Board meeting on May 27, 2015.
2. Department of Public Services – The Department of Public Services will provide their comments prior to the Planning Board meeting on May 27, 2015.
3. Fire Department – The Fire Department will provide their comments prior to the Planning Board meeting on May 27, 2015.

4. **Police Department** – The Police Department has no concerns with the development as proposed.
5. **Building Division** – The Building Division will provide their comments prior to the Planning Board meeting on May 27, 2015.

## **7.0 Design Review**

The applicant is proposing to utilize the following materials for the construction of the proposed grocery store building:

- Cast stone;
- Brick;
- Ceramic tile “wood” siding;
- Stone for the base of the building;
- Aluminum and glass storefront with clear glazing;
- Pre-finished metal coping along the parapet;
- Fritted glass for upper windows along the west, north and east elevations;  
**However, fritted glass is not permitted. The applicant must remove the proposed fritted glass.**
- Metal canopies and column covers along the north and south elevations;  
and
- Metal frame sunshades surrounding windows along the east and west elevations.

**No material samples or colors have been provided at this time, but will be required at the time of Final Site Plan review. The plans do not indicate the percentage of glazing provided however it does not appear that the building as proposed meets the 70% glazing requirement as listed in section 4.83 of the Zoning Ordinance. The Planning Division will reserve detailed analysis and comments regarding architectural standards and design related issues for the Final Site Plan and Design Review.**

## **8.0 Approval Criteria**

In accordance with Article 7, section 7.27 of the Zoning Ordinance, the proposed plans for development must meet the following conditions:

- (1) The location, size and height of the building, walls and fences shall be such that there is adequate landscaped open space so as to provide light, air and access to the persons occupying the structure.
- (2) The location, size and height of the building, walls and fences shall be such that there will be no interference with adequate light, air and access to adjacent lands and buildings.

- (3) The location, size and height of the building, walls and fences shall be such that they will not hinder the reasonable development of adjoining property not diminish the value thereof.
- (4) The site plan, and its relation to streets, driveways and sidewalks, shall be such as to not interfere with or be hazardous to vehicular and pedestrian traffic.
- (5) The proposed development will be compatible with other uses and buildings in the neighborhood and will not be contrary to the spirit and purpose of this chapter.
- (6) The location, shape and size of required landscaped open space is such as to provide adequate open space for the benefit of the inhabitants of the building and the surrounding neighborhood.

## **9.0 Recommendation**

Based on a review of the site plan revisions submitted, the Planning Division recommends that the Planning Board **POSTPONE** the Preliminary Site Plan for 2100 E. Maple Rd. pending receipt of the following information:

- (1) A detailed site plan which clearly indicates all dumpster locations and associated screening details, parking lot screening details (along E. Maple), meets all landscaping requirements, and clearly indicates existing and proposed curb cut locations and pedestrian circulation and access details to and from the site and within the site; and
- (2) The provision of detailed floor plans.

## **10.0 Sample Motion Language**

Motion to **POSTPONE** the Preliminary Site Plan for 2100 E. Maple Rd. pending receipt of the following information:

- (1) A detailed site plan which clearly indicates all dumpster locations and associated screening details, parking lot screening details (along E. Maple), meets all landscaping requirements, and clearly indicates existing and proposed curb cut locations and pedestrian circulation and access details to and from the site and within the site; and
- (2) The provision of detailed floor plans.

Or

Motion to APPROVE the Preliminary Site Plan for 2100 E. Maple Rd. subject to the following conditions:

- (1) Approval of the rezoning of the property to B2 (General Business);
- (2) A detailed site plan which clearly indicates all dumpster locations and associated screening details, parking lot screening details (along E. Maple), meets all landscaping requirements, and clearly indicates existing and proposed curb cut locations and pedestrian circulation and access details to and from the site and within the site;
- (3) The provision of detailed floor plans;
- (4) Applicant must submit specifications on all mechanical equipment and associated screening for Final Site Plan and Design Review;
- (5) Applicant must comply with the requirements of all City Departments;
- (6) Remove frittered glass from all elevations or obtain a variance from the Board of Zoning Appeals;
- (7) Add an active pedestrian entry along E. Maple Road; and
- (8) The applicant must provide material and color samples at Final Site Plan and Design Review.

OR

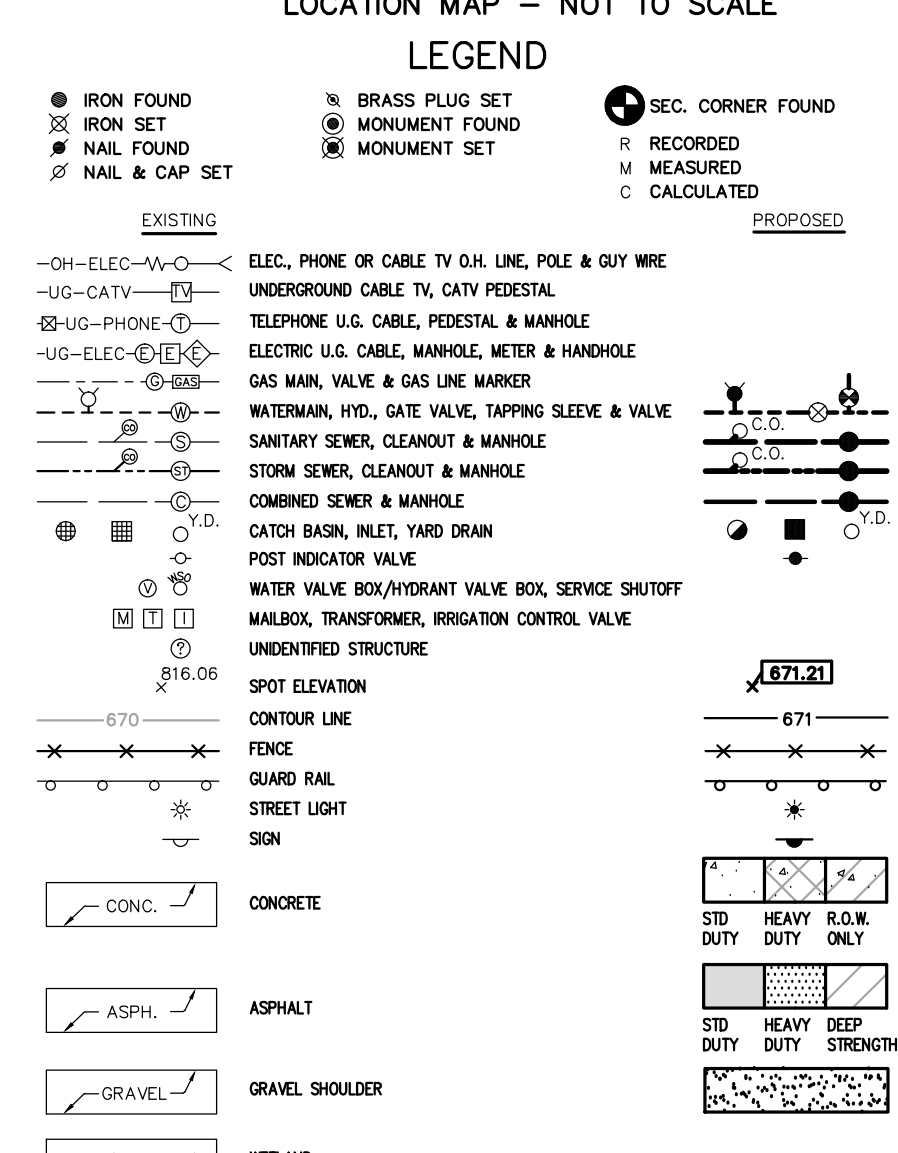
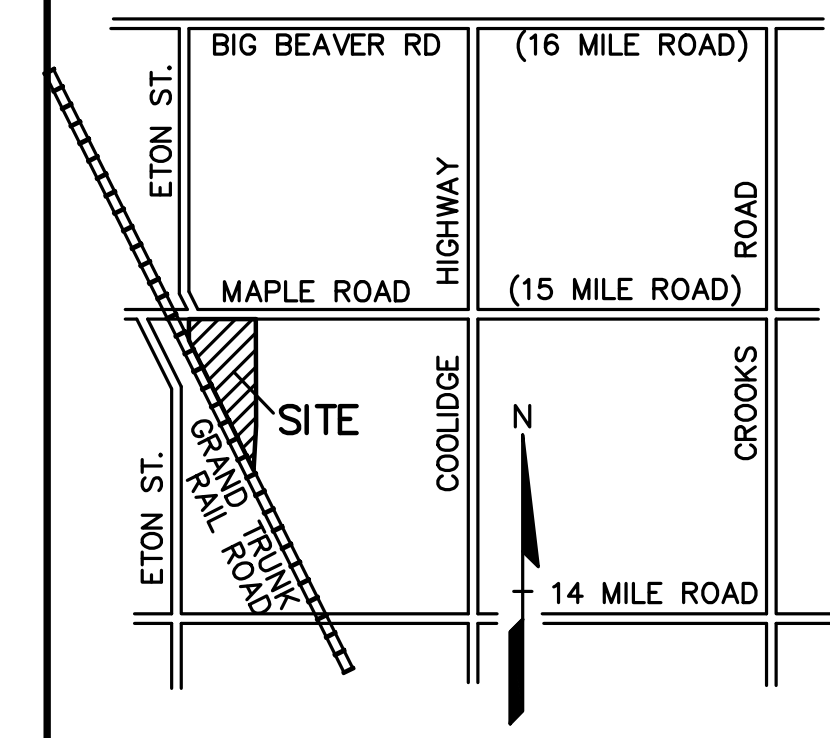
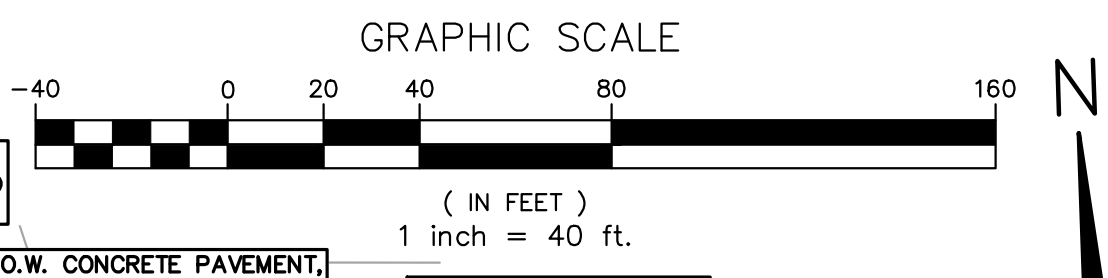
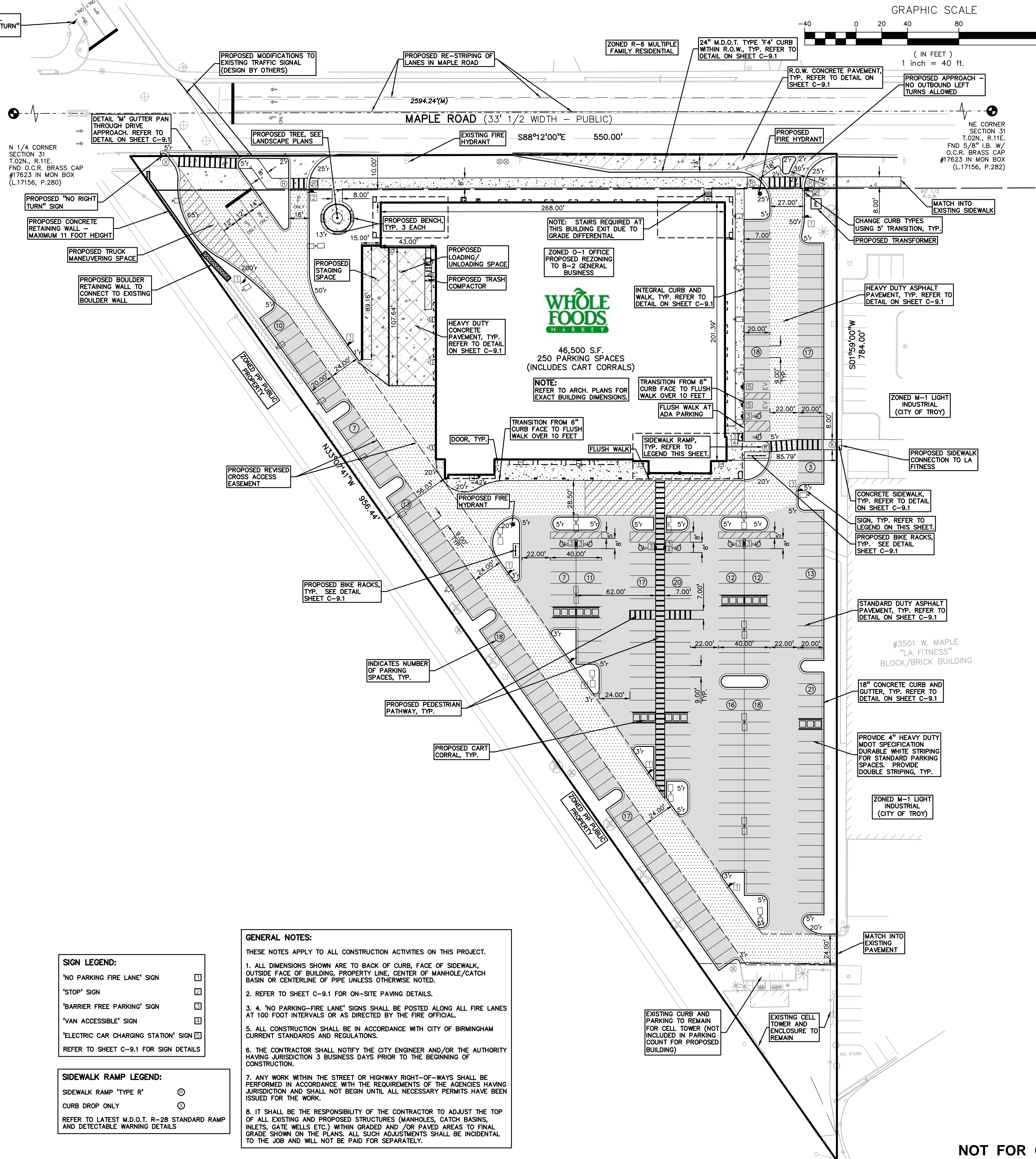
Motion to DENY the Preliminary Site Plan for 2100 E. Maple Rd.



**FLOODPLAIN:**  
By graphical plotting, the subject parcels are in "Areas determined to be outside of the 0.2% annual chance floodplain." (Zone X) per Flood Insurance Rate Map Number 26125C-0537F, Effective September 29, 2006.

**BENCHMARKS:**  
(GPS DERIVED - NAVD88)  
**BM #301:**  
Arrow on hydrant located at the southwest corner of Maple Road and Maple Lane Drive. Elev. = 748.29  
**BM #302:**  
Set benchtie in the east face of utility pole, approx. 200' north of the northwest building corner of "LA Fitness". Elev. = 748.20  
**BM #303:**  
Arrow on hydrant, approx. 70' south of the southwest building corner of "LA Fitness" and approx. 35' northeast of the cell site area. Elev. = 745.99

PROPOSED "ALL TRAFFIC MUST TURN" SIGN



**REFERENCE DRAWINGS**  
"1501 MAPLE LANE - ALTA/ACSM LAND TITLE SURVEY" PEA JOB# 2007-168, DATED 9-11-07  
"MAPLE ROAD DEVELOPMENT - ALTA/ACSM LAND TITLE SURVEY" PEA JOB# 2006-146, DATED 2-8-08  
"ALTA/ACSM LAND TITLE SURVEY - LA FITNESS" PEA JOB# 2010-207, DATED 7-15-2013  
CONSUMERS ENERGY QUARTER SECTION MAP #02-61-31-1, DATED 8-8-14  
MAPLE ROAD WIDENING HRC JOB #0010396, DATED 3-29-02  
CITY OF TROY SEWER MAP #13-01, DATED 5-4-84  
**LEGAL DESCRIPTION:**  
(PER OAKLAND COUNTY ONLINE PROPERTY GATEWAY)  
**PARCEL NO. 20-31-202-001**  
Town 2 North, Range 11 East, Section 31, "Birmingham Gardens", as recorded in Liber 31, Page 38, Oakland County Records. All that part of Lots 16 to 55 inclusive, Lots 96 to 108 inclusive & Lots 162 & 163 lying south of E Maple Road & lying East of the Easterly right-of-way line of Grand Trunk Railroad, Also vacated streets & alley adjacent to the same.

SITE DATA TABLE:									
1)	TOTAL SITE AREA	= 4.95 ACRES (GROSS)							
		= 4.62 ACRES (NET)							
2)	ZONED:	O-1 (OFFICE)							
	PROPOSED RE-ZONING:	B-2 (GENERAL BUSINESS DISTRICT)							
3)	PROPOSED USE:	GROCERY STORE							
4)	BUILDING SETBACKS:	REQUIRED:							
	FRONT:	10.00'							
	REAR:	56.03'							
	SIDE:	85.79'							
5)	MAX LOT COVERAGE:	N/A							
6)	MAX BUILDING HEIGHT:	40', 3 STORIES							
7)	PARKING:	REQUIRED:							
	1 SPACE PER 300 SF FLOOR AREA = 1 * 46,500 SF = 155 SPACES								
	250 TOTAL SPACES PROVIDED (INCLUDING 8 HC SPACES)								
8)	BICYCLE PARKING:	REQUIRED:							
	1 SPACE PER 3,000 S.F. BUILDING AREA = 46,500/3,000 = 16 SPACES								
	16 BICYCLE PARKING SPACES								

**SIGN LEGEND:**  
'NO PARKING FIRE LANE' SIGN  
'STOP' SIGN  
'BARRIER FREE PARKING' SIGN  
'VAN ACCESSIBLE' SIGN  
'ELECTRIC CAR CHARGING STATION' SIGN  
REFER TO SHEET C-9.1 FOR SIGN DETAILS

**SIDEWALK RAMP LEGEND:**  
SIDEWALK RAMP 'TYPE R'  
CURB DROP ONLY  
REFER TO LATEST M.D.O.T. R-28 STANDARD RAMP AND DETECTABLE WARNING DETAILS

**GENERAL NOTES:**  
THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.  
1. ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, FACE OF SIDEWALK, OUTSIDE FACE OF BUILDING, PROPERTY LINE, CENTER OF MANHOLE/CATCH BASIN OR CENTERLINE OF PIPE UNLESS OTHERWISE NOTED.  
2. REFER TO SHEET C-9.1 FOR ON-SITE PAVING DETAILS.  
3. 4. 'NO PARKING-FIRE LANE' SIGNS SHALL BE POSTED ALONG ALL FIRE LANES AT 100 FOOT INTERVALS OR AS DIRECTED BY THE FIRE OFFICIAL.  
5. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH CITY OF BIRMINGHAM CURRENT STANDARDS AND REGULATIONS.  
6. THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEER AND/OR THE AUTHORITY HAVING JURISDICTION 3 BUSINESS DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.  
7. ANY WORK WITHIN THE STREET OR HIGHWAY RIGHT-OF-WAYS SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE AGENCIES HAVING JURISDICTION AND SHALL NOT BEGIN UNTIL ALL NECESSARY PERMITS HAVE BEEN ISSUED FOR THE WORK.  
8. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ADJUST THE TOP OF ALL EXISTING AND PROPOSED STRUCTURES (MANHOLES, CATCH BASINS, INLETS, GATE WELLS ETC.) WITHIN GRADED AND/OR PAVED AREAS TO FINAL GRADE SHOWN ON THE PLANS. ALL SUCH ADJUSTMENTS SHALL BE INCIDENTAL TO THE JOB AND WILL NOT BE PAID FOR SEPARATELY.

REVISED	DATE	BY	REVISION
1	11/11/2015	JPB	ISSUED FOR PERMIT REVIEW

**CAUTION!!**  
THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY AN APPROXIMATION. THE CONTRACTOR SHALL BE EXCLUSIVELY RESPONSIBLE FOR DETERMINING THE EXACT UTILITY LOCATION AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.  
THIS DRAWING AND DESIGN ARE THE PROPERTY OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. THEY ARE SUBMITTED ON THE CONDITION THAT THEY ARE NOT TO BE USED, REPRODUCED, OR COPIED, IN WHOLE OR IN PART, OR USED FOR ANY PURPOSE OTHER THAN THAT SPECIFICALLY INTENDED. PROFESSIONAL ENGINEERING ASSOCIATES, INC. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHERWISE ARE HEREBY SPECIFICALLY RESERVED. © 2014 PROFESSIONAL ENGINEERING ASSOCIATES, INC.

CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTRACTUALLY AND NOT BE LIMITED TO NORMAL WORKING HOURS AND CONSTRUCTION INDEMNITY AND HOLD DESIGN PROFESSIONAL WARRANTIES FROM ANY AND ALL LIABILITY REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THE PROJECT EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE DESIGN PROFESSIONAL.

**3 FULL WORKING DAYS BEFORE YOU DIG CALL 811**  
Know what's below  
Call before you dig  
Miss Dig System, Inc.  
1-800-482-7171 www.missdig.net  
(TOLL FREE)

**PROFESSIONAL ENGINEERING ASSOCIATES**  
2430 Rochester Ct. Suite 100  
Troy, MI 48063-1872  
Phone: (248) 689-9090  
Fax: (248) 689-1044  
website: www.peainc.com

**2100 EAST MAPLE ROAD, LLC**  
2100 EAST MAPLE ROAD, SUITE 200  
BIRMINGHAM, MI 48069  
**PRELIMINARY SITE PLAN**  
**WHOLE FOODS MARKET**  
PART OF THE NE 1/4 OF SECTION 31, T. 2N, R. 11E,  
CITY OF BIRMINGHAM, OAKLAND COUNTY, MICHIGAN  
S:\PROJECTS\2015\2015053\DWG\15053 SITE PLANS\VBASE-15053.DWG  
S:\PROJECTS\2015\2015053\DWG\15053 SITE PLANS\VTBLK-15053.DWG

ORIGINAL ISSUE DATE:  
MAY 11, 2015  
PEA JOB NO. 2015-053  
SCALE: 1" = 40'  
DRAWING NUMBER:  
**C-3.0**

**NOT FOR CONSTRUCTION**  
XREF: S:\PROJECTS\2015\2015053\DWG\15053 TOPBASE.DWG  
XREF: S:\PROJECTS\2015\2015053\DWG\15053 SITE PLANS\VBASE-15053.DWG  
XREF: S:\PROJECTS\2015\2015053\DWG\15053 SITE PLANS\VTBLK-15053.DWG









VIA EMAIL

June 5, 2015

Ms. Jana L. Ecker  
Planning Director  
City of Birmingham  
151 Martin Street, P.O. Box 3001  
Birmingham, MI 48012

**RE: Proposed Whole Foods, Birmingham, Michigan  
PEA Letter Dated 6/4/15 and Site Plan (revision date 6-3-15) Review**

Dear Ms. Ecker:

The professional staff of Fleis & VandenBrink, Inc. (F&V) has completed our review of the above mentioned materials as they relate to our review letter dated May 27, 2015. We have the following comments/findings from these reviews:

- The applicant has addressed the three requested items in our May 27<sup>th</sup> letter.
- Based on the criteria used by the Road Commission for Oakland County, a right turn lane is warranted at the east site driveway.
- With or without the right turn lane added to the east driveway the level of service at the driveway is acceptable during both peak traffic periods.
- The traffic simulation with and without the right turn lane at the east driveway does not exhibit any significant differences in traffic flow or queues.
- Based on the results of the traffic analysis, the City can consider not requiring the proposed right turn lane at the east driveway.
- The project is providing improvements to the pedestrian facilities and is installing bike rack systems at the access points to the store.

We hope that this report addresses the City's needs regarding this project.

If you have any questions, please do not hesitate to contact us at your convenience.

Sincerely,

FLEIS & VANDENBRINK ENGINEERING, INC.

Michael J. Labadie, PE  
Group Manager

27725 Stansbury Boulevard, Suite 150  
Farmington Hills, MI 48334  
P: 248.536.0080  
F: 248.536.0079  
www.fveng.com





**PROFESSIONAL ENGINEERING ASSOCIATES, INC.**  
Civil Engineers | Land Surveyors | Landscape Architects

Corporate Office • 2430 Rochester Court • Suite 100 • Troy, MI 48083  
(P) 248.689.9090 • (F) 248.689.1044 • [www.peainc.com](http://www.peainc.com)

June 4, 2015  
PEA Project No: 2015-053

Via Email: [jecker@bhamgov.org](mailto:jecker@bhamgov.org)

Ms. Jana L. Eckler, Planning Director  
City of Birmingham Community Development Department  
151 Martin Street  
Birmingham, Michigan 48012

**RE: COMMUNITY IMPACT STUDY  
WHOLE FOODS MARKET (2100 EAST MAPLE ROAD)**

Dear Ms. Eckler:

Based upon the comments received in your memorandum dated May 20, 2015, the comments noted in the May 27, 2015 review letter prepared by Fleis & Vandenbrink, and the comments received from the Planning Commission at the May 27, 2015 meeting. We have prepared this summary response letter for your review related to the Community Impact Study. Our responses are noted as follows:

**Community Development Department (5-21-15)**

1. At the May 27<sup>th</sup> Meeting, the requested rezoning from O-1 to B-2 was recommended for approval by the Planning Commission.
2. The revised plan submitted to your office depicts the addition of a pedestrian “node” with seating along the west side of the proposed development.
3. As stated at the Planning Commission Meeting, the anticipated haul road for construction activities would be to enter from the west side of the development and exit from the existing access drive located on the adjacent LA Fitness development. It is expected that construction traffic would generally be directed towards east bound Maple Road.
4. During the construction process it is anticipated that issues with vibration and dust will temporarily increase. It is expected that the general contractor for the project implement appropriate measures throughout the construction process to mitigate these issues.
5. As a part of the final site plan approval submittal, we will provide specifications sheets for the mechanical equipment to ensure that the noise output will not exceed established City noise limits. It should be noted that it is anticipated that the mechanical equipment will be roof mounted and that the building will have a raised parapet to screen the equipment.
6. As stated at the Planning Commission Meeting, the project will be utilizing a compactor unit in lieu of the trash dumpster. The compactor is to be located in the truck well area located along the west side of the building. This truck well will be recessed approximately 4 feet from the proposed finish floor elevation and will be screened. Recycling will occur within the building, unfortunately the specifics of the recycling program has not been determined. This will be provided as a part of the final site plan approval submittal.
7. Specifics of the building security system will be provided as a part of the final site plan approval submittal.



8. Specific related to the fire suppression system, fire access and the location of the Knox Box will be provided as a part of the final site plan approval submittal.

**Fleis & Vandenbrink (5-27-15)**

1. The proposed monument sign has been eliminated.
2. Attached please find a copy of the right turn lane warrant analysis for the proposed right turn proposed at the east entrance. Parsons based the analysis on the projected right turn volumes shown on Figure 6 of their traffic impact study and the 24 hour Maple Road volume cited on page 1 of the same study. The analysis shows a right turn lane is warranted.
3. The Whole Foods Market project proposes to implement the following improvements which will positively impact the City of Birmingham's Multi-Modal Transportation Plan:
  - Install an 8' wide sidewalk along the south side of Maple Road along the entire frontage of the proposed development. This sidewalk will be connected to the internal sidewalk network of the Whole Foods Market and will provide ready access to the front door for pedestrians using the sidewalk.
  - Install a pedestrian "node" along the proposed Maple Road sidewalk.
  - Install a bike rack system at the access points to the Whole Food Market to allow for temporary storage of bikes for those shoppers using this mode of transportation.
  - Install an improved cross walk along the west entrance to improve pedestrian access across this drive approach.
  - Improvement the pedestrian crossing at the west access drive at Maple Road with the installation of pedestrian signals at this location

If you should have any questions, or require any additional information please construction this office directly,

Sincerely,

**PROFESSIONAL ENGINEERING ASSOCIATES, INC.**

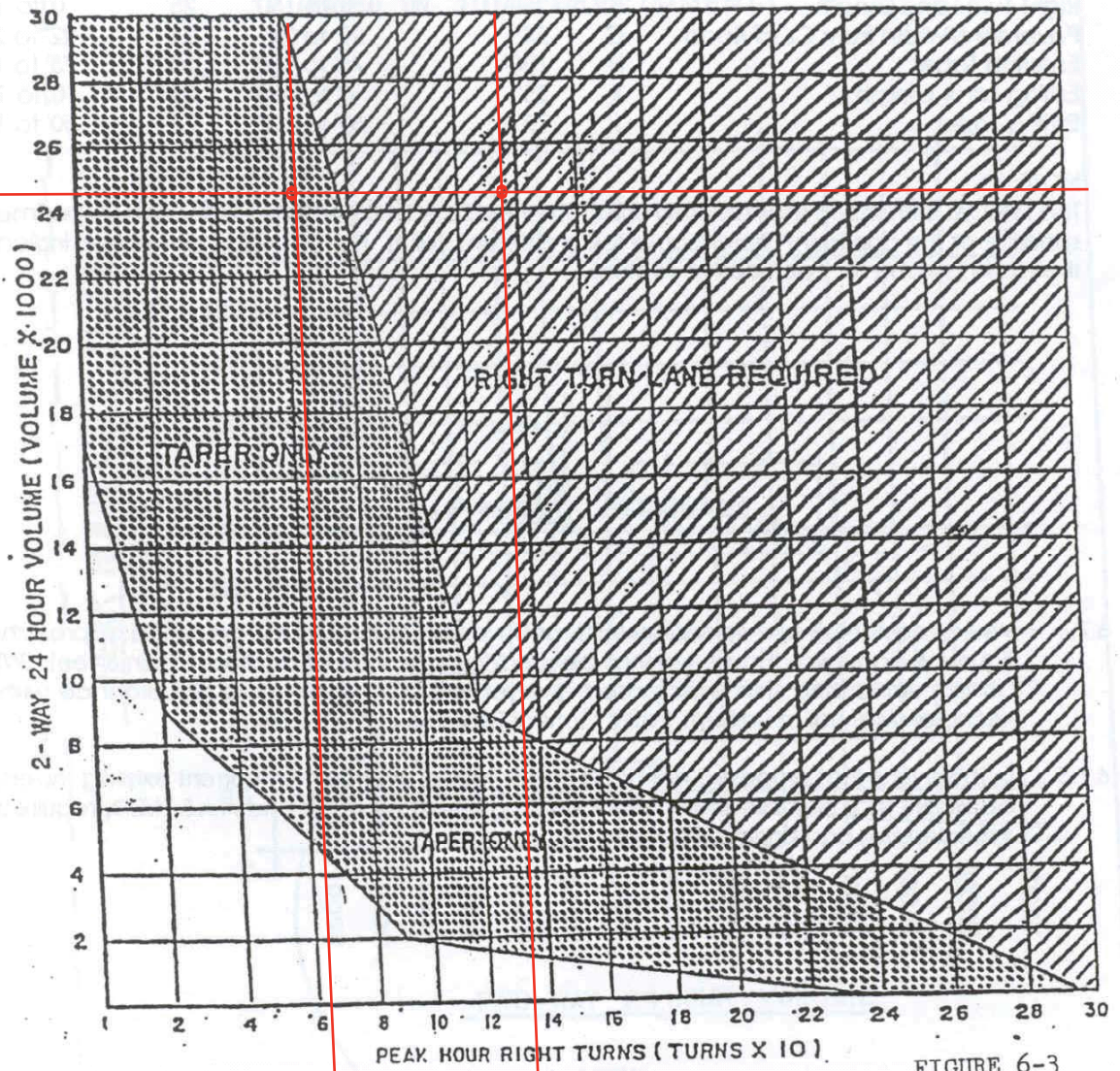


James P. Butler, PE  
President

Enclosure: Right Turn Lane Warrant Analysis

# WARRANTS FOR RIGHT TURN DECELERATION LANE OR TAPER

24,600 VPD  
(2009 Count)



63 AM Peak  
Hour Right  
Turns

134 PM  
Peak Hour  
Right Turns





May 27, 2015

Ms. Jana L. Ecker  
Planning Director  
City of Birmingham  
151 Martin Street, P.O. Box 3001  
Birmingham, MI 48012

VIA EMAIL

**RE: Proposed Whole Foods, Birmingham, Michigan  
Traffic Impact Assessment and Site Plan Review**

Dear Ms. Ecker:

The professional staff of Fleis & VandenBrink, Inc. (F&V) has completed our review of the above mentioned Traffic Impact Assessment, dated May 15, 2015 prepared by Parsons and the site plan prepared by PEA, dated May 11, 2015. We have the following comments/findings from these reviews:

- The study was completed consistent with current traffic engineering practice.
- Currently the traffic delays the intersections of Maple & N. Eton and Maple & Coolidge Highway are LOS E and F for AM and PM peak respectively.
- Improved traffic signal timing will mitigate background to LOS D except for PM peak at Maple & N. Eton which will continue to operate at LOS F.
- Eastbound right and southbound through moments will need to be restricted at the west site drive due to geometry limitations and resulting traffic backups affecting intersections to the west.
- Westbound left turns permitted at the west site drive with the addition of a westbound left turn lane.
- With the geometry changes noted above, the addition of push buttons for pedestrian movements and the addition of the proposed development traffic, the intersection of N. Eton & Maple will operate with reduced delay during both peak traffic periods.
- We recommend that the store monument sign be moved near the east site drive.
- Parsons provide the warrant analysis necessary to support the right turn lane proposed at east site drive.
- Parsons prepare a discussion regarding the impact the proposed project will have on multi-modal improvements in the area identified in the City's Multi-Modal Transportation Plan.

We hope that this report addresses the City's needs regarding this project.

If you have any questions, please do not hesitate to contact us at your convenience.

Sincerely,

FLEIS & VANDENBRINK ENGINEERING, INC.

A handwritten signature in black ink, appearing to read "Michael D. Lebackie".

27725 Stansbury Boulevard, Suite 150  
Farmington Hills, MI 48334

P: 248.536.0080

F: 248.536.0079

www.fveng.com

Michael J. Labadie, PE  
Group Manager



Jana Ecker &lt;jecker@bhamgov.org&gt;

---

**Fwd: Whole Foods Market Proposal**

1 message

---

**Matthew Baka** <mbaka@bhamgov.org>  
To: Jana Ecker <Jecker@bhamgov.org>

Tue, Jun 2, 2015 at 2:02 PM

FYI

----- Forwarded message -----

From: **Michael Kopmeyer** <polkaboy2001@yahoo.com>  
Date: Tue, Jun 2, 2015 at 1:50 PM  
Subject: Whole Foods Market Proposal  
To: "mbaka@bhamgov.org" <mbaka@bhamgov.org>

Dear Mr. Baka:

After viewing preliminary plans for the proposed Whole Foods Market, and after listening to much discussion on traffic at the intersection of Maple and North Eton, I ask for all parties involved to please consider the following:

1. Retail activity, including all primary entrances and exits, must be on the Maple Road side of the building; please put offices to the rear.
2. In the interest of safety and to better connect the greatest number of Birmingham residents to this proposed retail site, two tunnels should be constructed for the benefit of pedestrians and cyclists: one tunnel under Maple at North Eton and one tunnel under the railroad tracks at Villa Street.
3. Convert the driveway, now at the west end of the property, next to the railroad tracks, into a pedestrian plaza, while wrapping one corner front entrance around to the plaza, and consider options for additional use/retail along the tracks.

Thank you.

Michael Kopmeyer  
1351 Bennaville Avenue  
Birmingham, MI 48009

[248.220.2459](tel:248.220.2459)

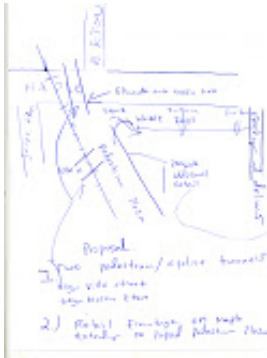
Attachment

---

Matthew Baka  
Senior Planner

The City of Birmingham  
[mbaka@bhamgov.org](mailto:mbaka@bhamgov.org)  
1(248) 530-1848

---



**img001.jpg**  
503K



## MEMORANDUM

Community Development

**Date:** June 4, 2015

**To:** Planning Board Members

**From:** Chris Elliott, Planning Intern

**Re:** Final Site Plan Review—1691/1693 Haynes St

---

The subject site is a .20 acre parcel located on the north side of Haynes between Eton and Adams in the R-4 Zoning District. The existing parcel has a two-family residential duplex on it. The applicant is planning to build two detached garages in the back yard along with a concrete driveway and a new concrete porch for each residence. Article 7 Section 7.25 requires that accessory structures in all zones (except single family residential) must obtain site plan approval.

### 1.0 Land Use and Zoning

- 1.1 Existing Land Use - The existing site is currently occupied by two separate tenants and owned by a landlord. Land uses surrounding the site include attached single family, traditional single family residential dwellings and two family residential.
- 1.2 Existing Zoning – The existing site is currently zoned R-4, Two-Family Residential. The existing two-family use is permitted in the R-4 zones.
- 1.3 2016 Report – The entire subject site is located outside the boundaries of the Downtown Birmingham 2016 Overlay District. The Regulating Plan does not apply in this case.
- 1.4 Summary of Land Use and Zoning - The following chart summarizes existing land use and zoning adjacent to and/or in the vicinity of the subject site.

	North	South	East	West
Existing Land Use	Single Family Residential	Single Family Residential	Two-Family Residential	Two-Family Residential
Existing Zoning District	R-2, Single Family Residential	R-2, Single Family Residential	R-4, Two-Family Residential	R-4, Two-Family Residential
2016 Regulating Plan	NA	NA	NA	NA

## 2.0 Setback and Height Requirements

The project appears to meet the required bulk, height, area and placement regulations for single family homes and accessory structures in the R4 Zoning District. A summary of standards is attached for your review.

## 3.0 Screening and Landscaping

**3.1** Screening – Screen walls are required adjacent to ground-mounted mechanical or electrical equipment which is visible to the public. Such structures are not permitted in the required front or side open space. No mechanical equipment is indicated on the plans. However, any future mechanical equipment must be screened in accordance with the Zoning Ordinance.

**3.2** Landscaping - The applicant has not submitted any plans referring to landscape. While specific landscaping requirements for the R4 are not provided in the Zoning Ordinance, Article 04, section 4.20 (D) 1 requires that all undeveloped or unpaved portions of a site must be planted with grass, ground cover, shrubs, or other suitable live plant material, which shall extend to any abutting street pavement edge.

## 4.0 Open Space

**4.1** Projections into Open Space – Chapter 126, Article 04, section 4.28 (C) states that an unenclosed porch and/or steps may project into a front open space for a maximum distance of 10 feet. The proposed porch in front of the house is 10' which is the maximum allowed sized.

## **5.0 Parking, Loading and Circulation**

- 5.1 Parking - A total of 2 parking spaces are required. The applicant is proposing two one-car, 356 square foot detached garages, thus meeting the parking requirement.
- 5.2 Circulation – The garages will be accessed via the concrete driveway located on each side of the duplex over the existing, split concrete driveway. Pedestrian access to the home will be via the driveway and new concrete steps leading to the proposed concrete porch.

## **6.0 Lighting**

No lighting additions have been noted in the plan but any additional lighting be subject to reviewed as stated in Article 04 of the Zoning Ordinance.

## **7.0 Departmental Reports**

- 7.1 Engineering Division – No concerns were reported.
- 7.2 Department of Public Services – No concerns were reported.
- 7.3 Fire Department – No concerns were reported.
- 7.4 Police Department - No concerns were reported.
- 7.5 Building Division – The proposed construction must comply with 2009 Michigan Residential Code.

## **8.0 Approval Criteria**

In accordance with Article 7, section 7.27 of the Zoning Ordinance, the proposed plans for development must meet the following conditions:

- (1) The location, size and height of the building, walls and fences shall be such that there is adequate landscaped open space so as to provide light, air and access to the persons occupying the structure.
- (2) The location, size and height of the building, walls and fences shall be such that there will be no interference with adequate light, air and access to adjacent lands and buildings.
- (3) The location, size and height of the building, walls and fences shall be such that they will not hinder the reasonable development of adjoining property not diminish the value thereof.

- (4) The site plan, and its relation to streets, driveways and sidewalks, shall be such as to not interfere with or be hazardous to vehicular and pedestrian traffic.
- (5) The proposed development will be compatible with other uses and buildings in the neighborhood and will not be contrary to the spirit and purpose of this chapter.
- (6) The location, shape and size of required landscaped open space is such as to provide adequate open space for the benefit of the inhabitants of the building and the surrounding neighborhood.

## **9.0 Conformance with Downtown Birmingham 2016 Report**

The site is outside the boundaries of the 2016 Report Overlay District. The Regulating Plan does not apply in this case.

## **10.0 Design Review**

The applicant is proposing two, one story, 16 ft. x 24 ft. detached garages in the northeast and northwest corners of the property. The design proposes to use Certainteed "Mainstreet White" horizontal vinyl siding around the garage and Certainteed "Moire Black" shingles on the roof. They are using the same shingles as used on the existing duplex. The garages will have a gable roof. There will be screened gable vents at the top of the front and rear elevation of the garage. The applicant proposes two sliding windows on the right and left elevation of each garage with 4 "decorative trim. One vehicular door is proposed on the front elevation of each garage with decorative trim.

## **11.0 Recommendation**

Based on our review of the site plan submitted, The Planning Division recommends the Planning Board APPROVE the Final Site Plan for 1691 & 1693 Haynes Rd.

## **12.0 Sample Motion Language**

Motion to APPROVE the Final Site Plan for 1691 & 1693 Haynes Rd.

OR

Motion to DENY the Final Site Plan for 1691 & 1693 Haynes Rd.

OR



Motion to APPROVE the Final Site Plan for 1691 & 1693 Haynes Rd.  
with the following conditions.

1. \_\_\_\_\_
2. \_\_\_\_\_

## Zoning Compliance Summary Sheet

### Final Site Plan Review

1691 & 1693 Haynes Street

**Existing Zoning:** R-4, Two-Family Residential,

**Existing Land Use and Zoning of Adjacent Properties:**

	North	South	East	West
<b>Existing Land Use</b>	Single-Family Residential	Single-Family Residential	Two-Family Residential	Two-Family Residential
<b>Existing Zoning District</b>	R-2, Single Family Residential	R-2, Single Family Residential	R-4, Two-Family Residential	R-4, Two-Family Residential

**Land Area:** existing: 12,360 sq. ft.  
proposed: 12,360 sq. ft.

**No. Dwelling Units:** existing: 2  
proposed: 2

**Minimum Lot Area:** required: 3,000 sq. ft. per dwelling unit  
proposed: 4,315 sq. ft. per dwelling unit

**Minimum Floor Area:** required: 800 sq. ft. per dwelling unit  
proposed: no proposed change

**Front Setback:** required: 25 ft.  
proposed: 30 ft.

**Side Setbacks:** required: Accessory building or structures must be at least 3 feet from any lot line  
proposed: West side -3.5 ft.  
East side – 3.5 ft.  
Combined total – 7 ft.

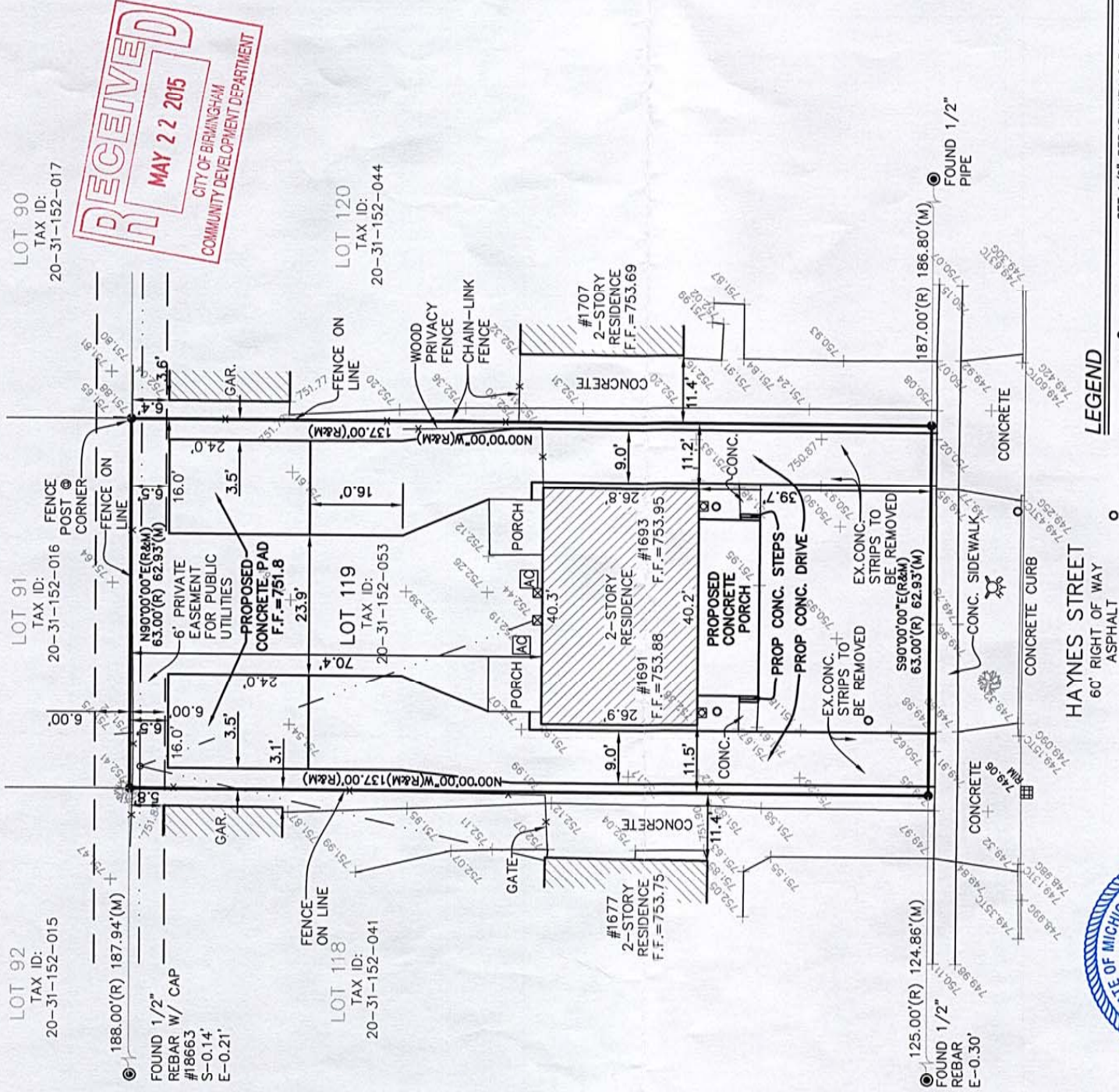
**Rear Setback:** required: Accessory building or structures must be at least 3 feet from any lot line  
proposed: 6.5 ft.

<b>Minimum Open Space:</b>	required:	N/A
	proposed:	N/A
<b>Max. Bldg. Height:</b>	permitted:	12 ft. to the midpoint and 2 stories (Accessory)
	proposed:	8' 2" to the eaves (Garages)
<b>Parking:</b>	required:	2 spaces
	proposed:	2 parking spaces provided in garages
<b>Area:</b>	required:	Max. 10% of the total lot area, (1,236 sq. ft.)
	proposed:	768 sq. ft. combined
<b>Projections into Open Spaces:</b>	permitted:	Steps may project into a front open space for a maximum distance of 10 feet. This provision shall not reduce the required front setback to less than ten (10) feet.
	proposed:	Front porch projects 10 ft. into front yard

SCALE: 1"=20'



**NOTE:**  
A CURRENT TITLE POLICY HAS NOT BEEN FURNISHED AT TIME OF SURVEY, THEREFORE EASEMENTS AND/OR ENCUMBRANCES AFFECTING SUBJECT PARCEL MAY NOT BE SHOWN.



**LEGEND**



SET 1/2" REBAR WITH CAP P.S. 47976  
FOUND MONUMENT (AS NOTED)  
RECORD AND MEASURED DIMENSION  
RECORD DIMENSION  
MEASURED DIMENSION  
GROUND POINT  
ELECTRIC METER  
AIR CONDITIONING UNIT  
UTILITY POLE  
GAS LINE MARKER  
GAS METER  
SQUARE CATCH BASIN  
FIRE HYDRANT  
DECIDUOUS TREE

I HEREBY CERTIFY THAT I HAVE SURVEYED THE PROPERTY HEREIN DESCRIBED. THE ELEVATIONS SHOWN HEREON ARE BASED ON A FIELD SURVEY AND THE DRAWING HEREON DELINEATED IS A CORRECT REPRESENTATION OF THE SAME.

*Anthony T. Sycko, Jr.*



**KEM-TEC & ASSOCIATES**  
22556 GRATIOT AVE. EASTPOINTE, MI 48021  
PROFESSIONAL SURVEYORS - PROFESSIONAL ENGINEERS  
(586)772-2222 \* FAX (586)772-4048





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HEAD OFFICE  
2950 Boul. Lemire, Drummondville  
( Quebec ) Canada, J2B 7J6  
1-800-567-5267

#### NOTES:

THESE PLANS HAVE BEEN DRAWN ACCORDING TO HIGH-QUALITY STANDARDS AND PRACTICES AND ARE AN ACCURATE GUIDE TO BUILDING CONSTRUCTION. HOWEVER, LOCAL REGULATIONS AND LOCAL BUILDING CODE REQUIREMENTS VARY, AND AS SUCH, MAY REQUIRE CHANGES. THE BUILDING CONTRACTOR MUST REVISE AND ENSURE WITH HIS CLIENT THAT THE PLANS CONFORM TO ALL CURRENT GOVERNMENTAL AND/OR BUILDING CODE REQUIREMENTS IN THE MUNICIPALITY WHERE THE HOME WILL BE BUILT. THE CLIENT IS RESPONSIBLE FOR THE VERIFICATION OF MUNICIPAL REGULATIONS.

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#### CLIENT INFO

SUSAN COYLE

1691 HAYNES ST  
BIRMINGHAM  
MI, US 48009-6820

(248)535-1845

#### REVISION

NO DATE BY

#### STAMP BY:

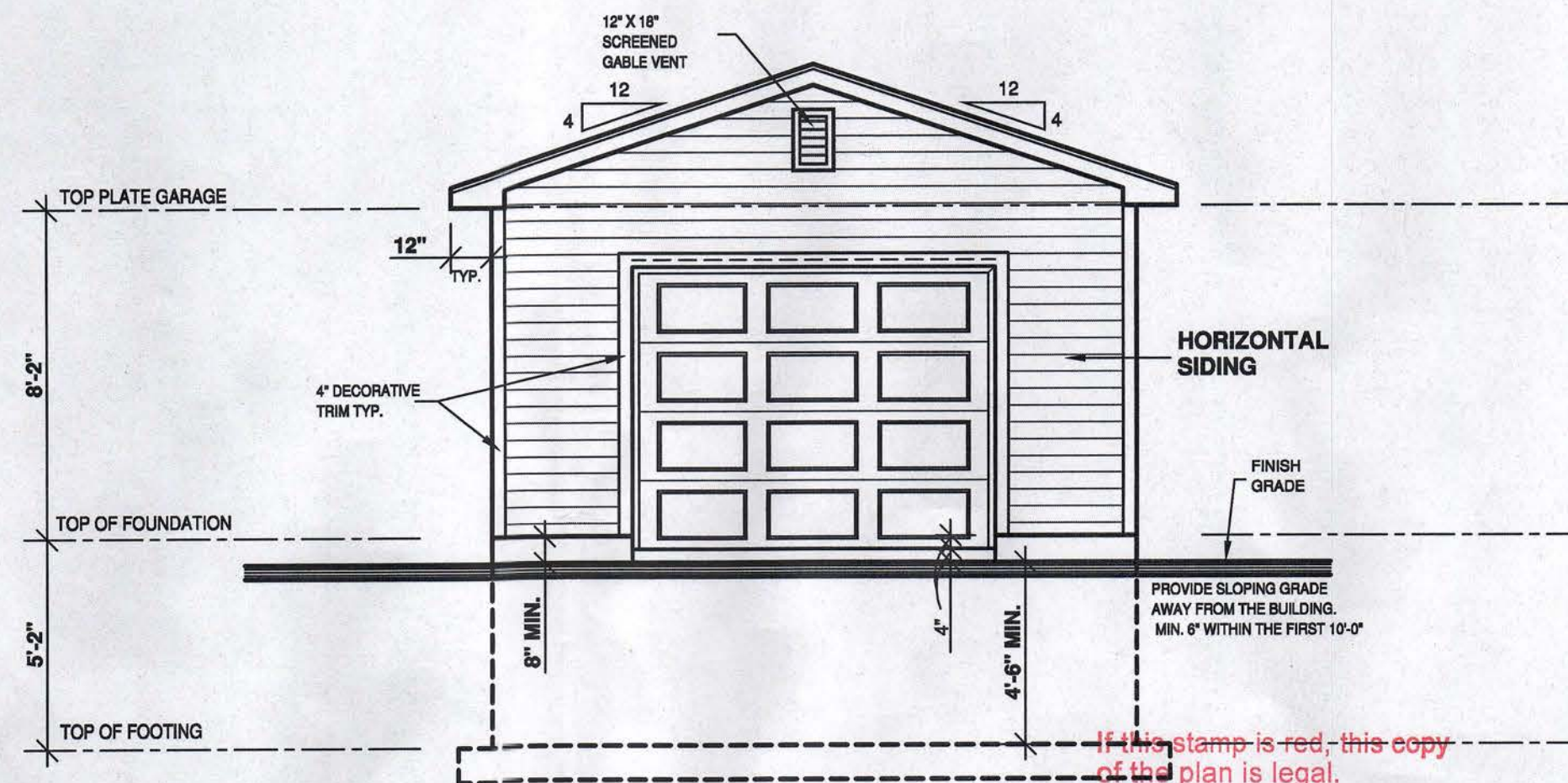
#### PROJECT INFO

**NEW CONSTRUCTION**  
(Floating Slab)

#### DRAWING

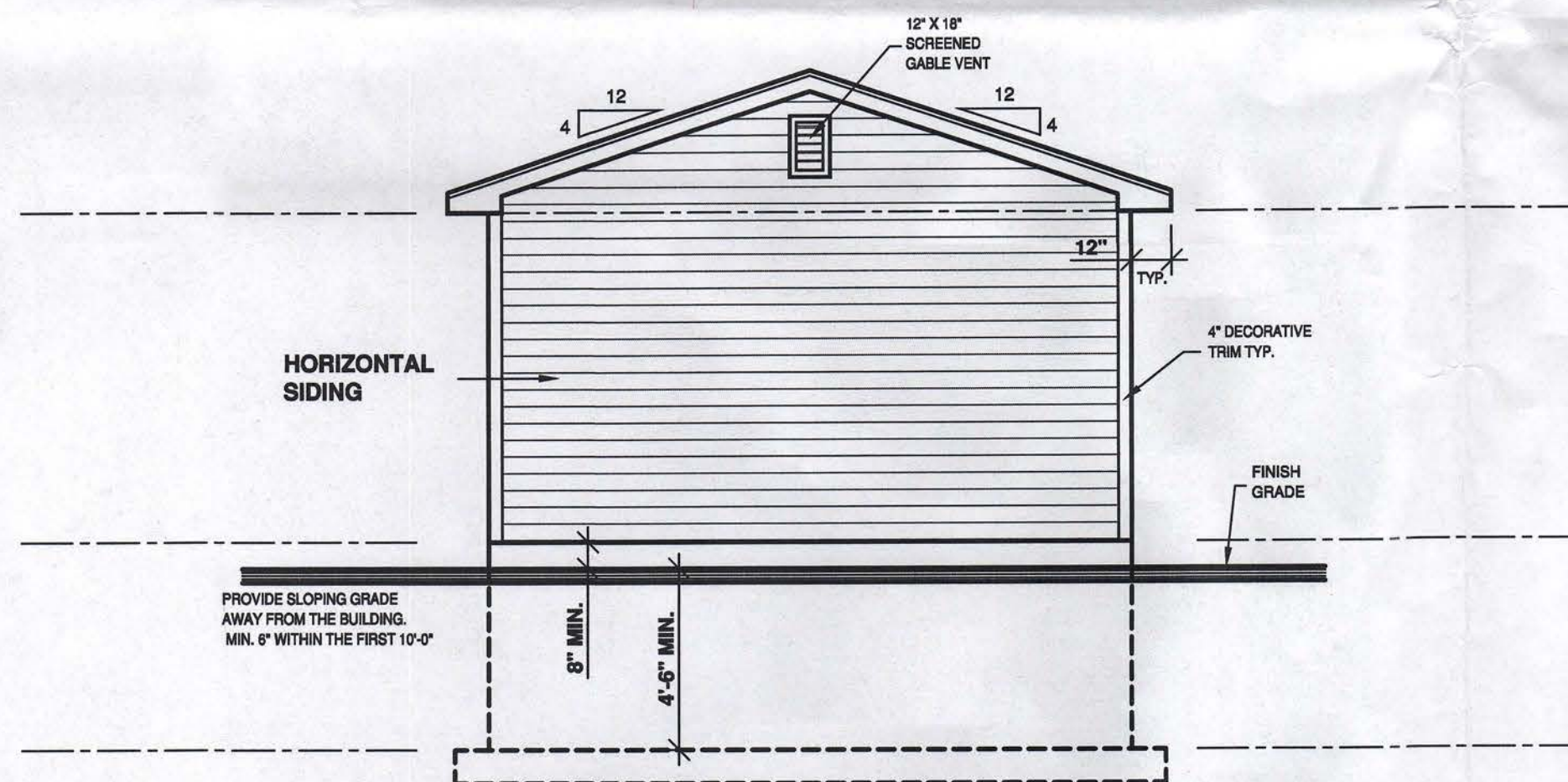
**ELEVATIONS**

DESIGNED BY	DRAWN BY	CHECKED BY
D.C.	V.R.	S.M.
DATE	18/03/2014	SCALE 1/4"=1'-0"
PLAN NO.	2996-16	SHEET NO. 1-5



## FRONT ELEVATION

If this stamp is red, this copy of the plan is legal. Otherwise, it violates the copyright laws. Any offender, including the print shop, is exposed to law suits.



## REAR ELEVATION

AREA SCHEDULE	SQ.FT.
GROUND FLOOR	----
SECOND FLOOR	----
BASEMENT	----
OTHER	----
BONUS ROOM	----
GARAGE	384

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1-800-567-5267

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CLIENT INFO

SUSAN COYLE

1691 HAYNES ST  
BIRMINGHAM  
MI, US 48009-6820

(248)535-1845

REVISION

NO DATE BY

STAMP BY:

PROJECT INFO

**NEW CONSTRUCTION**  
(Floating Slab)

DRAWING

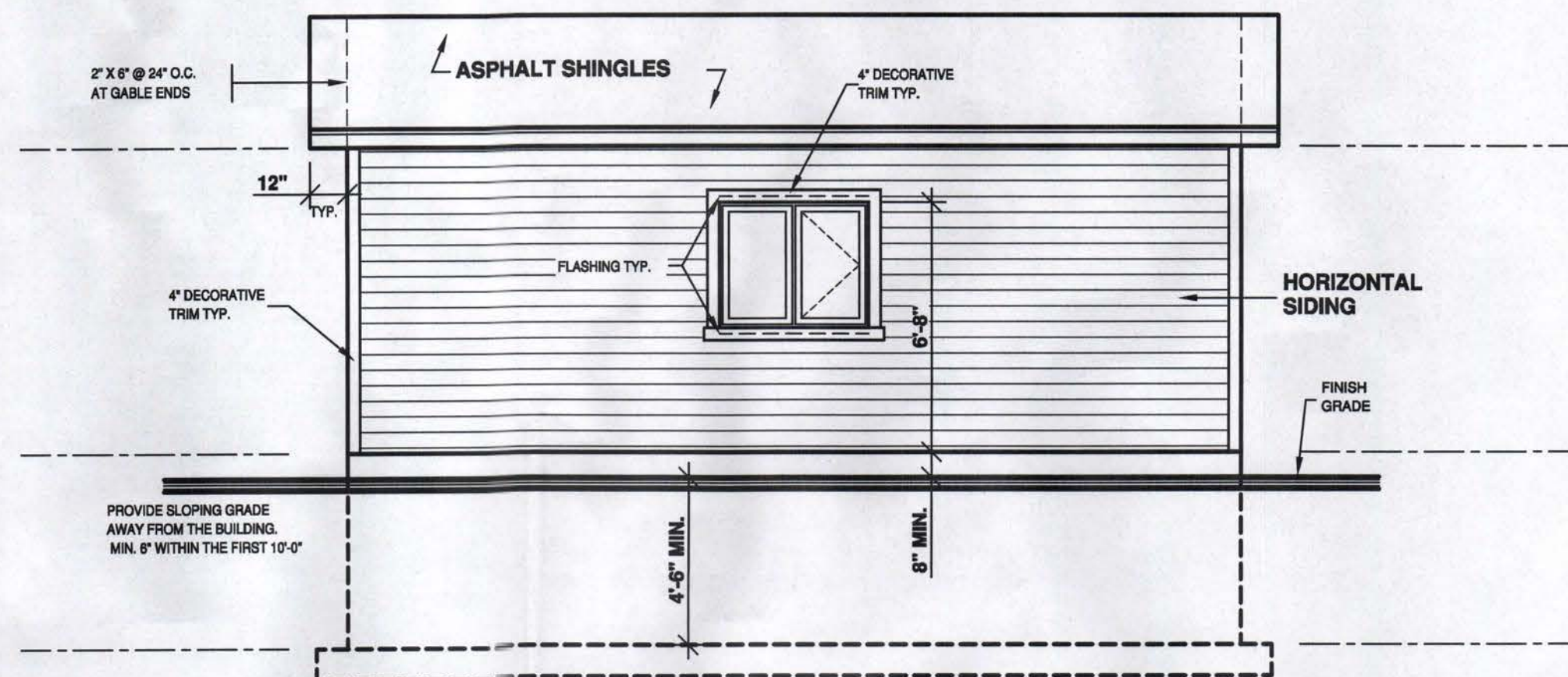
**ELEVATIONS**

DESIGNED BY D.C. DRAWN BY V.R. CHECKED BY S.M.

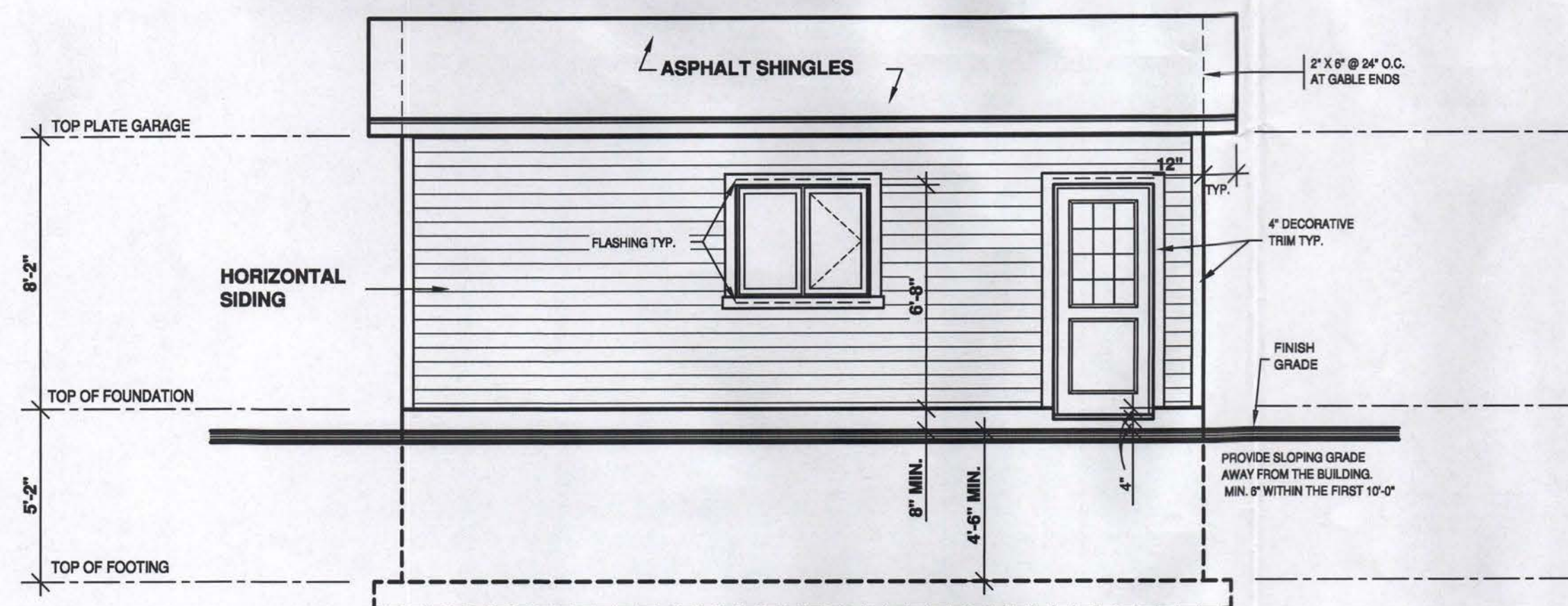
DATE 18/03/2014 SCALE 1/4" = 1'-0"

PLAN NO. 2996-16 SHEET NO. 2-5

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## RIGHT ELEVATION



## LEFT ELEVATION



GENERAL NOTES:

- BUILDING CONTRACTOR MUST:  
1) VERIFY ALL DIMENSIONS BEFORE PROCEEDING.  
2) RESPECT, IF APPLICABLE, REQUIREMENTS OF DOCUMENTATION ATTACHED TO PLANS (EX: SPECIFICATIONS, VENTILATION PLAN ETC.) AND INFORM THE CLIENT OF ANY CONSEQUENCES RESULTING FROM CHANGES TO PLAN.
- CERTAIN DIMENSIONS MAY VARY ACCORDING TO THE MATERIAL USED AND/OR THE CONTRACTOR'S BUILDING METHODS. IF VARIATIONS EXIST BETWEEN THE BUILDING SITE AND PLANS, THE CONTRACTOR MUST ADVISE DRUMMOND DESIGNS INC. AS SOON AS POSSIBLE.
- WRITTEN DIMENSIONS ALWAYS TAKE PRECEDENCE OVER SCALE MEASUREMENTS.
- INFORM IMMEDIATELY DRUMMOND DESIGNS INC. OF ANY OMISSIONS OR ERRORS ON THESE PLANS.

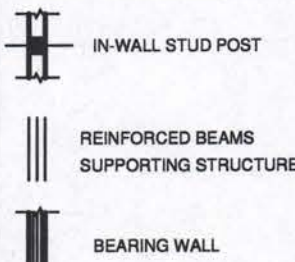
IMPORTANT NOTES:

- APPLICATION OF WOOD PRESERVATIVE REQUIRED AT EXTREMITIES OF BEAMS OR ELEMENTS SUPPORTED ON MASONRY WALLS.
- AS PRECAUTION, A FIREPROOF LINER SHOULD BE INSTALLED BETWEEN FIREBOX OR PREFABRICATED FIREPLACE AND COMBUSTIBLE FRAMING.
- MINIMUM FOUNDATION DEPTH BELOW FINISHED GRADE IS 4'-6". THIS DEPTH MAY VARY ACCORDING TO LOCAL CODES REQUIREMENTS.
- ENDS OF STEEL BEAMS SUPPORTED ON EXTERIOR WALL SHELL BE INSULATED ON A MINIMUM DISTANCE OF 24".
- TO PROTECT AGAINST FROST HEAVE ON ALL CONCRETE FOUNDATIONS WALLS, PILASTER AND CONCRETE ANGLES, MUST BE COVERED WITH EITHER A PROTECTIVE MEMBRANE, 15 LBS ASPHALT PAPER, POLYETHYLENE, RIGID INSULATION OR 1/2" TAR FIBERBOARD.
- 2" MINIMUM SPACE REQUIRED BETWEEN THE ROOF AREA AND MOISTURE SENSITIVE EXTERIOR FACING.
- THE WINDOW DIMENSIONS DESCRIBED IN THIS PLAN MAY VARY DEPENDING ON THE CHOSEN MANUFACTURER
- BASEMENT WINDOWS BELOW GARDE WILL BE PROTECTED BY GALVANIZED STEEL WINDOWS WELLS, AND GRAVEL-FILLED 4" DRAIN, CONNECTED TO PERIMETER DRAIN AND INSTALLED ACCORDING TO THIS PLAN.
- ALL VAPOR BARRIERS INDICATED IN THIS PLAN SHOULD BE SEALED WHEN THEY ARE OVER LAPPED AND AROUND OPENINGS.
- IT IS IMPORTANT THAT A PIECE OF VAPOR BARRIER IS INSTALLED BEHIND THE ELECTRICAL PANEL ON AN EXTERIOR WALL, AND SHOULD BE SEALED WITH THE MAIN VAPOR BARRIER.

STRUCTURAL NOTES:

- FRAMING LUMBER (BEAMS, LINTELS, JOIST) TO BE GRADE NO 1 & 2 SPRUCE UNLESS OTHERWISE INDICATED.
- ALL LINTELS TO BE 2-2"x10" UNLESS OTHERWISE NOTED.
- ALL POST IN EXTERIOR WALL TO BE MIN. 3-2"x6" UNLESS OTHERWISE NOTED.
- ALL INTERIOR POST TO BE MIN. 3-2"x4" UNLESS OTHERWISE NOTED.
- THE TRUSS MANUFACTURER MUST VERIFY ALL DIMENSIONS TO CHECK FOR CONCORDANCE WITH WHOLE PLAN. SOME DIMENSIONS MAY REQUIRE MODIFICATIONS FOLLOWING MANUFACTURE'S DESIGN.
- TEMPORARY AND PERMANENT TRUSS BRACING MUST BE INSTALLED AS PER TRUSS MANUFACTURER AND CODE REGULATIONS.
- THE PLYWOOD PANELS COULD BE REPLACED BY AN ORIENTED STRESS BOARD PANEL (OSB) OR EQUIVALENT.
- PROVIDE BRACING IN ALL EXTERIOR WALL CORNERS. BRACING CAN BE MADE OF 1/2" PLYWOOD, 1/2" OSB OR STEEL BRACING INSTALLED AT 45° ANGLE.
- IN A MASONRY WALL, THE HORIZONTAL STEEL CLAMPS ARE TO BE PLACED AT A MAX. 3'-0" SPACING, THE VERTICAL ONES AT 16" MAX. THE SIZE OF THE STEEL LINTELS (OVER OPENINGS) WILL NEED TO BE DETERMINED BY THE MASON.
- IT IS RECOMMENDED THAT A 5/8" GYPSUM BOARD ON A 1/2" FURRING @ 16" C/C BE INSTALLED ON THE BASEMENT CEILING TO INCREASE THE FIREPROOFING.

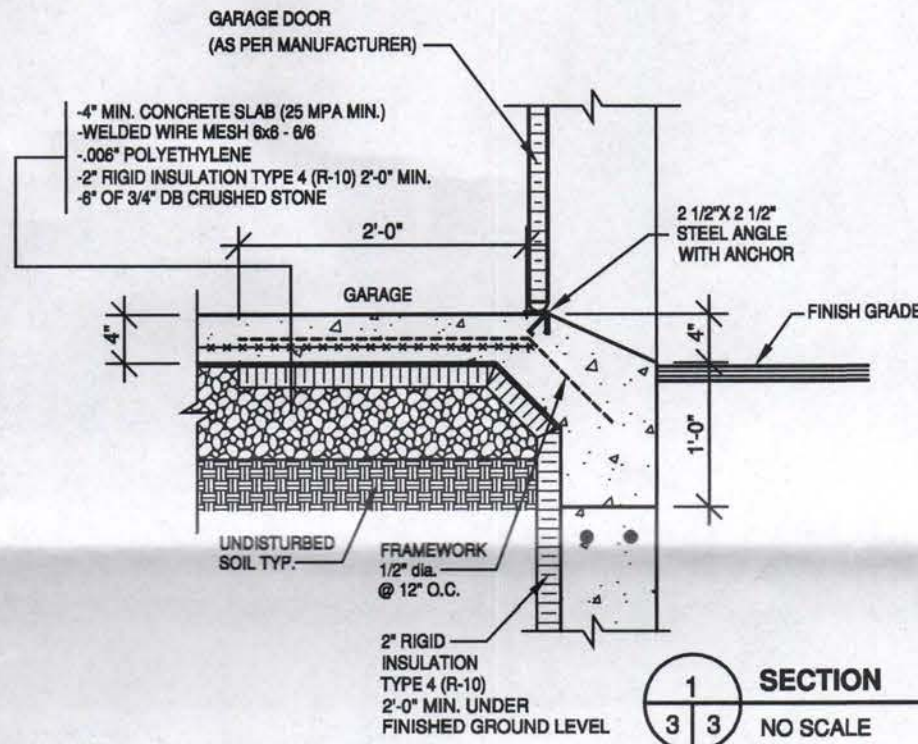
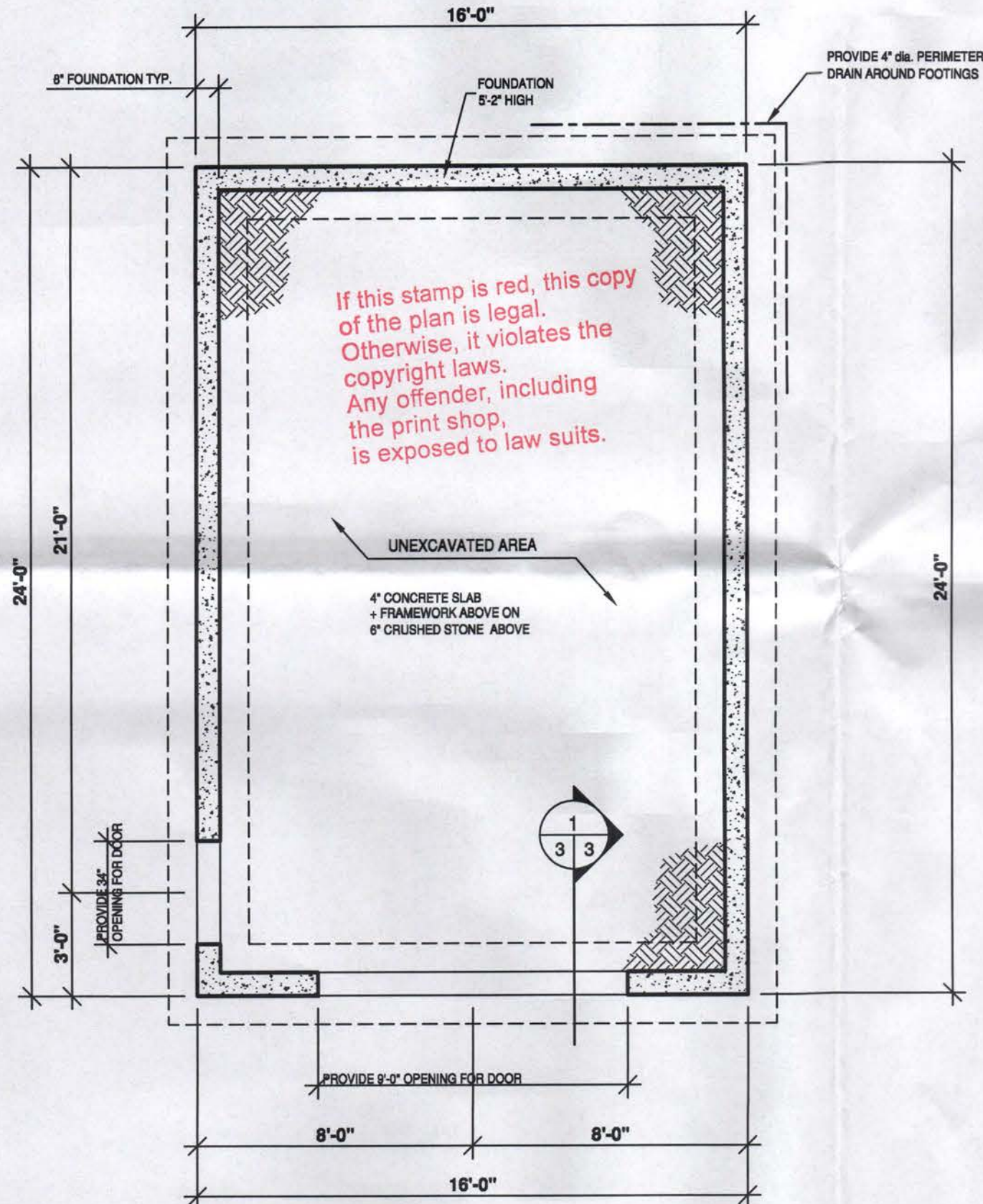
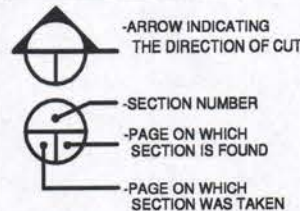
STRUCTURAL LEGEND



ABBREVIATION

- H.R. HANDRAIL  
M.W. MICROWAVE OVEN  
E MARK REFERRING TO DOOR SIZE SCHEDULE

CROSS SECTION



DOOR SCHEDULE	
MARK	SIZE
A	16" x 80"
B	18" x 80"
C	24" x 80"
D	28" x 80"
E	30" x 80"
F	32" x 80"
G	36" x 80"
H	48" x 80"
J	60" x 80"
K	34" x 82"
L	72" x 80"



CONTACT US

HEAD OFFICE  
2950 Boul. Lemire, Drummondville  
( Quebec ) Canada, J2B 7J6  
1-800-567-5267

NOTES:

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CLIENT INFO

SUSAN COYLE  
1691 HAYNES ST  
BIRMINGHAM  
MI, US 48009-6820  
(248)535-1845

REVISION	NO	DATE	BY

STAMP BY:

PROJECT INFO

NEW CONSTRUCTION  
(Floating Slab)

DRAWING

DETAILS and  
FOUNDATIONS PLAN

DESIGNED BY D.C.	DRAWN BY V.R.	CHECKED BY S.M.
DATE 18/03/2014	SCALE 1/4"=1'-0"	SHEET NO. 3-5
PLAN NO. 2996-16		





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#### CONTACT US

HEAD OFFICE  
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#### NOTES:

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MI, US 48009-6820

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CLIENT INFO

REVISION

NO

DATE

BY

STAMP BY:

PROJECT INFO

**NEW CONSTRUCTION**  
(Floating Slab)

DRAWING

**DETAILS and  
GROUND FLOOR PLAN**

DESIGNED BY  
D.C.

DRAWN BY  
V.R.

CHECKED BY  
S.M.

DATE

18/03/2014

SCALE

1/4"=1'-0"

PLAN NO.

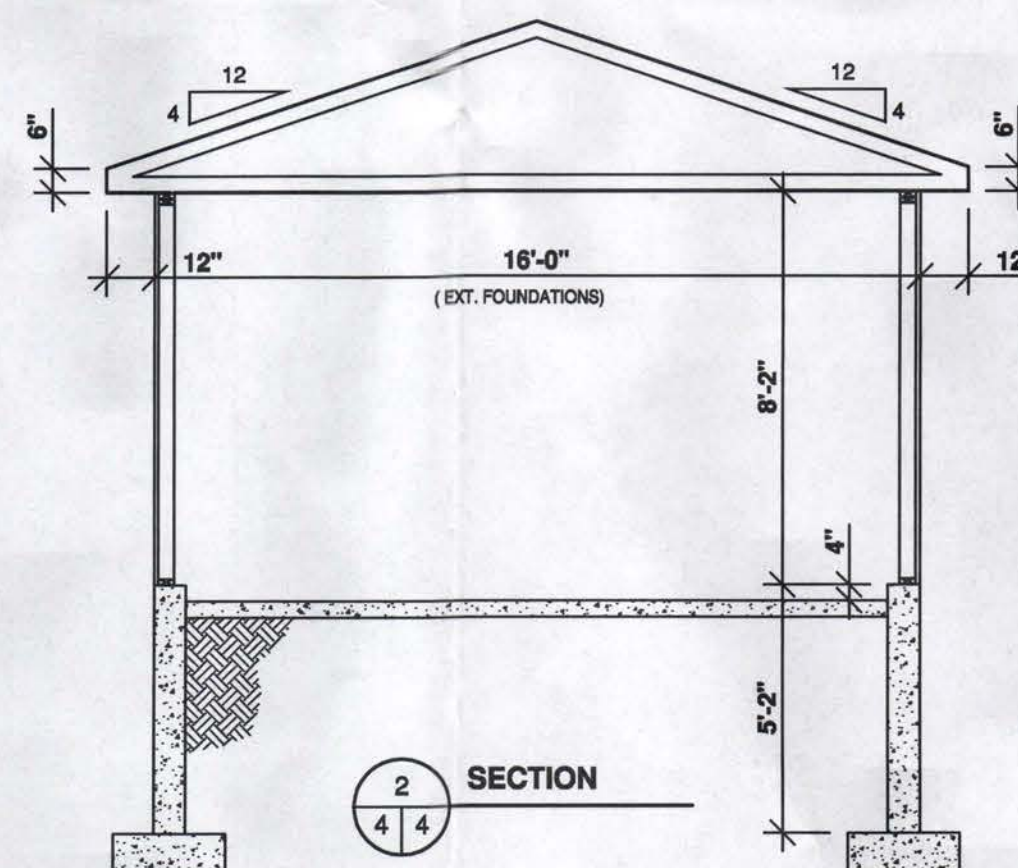
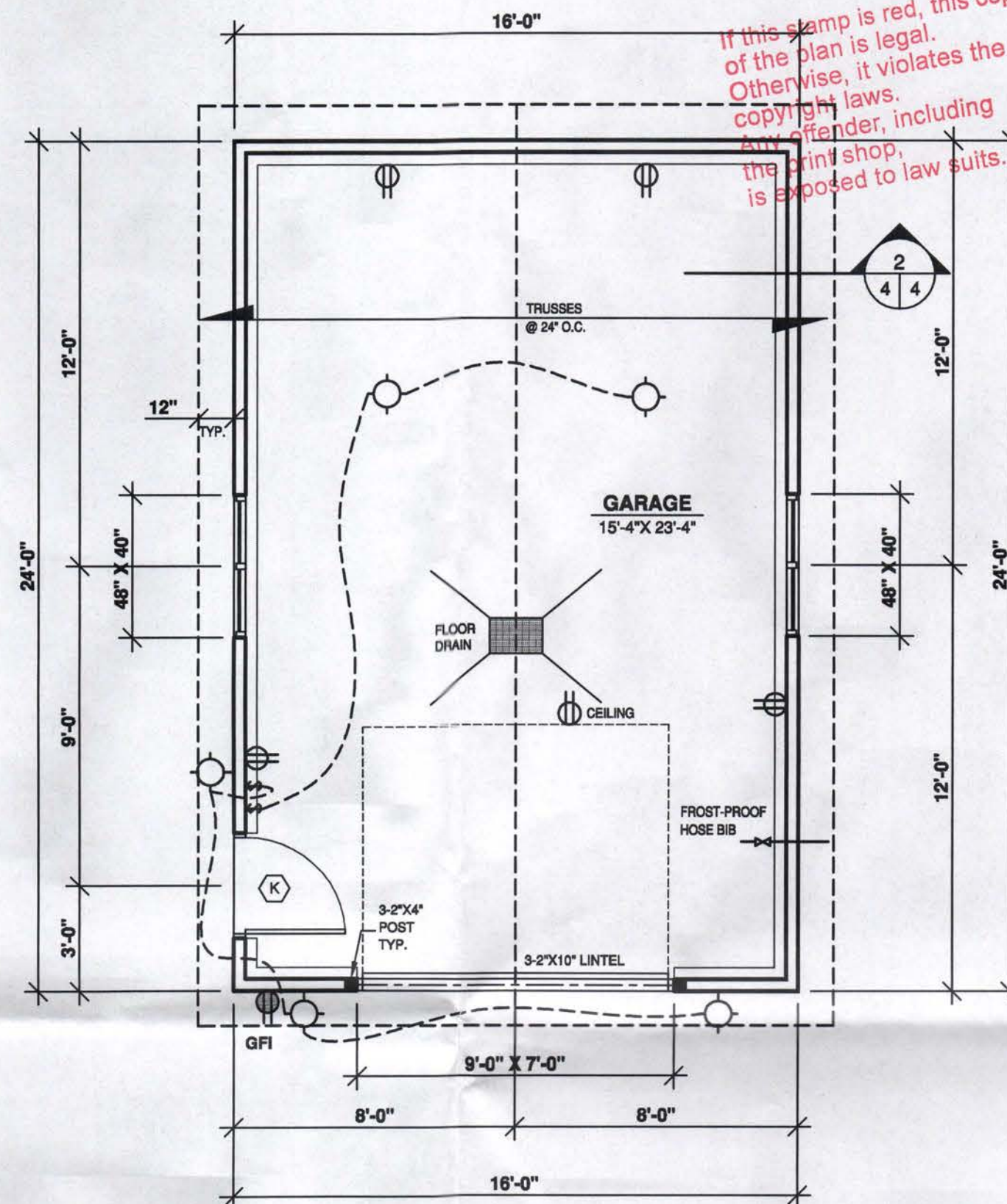
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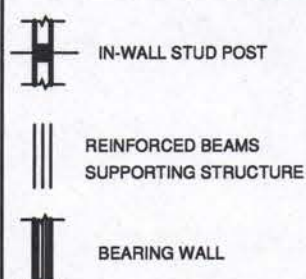
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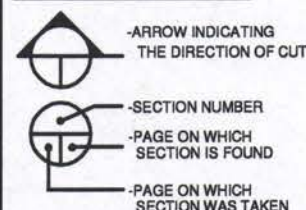
#### STRUCTURAL LEGEND



#### ABBREVIATION

H.R. HANDRAIL  
M.W. MICROWAVE OVEN  
E MARK REFERRING TO DOOR  
SIZE SCHEDULE

#### CROSS SECTION



DOOR SCHEDULE	
MARK	SIZE
A	16" x 80"
B	18" x 80"
C	24" x 80"
D	28" x 80"
E	30" x 80"
F	32" x 80"
G	36" x 80"
H	48" x 80"
J	60" x 80"
K	34" x 82"
L	72" x 80"





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#### REVISION

NO	DATE	BY

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#### PROJECT INFO

**NEW CONSTRUCTION**  
(Floating Slab)

#### DRAWING

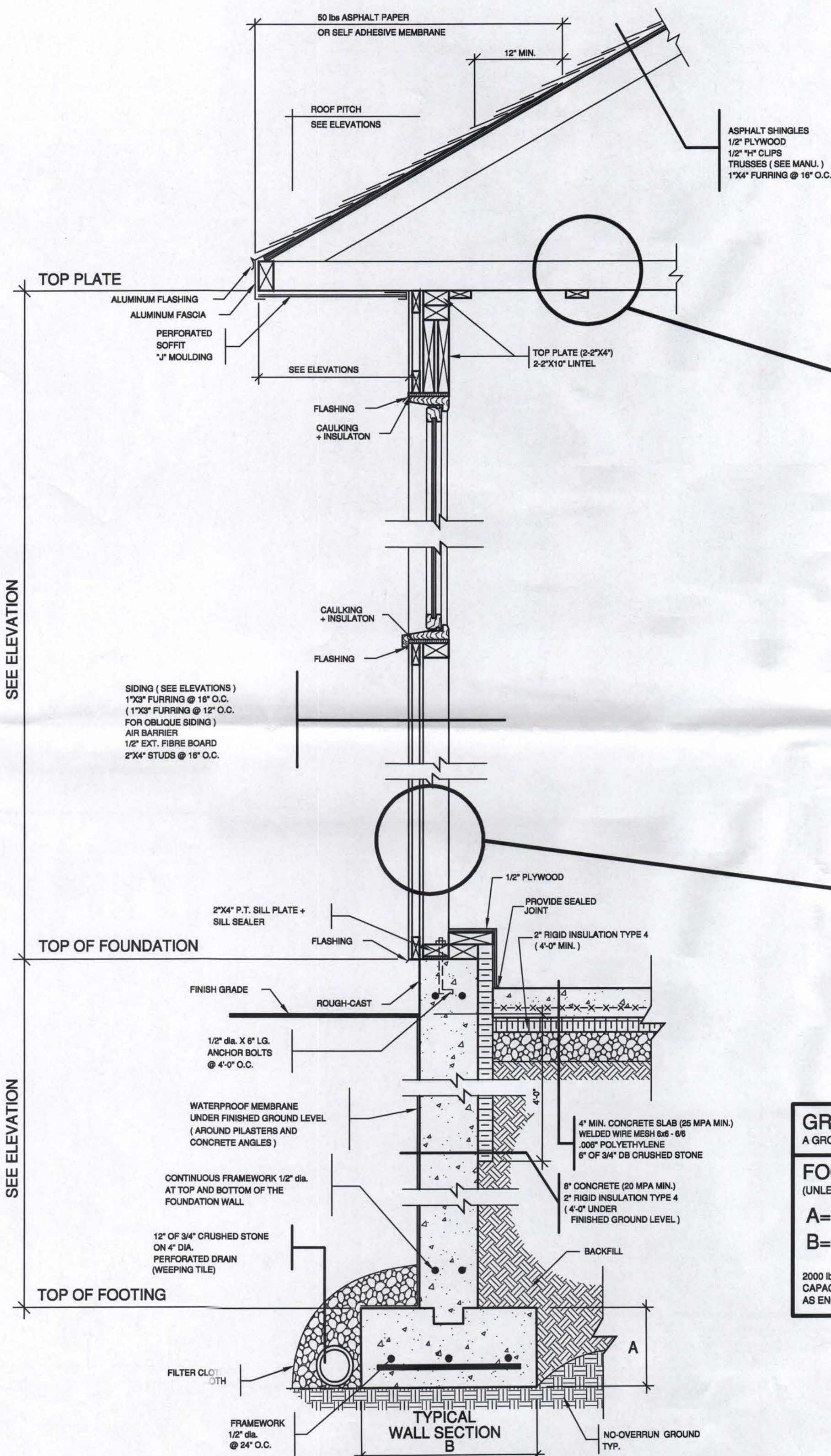
**TYPICAL WALL SECTION**

DESIGNED BY D.C.	DRAWN BY V.R.	CHECKED BY S.M.
DATE 18/03/2014	SCALE 1"=1'-0"	SHEET NO. 5-5
PLAN NO. 2996-16		

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SEE ELEVATION

SEE ELEVATION



#### GROUND LOAD CAPACITY

A GROUND LOAD CAPACITY TEST MUST BE OBLIGATORY.

#### FOOTING DIMENSION

(UNLESS OTHERWISE NOTED)

A= 10"      C= 30"  
B= 24"      D= 20"

2000 lbs/sq.ft ASSUMED GROUND LOAD CAPACITY. FOR LESSER CAPACITY, CONTRACTOR MUST ADJUST FOOTING DIMENSIONS, AS ENGINEER RECOMMENDATIONS.





## MEMORANDUM

Community Development Department

**DATE:** June 4, 2015

**TO:** Planning Board Members

**FROM:** Jana L. Ecker, Planning Director

**SUBJECT:** Study Session on Proposed D5: Downtown Gateway District

---

Please find attached an application received by the Planning Division from the owners of the 555 S. Old Woodward building to request an amendment to the Zoning Ordinance to create a new D5 zoning classification to the Downtown Birmingham Overlay District.

The owners of the 555 S. Old Woodward building are interested in renovating the existing building, and adding new residential units along S. Old Woodward, as well as adding an addition to the south of the existing residential tower for new retail space and residential units. The Building Official previously ruled that any changes to the existing legal non-conforming building would increase the non-conformity, and thus be prohibited unless numerous variances were approved.

Accordingly, the applicant is requesting a Zoning Ordinance amendment to create a new D5: Downtown Gateway Over Five Stories zoning classification in the Downtown Birmingham Overlay District. Over the past several months, the applicant has reviewed several drafts of proposed ordinance language with City staff. On May 13, 2015, the Planning Board began discussing the applicant's proposal to create a new D5: Downtown Gateway Over Five Stories zoning classification in the Downtown Birmingham Overlay District.

Please find attached proposed ordinance language to amend Article 3, section 3.01, 3.02 and 3.04 of the Birmingham Zoning Ordinance for the Planning Board to review and consider. All changes proposed by the applicant are noted in **bold text** (new language) and ~~strike through text~~ (existing language to be removed). Please note the text highlighted in pink as sections recommended for detailed review and discussion by the Planning Board.

### **Suggested Action:**

To conduct another study session on the proposed D5 zone district at the July 8, 2015 meeting of the Planning Board;

OR

To set a public hearing to consider amendments to Article 3, section 3.01, 3.02 and 3.04 of the Zoning Ordinance on **July 8, 2015**.

**ORDINANCE NO. \_\_\_\_\_**

**THE CITY OF BIRMINGHAM ORDAINS:**

AN ORDINANCE TO AMEND CHAPTER 126, ZONING, OF THE CODE OF THE CITY OF BIRMINGHAM:

TO AMEND ARTICLE 03, DOWNTOWN BIRMINGHAM OVERLAY DISTRICT, SECTIONS 3.01 TO 3.04, TO CREATE A NEW D5: DOWNTOWN GATEWAY DISTRICT, AND TO ESTABLISH DEVELOPMENT STANDARDS FOR THIS DISTRICT.

Article 03 shall be amended as follows:

**Section 3.01 Purpose**

The purposes of the Downtown Birmingham Overlay District are to:

- A. Encourage and direct development within the boundaries of the Downtown Birmingham Overlay District and implement the Downtown Birmingham 2016 Plan;
- B. Encourage a form of development that will achieve the physical qualities necessary to maintain and enhance the economic vitality of Downtown Birmingham and to maintain the desired character of the City of Birmingham as stated in the Downtown Birmingham 2016 Plan;
- C. Encourage the renovation of buildings; ensure that new buildings are compatible with their context and the desired character of the city; ensure that all uses relate to the pedestrian; and, ensure that retail be safeguarded along specific street frontages; and
- D. Ensure that new buildings are compatible with and enhance the historic districts which reflect the city's cultural, social, economic, political, and architectural heritage.
- E. Establish a gateway overlay zone to enhance and implement the master plan concept and desired character of Birmingham's gateways as stated in the Downtown Birmingham 2016 Plan, as has been applied and updated.**

**Section 3.02 Applicability**

- A. The Downtown Birmingham Overlay District shall be an overlay district that applies over the existing zoning districts.
- B. Use and development of land within the Downtown Birmingham Overlay District shall be regulated as follows:

1. Any existing use shall be permitted to continue and the use shall be subject to the underlying zoning requirements and not the Downtown Birmingham Overlay District.
  2. Where the usage within an existing building is proposed to be expanded by more than 50% of the existing size, the new use shall be subject to the building use standards of the Downtown Birmingham Overlay District to the maximum extent practical, as determined by the Planning Board.
  3. Any expansion to an existing building that expands the area of the building by more than 40% of the existing building area shall subject the entire building to the requirements of the Downtown Birmingham Overlay District and shall be brought into compliance with the requirements of the Downtown Birmingham Overlay District to the maximum extent practical, as determined by the Planning Board.
  4. Where a new building is proposed, the use and site shall be subject to the requirements of the Downtown Birmingham Overlay District.
- C. Development applications within the Downtown Birmingham Overlay District shall be required to follow the Site Plan Review and Design Review standards contained in Article 7.
- D. A Downtown Birmingham Overlay District Regulating Plan has been adopted that divides the Downtown Birmingham Overlay District into zones. Each zone designated on the Regulating Plan prescribes requirements for building form, height and use as follows:

D2: Downtown Two or Three Stories

D3: Downtown Three or Four Stories

D4: Downtown Four or Five Stories

**D5: Downtown Gateway Over Five Stories**

C: Community Use

P: Parking

### **Section 3.03 General Standards**

- A. The design of buildings and sites shall be regulated by the provisions of the Downtown Birmingham Overlay District.
- B. Section 3.01 to Section 3.04 shall govern the design of all privately owned land within the Downtown Birmingham Overlay District.
- C. The provisions of the Downtown Birmingham Overlay District, when in conflict with other articles of the Zoning Ordinance, shall take precedence.
- D. The provisions of the Downtown Birmingham Overlay District shall specifically supersede the floor-area- ratio, maximum height, and minimum setback regulations contained in each two-page layout in Article 2 of the Zoning Ordinance.
- E. The provisions of the building and building regulations Chapter 22 of the Birmingham City Code and the historic preservation regulations in Chapter 62 of

the Birmingham City Code, when in conflict with the Downtown Birmingham Overlay District, shall take precedence.

- F. The design of community buildings and improvements shall not be subject to the specific standards of this article, but shall be subject to design review by the Planning Board.
- G. Locations designated on the Regulating Plan for new **public** parking garages and civic buildings shall be reserved for such development.

### Section 3.04 Specific Standards

- A. Building Height, Overlay: The various elements of building height shall be determined as follows for the various zones designated on the Regulating Plan:
  - 1. D2 Zone (two or three stories):
    - a. Eave line for sloped roofs shall be no more than 34 feet.
    - b. Peak or ridge of any sloped roof shall be no more than 46 feet as measured to the average grade.
    - c. Maximum overall height including the mechanical and other equipment shall be no more than 56 feet.
    - d. A third story is permitted if it is used only for residential.
    - e. All buildings in D2 Zone containing a third story should be designed harmoniously with adjacent structures in terms of mass, scale and proportion, to the best extent possible.
    - f. A third story shall continue in a different plane, beginning at the eave line, not greater than 45 degrees measured to the horizontal or setback 10 feet from any building facade.
    - g. All buildings constructed in the D2 Zone must have a minimum eave height or 20 feet.
  - 2. D3 Zone (three or four stories):
    - a. Eave line for sloped roofs shall be no more than 46 feet.
    - b. Peak or ridge of any sloped roof shall be no more than 58 feet as measured to the average grade.
    - c. Maximum overall height including the mechanical and other equipment shall be no more than 68 feet.
    - d. A fourth story is permitted if it is used only for residential.
    - e. All buildings in D3 Zone containing a fourth story should be designed harmoniously with adjacent structures in terms of mass, scale and proportion, to the best extent possible.
    - f. The fourth story shall continue in a different plane, beginning at the eave line, no greater than 45 degrees measured to the horizontal or setback 10 feet from any building facade.
    - g. All buildings constructed in a D3 Zone must contain a minimum of 2 stories and must have a minimum eave height of 20 feet.
  - 3. D4 Zone (four or five stories):
    - a. Eave line shall be no more than 58 feet.

- b. Peak or ridge of any sloped roof shall be no more than 70 feet as measured to the average grade.
- c. Maximum overall height including mechanical and other equipment shall be no more than 80 feet.
- d. The fifth story is permitted if it is used only for residential.
- e. All buildings containing a fifth story should be designed harmoniously with adjacent structures in terms of mass, scale and proportion, to the best extent possible.
- f. The fifth story shall continue in a different plane, beginning at the eave line, no greater than 45 degrees measured to the horizontal or set back 10 feet from any building facade.
- g. All buildings constructed in the D4 Zone must contain a minimum of 2 stories and must have a minimum eave height of 20 feet.

**4. D5 Zone (over 5 stories)**

- a. Eave line or roof height of any flat roof building shall be no more than 168 feet as measured to the average grade.**
- b. Peak or ridge of any sloped roof shall be no more than 180 feet as measured to the above average grade.**
- c. Maximum overall height including mechanical and other equipment shall be no more than 180 feet.**
- d. All buildings should be designed harmoniously with adjacent structures in the D5 Zone in terms of mass, scale and proportion to the best extent possible.**

**4-5** C and P Zones: Downtown Birmingham Overlay District building height shall comply with the underlying height restrictions listed in each two-page layout in Article 2 of the Zoning Ordinance, but may be negotiated by the Planning Board.

**5-6.** Stories at sidewalk level shall be a minimum of 10 feet in height from finished floor to finished ceiling, **except this subsection 3.04 (A)(6) shall not apply to those renovations to existing buildings in a D5 Zone that do not have stories existing at the sidewalk level.** The Planning Board may reduce this standard for renovations to existing buildings that do not meet this standard.

**6-7.** A transition line shall be provided between the first and second stories. The transition shall be detailed to facilitate an awning, **except this subsection shall not apply to those renovations to existing buildings in a D5 Zone that do not have a transition line that will facilitate an awning.**

**7-8** The maximum width of all dormers per street elevation on buildings may not exceed 33% of the width of the roof plane on the street elevation on which they are located.

B. Building placement. Buildings and their elements shall be placed on lots as follows:

1. Front building facades at the first story shall be located at the frontage line, except the Planning Board may adjust the required front yard to the average front setback of any abutting building, **except this subsection shall not apply to renovations to any existing building in the D5 Zone where the placement of the building shall not be relocated by the proposed renovations.**
  2. In the absence of a building facade, a screenwall shall be built along the frontage line and aligned with the adjacent building facade. Screenwalls shall be between 2.5 and 3.5 feet in height and made of brick, stone or other masonry material matching the building. Upon approval by the Planning Board, screen- walls may be a continuous, maintained evergreen hedge or metal fencing. Screenwalls may have openings a maximum of 25 feet to allow vehicular and pedestrian access.
  3. Side setbacks shall not be required.
  4. A minimum of 10 foot rear yard setback shall be provided from the midpoint of the alley, except that the Planning Board may allow this setback to be reduced or eliminated. In the absence of an alley, the rear setback shall be equal to that of an adjacent, preexisting building. **This subsection 3.04(B)(4) shall not apply to renovations to existing buildings in a D5 Zone where the rear property line abuts a street and the placement of the building shall not be relocated by the proposed renovations.**
  5. First-floor awnings may encroach upon the frontage line and public sidewalk, but must avoid the street trees; provide at least 8 feet of clearance above the sidewalk; and be set back a minimum of 2 feet from the road curb.
  6. Upper-floor awnings shall be permitted only on vertically proportioned windows, provided that the awning is only the width of the window, encroaches upon the frontage line no more than 3 feet, and is not used as a backlit sign.
  7. Loading docks and service areas shall be permitted only within rear yards. Doors for access to interior loading docks and service areas shall not face a public street. **This section shall not apply where a building faces more than one public street, loading docks, service areas and access doors shall not face the front property line that faces the public street designated as the address of the building.**
  8. All buildings shall have their principal pedestrian entrance **facing the** ~~on a~~ frontage line.
- C. Building use. Buildings shall accommodate the following range of uses for the various designations on the Regulating Plan of the Downtown Birmingham Overlay District:
1. Uses shall be limited to those allowed in each underlying zoning district, unless otherwise specifically provided for herein.
  2. The following uses and conditions are prohibited:
    - a. Automatic food and drink vending machines outdoors;

- b. Drive-in facilities or any commercial use that encourages patrons to remain in their automobiles while receiving goods or services, **except for the D5 Zone where drive-in banks are permitted on the Woodward Avenue frontage;**
  - c. Outdoor advertising.
3. Community uses (C).
  4. Those sites designated as parking uses (P) on the Regulating Plan shall be premises used primarily for parking, except retail frontages shall be encouraged at the first floor level.
  5. Those sites designated D2 Zone, D3 Zone, ~~or~~ D4 Zone, **or D5 Zones** on the Regulating Plan may be used for any commercial, office or residential use as allowed in the underlying zoning district. Upper story uses may be commercial, office or residential, provided that no commercial or office use shall be located on a story above a residential use.
  6. Buildings that have frontage along the required retail frontages, as specified on the Regulating Plan, shall consist of retail with a minimum depth of 20 feet from the frontage **façade** line within the first story. Lobbies for hotels, offices, and multiple-family dwellings may be considered as part of the required retail frontage, provided that any such lobby occupies no more than 50% of the frontage of said building. **This subsection 3.04 (C)(6) shall not apply to existing buildings in a D5 Zone where retail does not exist at the front façade line.**
  7. Retail, office or residential uses are required to have minimum depth of 20 feet from the frontage line on all stories. The remaining depth may be used for off-street parking. Parking access on a frontage line shall be an opening a maximum of 25 feet wide. Openings for parking garage access shall repeat the same rhythm and proportion as the rest of the building to maintain a consistent streetscape.
  8. In any D2 Zone, D3 Zone, or D4 Zone, the first floor shall consist of retail with a minimum depth of 20 feet from the frontage line where designated on the Regulating Plan as a retail frontage line in conformance with Section 3.04(C)(5) and Section 3.04(C)(6).
  9. Office use is limited to one story, except:
    - a. In any D3 Zone or D4 Zone, a two-story building dedicated to office use is permissible; ~~and~~
    - b. In a D4 Zone, two stories may be dedicated to office use when the Planning Board permits a fifth story; **and**
    - c. **In a D5 Zone, a maximum of 3 stories may be dedicated to office use.**
  10. Bistros are permitted with a valid Special Land Use Permit with the following conditions:
    - a. No direct connect additional bar permit is allowed and the maximum seating at a bar cannot exceed 10 seats;
    - b. Alcohol is served only to seated patrons, except those standing in a defined bar area;
    - c. No dance area is provided;
    - d. Only low key entertainment is permitted;



- e. Bistros must have tables located in the storefront space lining any street, or pedestrian passage;
  - f. A minimum of 70% glazing must be provided along building facades facing a street or pedestrian passage between 1 foot and 8 feet in height. **Except in a D5 Zone, this subsection 3.04(C)(10)(f) shall apply only to the building façade facing the front property line for the building, and the 1 foot and 8 foot in height regulation shall not apply to other facades of the building that are not facing the front property line that is adjacent to the public street designated as the address of the building.**
  - g. All bistro owners must execute a contract with the City outlining the details of the operation of the bistro; and
  - h. Outdoor dining must be provided, weather permitting, along an adjacent street or passage during the months of May through October each year. Outdoor dining is not permitted past 12:00 a.m. If there is not sufficient space to permit such dining on the sidewalk adjacent to the bistro, an elevated, ADA compliant, enclosed platform must be erected on the street adjacent to the bistro to create an outdoor dining area if the Engineering Department determines there is sufficient space available for this purpose given parking and traffic conditions.
11. Establishments operating with a liquor license obtained under Chapter 10, Alcoholic Liquors, Article II, Division 3, Licenses for Economic Development, are permitted with a valid Special Land Use Permit only on those parcels on Woodward Avenue identified on Exhibit 1; Appendix C. ~~and in a D5 Zone.~~
- D. Parking requirements.
- 1. For all nonresidential uses located within the parking assessment district, parking on the site shall not be required, provided such site is in full compliance with the requirements of the parking assessment district.
  - 2. For all residential uses located within the parking assessment district, the on-site parking requirements contained in Section 4.46, Section 4.49, Section 4.50 and Section 4.51 may be complied with through leasing the required spaces from an off-site parking area, provided the requirements of Section 4.45(G) are met and all parking is supplied on site or within 300 feet of the residential lobby entrance of the building.
  - 3. For all sites located outside of the parking assessment district, off-street parking must be provided in accordance with the requirements of Article 4 for parking, loading and screening.
  - 4. Notwithstanding the above regulations, residential dwelling units within the existing second and third floors of landmark buildings, as defined in Section 62-87 of the Birmingham City Code, located within the central business historic district are exempt from required off-street parking requirements.
  - 5. Off-street parking contained in the first story shall not be permitted within 20 feet of any building facade on a frontage line or between the building facade and the frontage line, **except in a D5 Zone this section 3.04(D)(5) shall only apply to the building façade facing the front property line that is adjacent to the public street designated as the address of the building.**

6. The placement of two abutting off-street parking lots with continuous street frontages shall not be permitted.
- E. Architectural standards. All buildings shall be subject to the following physical requirements:
1. At least 90% of the exterior finish material on all facades that face a street shall be limited to the following: glass, brick, cut stone, cast stone, **pre-cast or cast in place concrete**, coarsely textured stucco, or wood. Dryvit or E.F.I.S is prohibited.
  2. The primary colors of building exteriors shall be compatible with the colors of adjacent buildings and in character with the surrounding area **within the same Downtown Overlay zone**, although the trim may be of a contrasting color.
  3. Blank walls shall not face a public street. Walls facing a public street shall include windows and architectural features customarily found on the front facade of a building, such as awnings, cornice work, edge detailing or decorative finish materials.
  4. Storefronts shall be directly accessible from public sidewalks. Each storefront must have transparent areas, equal to **a minimum of 70%** of its portion of the facade, between one and eight feet from the ground. The wood or metal armature (structural elements to support canopies or signage) of such storefronts shall be painted, bronze, or powder-coated.
  5. Storefronts shall have mullion systems, with doorways and signage integrally designed. Mullion systems shall be painted, powder-coated, or stained.
  6. The glazed area of a facade above the first floor shall not exceed 35% of the total area, with each facade being calculated independently.
  7. Glass shall be clear or lightly tinted only. Opaque applications shall not be applied to the glass surface.
  8. Facade openings, including porches, windows, and colonnades, shall be vertical in proportion.
  9. Sliding doors and sliding windows are prohibited along frontage lines, **except for residential uses in a D5 Zone above street level**.
  10. ~~(Reserved for future use.)~~ **Notwithstanding any regulations set forth in the foregoing subsections, subsections 3.04(E)(3), (5), (6) and (7), in their entirety, do not apply to the existing buildings in a D5 Zone.**
  11. Cantilevered mansard roofs are prohibited.
  12. Balconies, railings, and porch structures shall be metal, wood, **glass**, **cast in place or preformed** concrete, or stone.
  13. Facades may be supplemented by awnings, which shall be straight sheds without side flaps, not cubed or curved. Awnings shall be between 8 and 12 feet above sidewalk grade at the lower drip edge.
  14. Outside dining tables and chairs shall be primarily metal, wood, or similar material. Plastic outside dining tables and chairs shall be prohibited.
  15. Any building that terminates a view, as designated on the Regulating Plan, shall provide distinct and prominent architectural features of enhanced character and visibility, which reflect the importance of the building's location and create a positive visual landmark.

16. Flat roofs shall be enclosed by parapets. Rooftop mechanical and other equipment shall be limited, positioned and screened to minimize views from adjacent properties and public rights-of-way in accordance with the regulations set forth in Section 4.16, Section 4.18, and Section 4.53.

F. Signage Standards. Signage, when provided, shall be as follows:

1. Building Sign Design Plan: For all newly constructed or exterior renovated buildings, an overall building sign design plan shall be approved by the appropriate reviewing body.
2. Design: Signage shall be integrally designed and painted with the storefront.
3. Address Numbers: Address numbers shall be a maximum of 8 inches in vertical dimension.
4. Sign Band:
  - a. General: A single external sign band or zone may be applied to the facade of a building between the first and second floors, provided that it shall be a maximum of 1.5 feet in vertical dimension by any horizontal dimension.
  - b. Woodward Avenue Address: The external sign band or zone shall be a maximum of 2 feet in vertical dimension by any horizontal dimension. The sign band or zone may contain multiple individual signs, but all must refer to a tenant of the building whose principal square footage is on the first floor. **Except in a D5 Zone where an existing building has retail below grade level, the sign band shall exist either between the below grade level and the next story above the below grade level, and/or above the first story that is above grade.**
  - c. Lowercase letters with ascenders and descenders that extend beyond the limits of the sign height by a maximum of 50% will not be calculated into total sign area.
  - d. Each business whose principal square footage is on the first story, may have one sign per entry. **Except in a D5 Zone where an existing building has retail below grade level, each business whose principal square footage is on either a below grade level or the first floor may have one sign per entry.**
  - e. Where the Historic District Commission, Design Review Board or Planning Board has determined that a horizontal sign band is not architecturally feasible based on building design, an alternative design will be considered, provided the following conditions are met:
    - i. The sign must fit within the total sign area allowed for the business;
    - ii. The sign must be compatible with the building's street design and will enhance the streetscape.
    - iii. The sign adheres to the goals of the 2016 Plan.
5. Building Identification:
  - a. **In a D5 Zone, lighted building identification signs may be placed on all sides of the building. The following sections 3.04 (F)(5)(c), (d) and (e) do not apply to buildings in a D5 Zone.**
    - ~~a~~.b. Signage identifying the entire structure by a building name may be permitted on the sign band.
    - ~~b~~.c. One sign will be allowed on the principal building frontage.

- ~~d~~**d** Two identical signs will be allowed on each elevation of a corner building.
- ~~d~~**e** Non-illuminated signs identifying the entire structure by a building name may be permitted above the first floor provided the following conditions apply:
- i. The building must be located on Woodward;
  - ii. A tenant name must have legal naming rights to the building;
  - iii. The sign must be located on the top floor; and
  - iv. Only one Building Identification sign may be located on the principal building frontage.
6. Tenant Directory Sign: A directory sign may be comprised of individual nameplates no larger than one square foot each, or a changeable copy board for characters not exceeding one inch in height.
  7. Additional Signs: Additional pedestrian signs for first floor tenants shall meet the following requirements:
    - a. These signs shall be attached to a building perpendicular to the facade, and extend up to 4 feet from the facade.
    - b. These signs shall be a maximum of 1.5 feet in vertical dimension and 4 feet in horizontal dimension.
    - c. There may be one (1) individual pedestrian sign for each business located on the first floor, provided that such signs are spaced no less than 20 feet apart horizontally; this shall not deny any first floor place of business at least one projecting sign.
  8. Glass: The storefront glass may be stenciled with signage not to exceed 1.5 feet in vertical dimension and 4 feet in horizontal dimension.
  9. First Floor Awning: The valance shall not be more than 9 inches in height. The valance of an awning may be stenciled with signage totaling no more than 33% of the valance area.
  10. Lighting:
    - a. General: External signs shall not be internally illuminated, but may be back lit or externally lit.
    - b. Woodward Avenue Address: External signs may be internally illuminated.

ORDAINED this \_\_\_\_\_ day of \_\_\_\_\_, 2015 to become effective 7 days after publication.

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Stuart Lee Sherman, Mayor

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Laura Pierce, City Clerk

**DRAFT Planning Board Minutes**  
**May 13, 2015**

**STUDY SESSION**

**Proposal to add D-5: Downtown Gateway Over Five Stories to the Downtown Birmingham Overlay District**

Ms. Ecker advised that the Planning Division has received an application from the owners of the 555 S. Old Woodward building to request an amendment to the Zoning Ordinance to create a new D-5 zoning classification to the Downtown Birmingham Overlay District.

The building owners are interested in renovating the existing buildings and adding new residential units along S. Old Woodward Ave., as well as adding an addition to the south of the existing residential tower for new retail space and residential units. The building official previously ruled that any changes to the existing legal non-conforming building would increase the non-conformity, and thus be prohibited unless numerous variances were approved. Therefore, the petitioner feels their hands have been tied in terms of making exterior and structural improvements to the building.

Accordingly, the applicant is requesting a Zoning Ordinance amendment to create a new D-5: Downtown Gateway Over Five Stories zoning classification in the Downtown Birmingham Overlay District. Over the past several months, the applicant has reviewed several drafts of the proposed ordinance language with City staff.

Proposed ordinance language to amend Article 3, section 3.01, 3.02 and 3.04 of the Birmingham Zoning Ordinance was presented for the Planning Board to review and consider.

Mr. Rick Rattner, 380 N. Old Woodward Ave., Attorney for the petitioner, was present with a representative of the owner, Mr. Jerry Reinhart; the architect, Mr. Bob Ziegelman; and a landscaper from his office. Mr. Rattner gave a presentation aimed at convincing the Planning Board why the petitioner would like to see the changes made and why it would work in this particular location. Their primary goal is to get the building zoned so that it comes into compliance. They want to do a building that is an icon in the City of Birmingham and a great gateway to the City, along with being completely in line with the 2016 Plan. Included in the presentation was a video depicting Andres Duany's comments when he came to the City in 2014. He stated it is a special building that requires special treatment and it could become incredibly exciting and really cool.

Mr. Koseck said they have not seen a site plan showing the footprint relative to property lines, along with the expansion opportunity. The building needs to be seen in its context. He received confirmation that the tall building is apartments and the other building contains office space. Ms. Ecker said the way this ordinance is written the commercial side could potentially go up an equivalent height to the apartment side.

Mr. DeWeese thought it would be appropriate for the board to think through, if they were going to allow a building of that scale, what they would want there that fits the spirit and essence of the rest of Downtown. He knows that the back side is not inviting at all from the Woodward

Ave. side and the front side is not pedestrian oriented the way it is set up. The lower levels could be made more friendly and the parking garage covered up.

Chairman Clein felt the board should look at the proposed ordinance and decide whether creating a D-5 Zone makes sense. Mr. Williams considered this an iconic structure that is long overdue for attention. The Planning Board has almost totally ignored the south end of town, so let's start with this.

Mr. Koseck noted there are buildings being built today that look a lot like this. They have beautiful high tech glass and he knows what Duany is talking about in terms of lighting it so that it glows. Mr. Williams thought the only practical way to proceed with this study is to set up a sub-committee of this board to work with staff.

Chairman Clein suggested the next step would be to come back to a study session to allow the board to review and provide their input. It was discussed that the board should not create the language of the district around a specific project. Everyone agreed that another study session is in order so that the board can look at all of the implications of the request. June 10 would be the earliest.

Mr. Rattner said it is important to him to put together a package for Ms. Ecker as quickly as they can. Chairman Clein asked for a graphic of an existing site plan so the board knows what parcels are included and what are not. Context should be shown so it is clear what is around the site and how that plays into it. Mr. Koseck added it is about the existing footprint, the applicant's ownership limits, and context within 200 ft.




Mr. Williams stated this is an important building and the board will treat it accordingly.

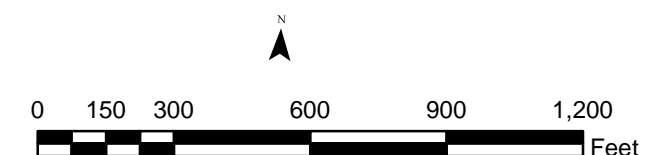




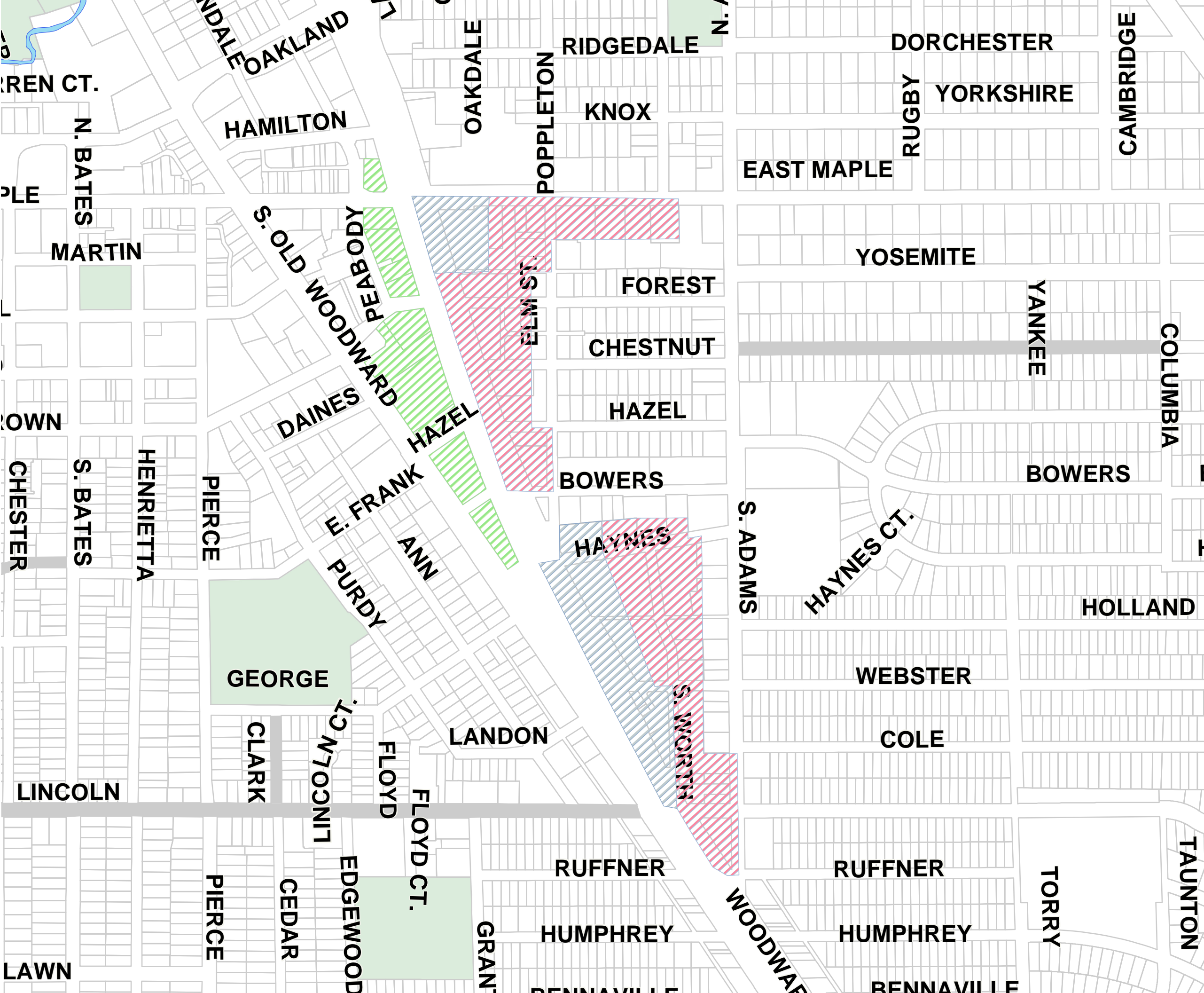
# Woodward Gateway D-5 Zone

## Legend

-  Zoning allows 9 stories
-  Zoning allows 7 stories
-  Possible D-5 Zone



1 in = 422.818 feet



Photo 

Hazeltown St  
S Old Woodward Ave



555 S Old Woodward Ave

1



Bowers St

Spa Mariana



Haynes St



Walgreens

S North St

Moosejaw



Papa Joe's Birm



North St

Ann St

S Old Woodward Ave

1

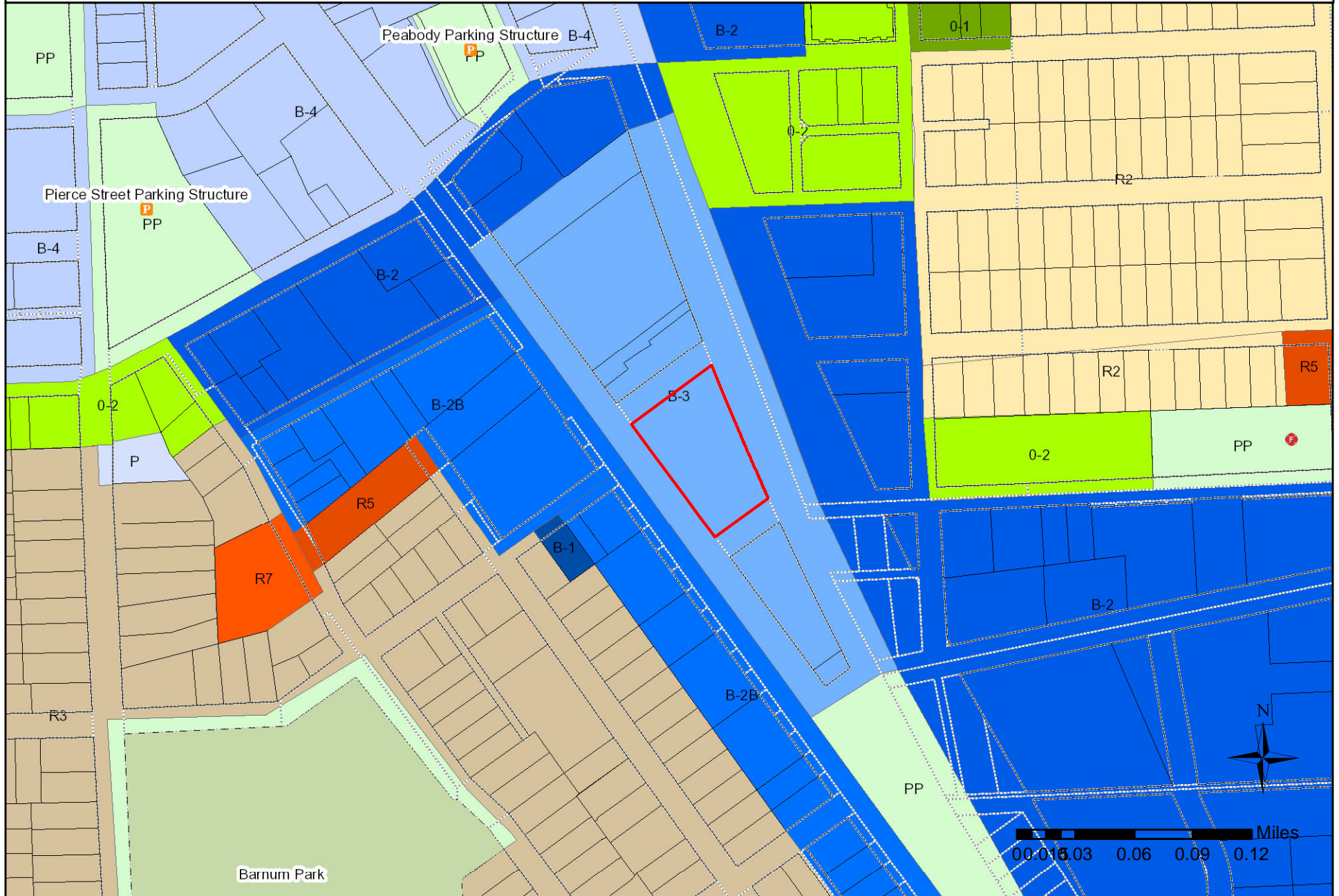


Webster St









### **SPECIFIC PROJECT 7: PIERCE STREET GARAGE**

**Finding:** The Pierce Street Garage creates awkward, under-utilized residual spaces.

**Discussion:** Two of the residual spaces around the Pierce Street Garage are landscaped as mini-parks, which are redundant given the proximity of Shain Park. A third residual space is an unnecessarily large and duplicative access driveway system. Its three existing driveways could be consolidated into a single system passing underneath a new building. Each of the three residual spaces is large enough to contain an infill building (contiguous with the deck's walls), with first-floor retail and upper-floor apartments.

**Recommendation:** Sell or lease these three valuable parcels of urban land for development, thereby masking the deck and completing a retail loop. This specific project could create an ongoing source of revenue for the City.

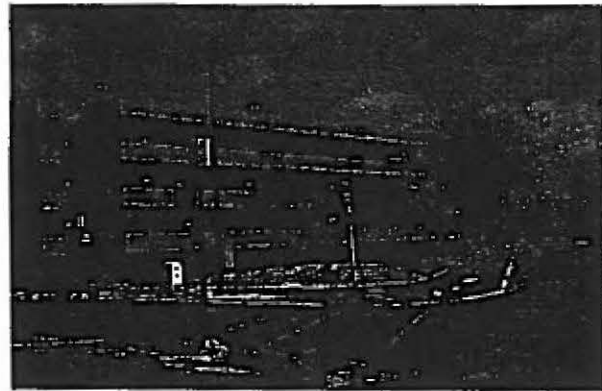
**References:** This has never been done as a redevelopment project before.

- Appendices G – 1 and G – 8.
- Illus. 57, 58, and 59.

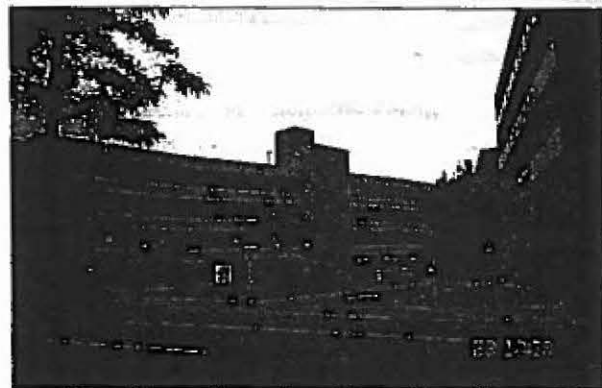
### **SPECIFIC PROJECT 8: MAPLE GATEWAY**

**Finding:** One of the main entrances to Birmingham's CBD is on Maple Road and Hunter Boulevard, which is currently flanked by two gasoline stations.

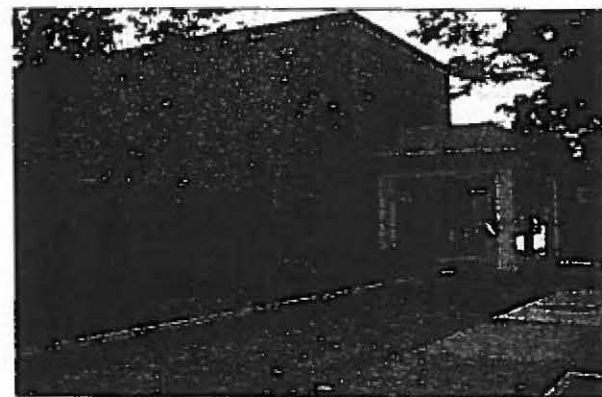
**Discussion:** As a site for a more urban building, the lot north of Maple is too small to contain its own on-site parking, but the Park Street Garage is near enough to fulfill the need. The site to the south is substantially larger. It is adequate, not only for a habitable building, but for a substantial parking deck. The portions of these sites' buildings which front Maple as a pair could form a significant gateway to downtown. Each building should be designed with reference to the other: they should share a similar height, massing and, as much as possible, architectural syntax.



*Illus. 57. Residual areas around the Pierce Street Garage are opportunities for installing liner buildings.*



*Illus. 58. There are gaps around the Pierce Street garage that commend themselves as excellent building sites.*



*Illus. 59. This type of glass storefront may be used to mask the Pierce Street Garage, although a multi-story mixed-use building would do better.*



**Recommendation:** The City should attempt to secure and hold the half-block circumscribed by Maple, Brownell, and Hunter, because it is the last block capable of containing a substantial parking deck for downtown expansion. This block and the block to the north (across Maple) should be carefully scrutinized at the time of their development. The City should encourage these developments to have reciprocal buildings, capable of forming a gateway to the CBD.

**References:** The procedures used to implement the previous generation of parking decks may be dusted off and analyzed for continued applicability.

Concerning the twin buildings proposed: they are so rare in the United States that, if Birmingham were to conjure up a pair like the ones illustrated, they may well become a regional or even a national landmark.

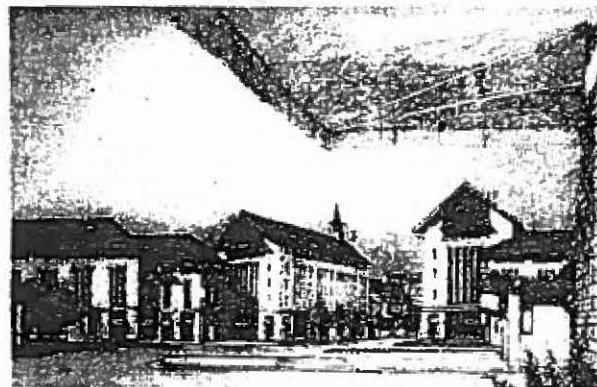
- Appendices G - 1, G - 9, G - 10, and G - 11.
- Illus. 60 and 61.

### **GENERAL AREA 1: EAST MAPLE**

East Maple Road between Adams and Hunter is currently a motley thoroughfare, but has the potential of becoming a respectable commercial area. Now in transition, it has automotive businesses (gas station, car rental agency), outdated commercial buildings (Nos. 745, 690, 700, 746, 1025, and 975), houses halfheartedly converted to commercial use (Nos. 772, 887, and 915), and a few new, handsome, well-landscaped buildings (The Fidelity Bank, Hamilton Funeral Home, and The Eccentric Building). As can be expected from such variety, the existing front-ages differ to the point of urban incoherence. They range from sidewalk build-to lines (about 40 percent) to landscaped front yards (about 20 percent) to strip-style parking lots (about 40 percent). This random, unpredictable mixture fails to create an aesthetic approach to downtown Birmingham, nor does it sustain its own commercial viability. Redevelopment is further complicated by the fact that the lots vary in depth and thus in parking capacity, and by the proximity of small houses at the rear of some lots. In the context of a 20-year Master Plan,

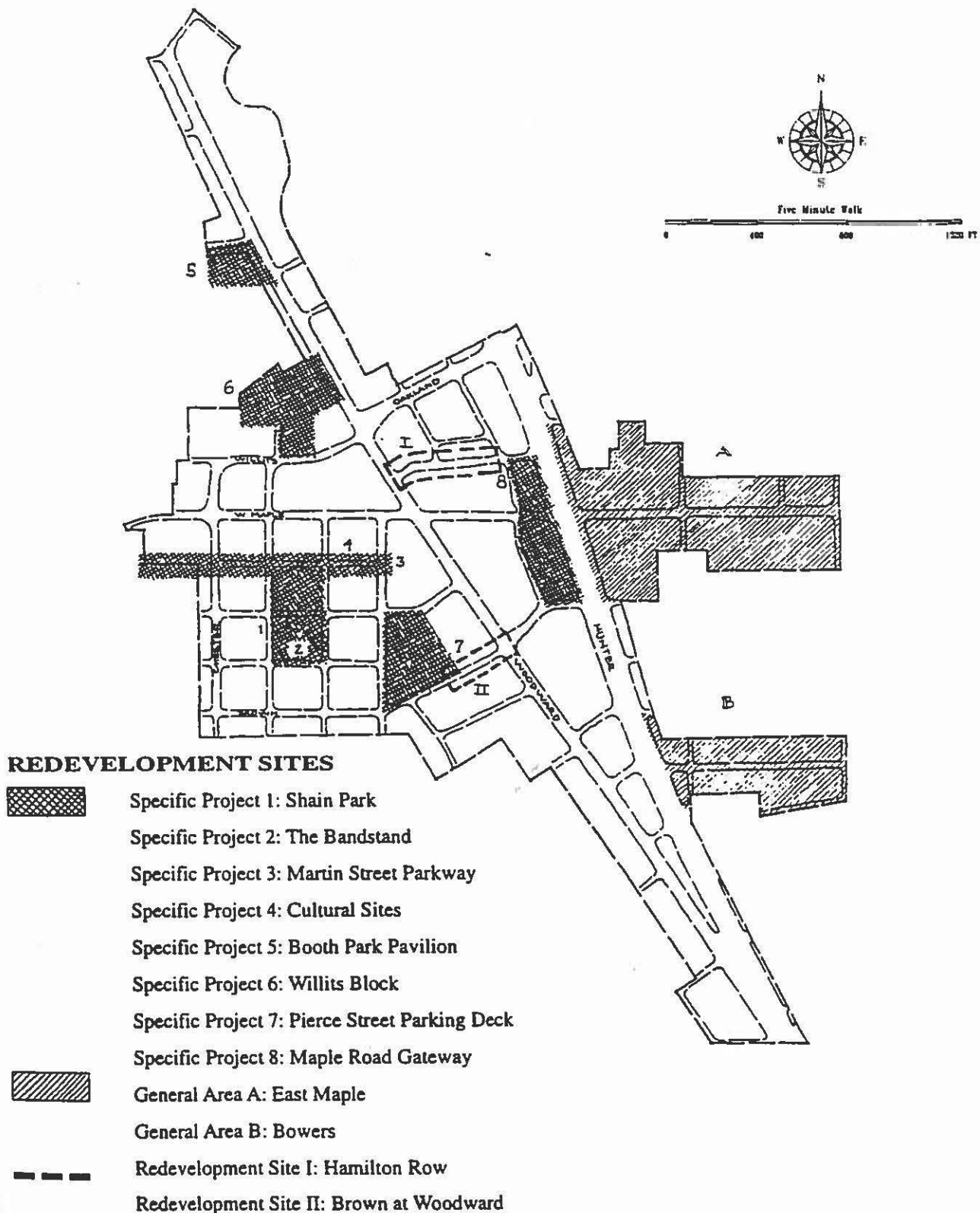


*Illus. 60. The current Maple gateway to the CBD is a pair of gasoline stations.*

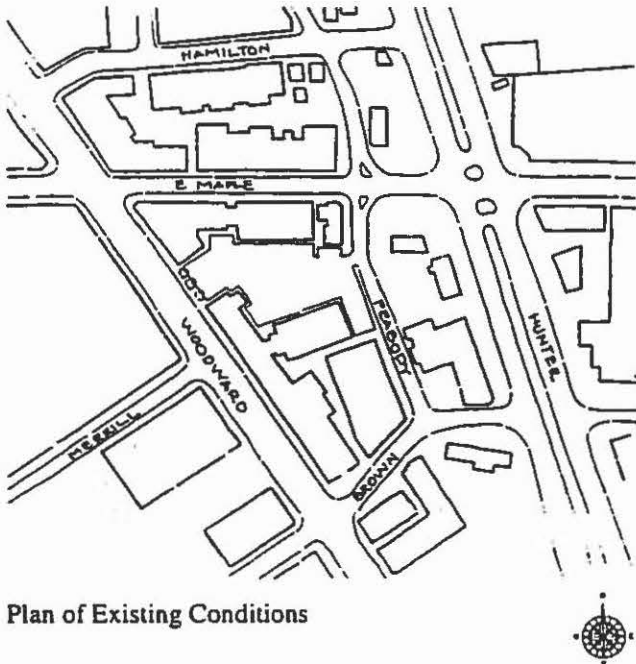


*Illus. 61. This pair of buildings replaces the pair of gasoline stations at Maple Road.*

**SPECIFIC PROJECTS 1 TO 8 AND GENERAL AREAS 1 & 2**

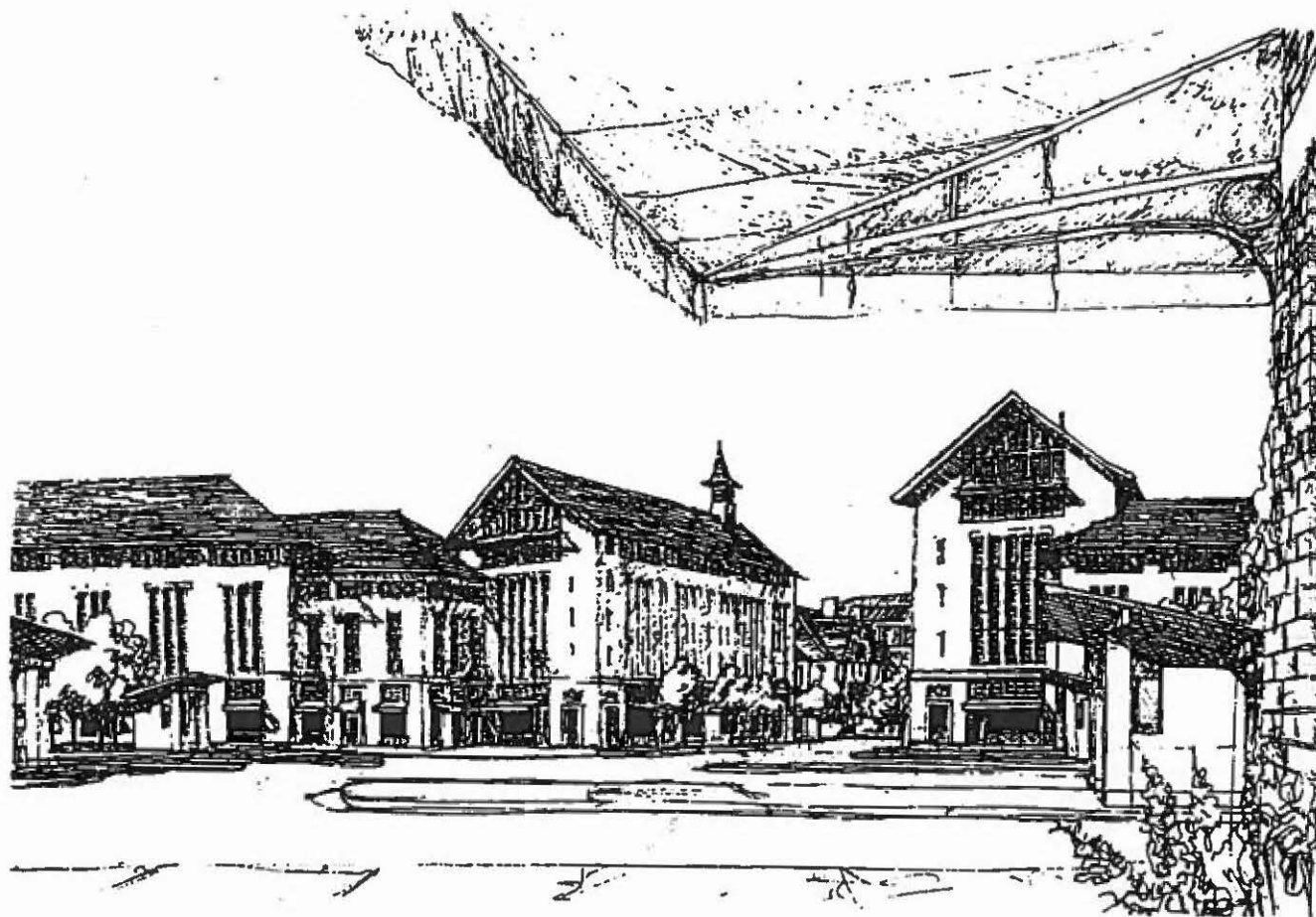


**SPECIFIC PROJECT 8: MAPLE ROAD GATEWAY**



Parking Deck

Mixed-Use Liner Building



**VIEW OF THE EAST MAPLE GATEWAY LOOKING WEST FROM THE KROGER SITE**



## A Vision for the Triangle

Imagine the Triangle District as a vibrant, mixed-use neighborhood of homes, shops, restaurants, offices and public plazas. There is a mixture of housing ranging from single family homes along tree-lined streets, to brownstones and townhomes along local streets, to apartments and condominiums above offices and storefronts on the primary commercial corridors. The centerpiece of the Triangle is Worth Plaza, south of Bowers Street. As a lively triangle-shaped place it is a metaphor for the District as a whole, lined with shops, residences, and sidewalk dining.

The Triangle District is a walkable neighborhood. It features wide, tree-lined sidewalks along comfortable streets that are safe for pedestrians and bicyclists as well as automobiles. Roadways are designed so traffic flows calmly through the District. Narrow streets are lined with pedestrian-oriented buildings that reveal plazas filled with gathering spaces, greenery and public art.

Instead of acting as a barrier, Woodward Avenue is a grand, tree-lined boulevard, lined with distinctive buildings and a streetscape that welcomes both vehicles and pedestrians. Rather than a hard edge that divides the Triangle from downtown, Woodward is the spine that joins the City together.

The Triangle District is a stage for bold and distinctive architecture that creates a unique identity for the neighborhood and City. Building masses are the primary features, replacing the bleak parking lots that currently dominate the landscape. To accommodate the increase in activity, inefficient surface parking will be replaced by well-organized parking structures integrated into the streetscape.

This vision for the Triangle District creates a vibrant, mixed-use neighborhood filled with interesting destinations that attract people from across the region and provide Birmingham residents with an integrated neighborhood in which to live, work, shop and recreate.



View south down Woodward from Maple



Overview of Triangle District

## Goals and Objectives

An analysis of conditions and goals of the community was conducted through a two-day intensive design charrette, with acknowledgement to existing City plans (see sidebar). The process involved the Planning Board, City staff, Triangle District business and property owners, residents and the general public in a public forum that included a walking tour of the District, one-on-one and group interviews, and topic-specific focus groups. The outcome was a set of policy objectives and physical plan concepts to guide public and private decision-making in the Triangle District as follows:

- Improve the visual appearance of the area, its streets, alleys, public spaces, and buildings by establishing guidelines for design and implementation of public and private projects.
- Improve the economic and social vitality by encouraging diversity of use and opportunities for a variety of experiences.
- Better utilize property through more compact, mixed-use development.
- **Link with Downtown across Woodward's high traffic barrier.**
- Improve the comfort, convenience, safety, and enjoyment of the pedestrian environment by create an inviting, walkable, pedestrian neighborhood and setting aside public plazas.
- Organize the parking and street system to facilitate efficient access, circulation, and parking to balance vehicular and pedestrian needs.
- Encourage sustainable development.
- Protect the integrity of established residential neighborhoods.

This plan is intended to provide a general framework for the redevelopment of the Triangle District. While some of the plan graphics show specific road alignments and development scenarios, these are illustrative of desired development form. The plan should be considered flexible in its implementation to reflect and respond to site-specific conditions and opportunities on a case-by-case basis.



Charrette Participants

The goals and objectives of this plan were developed through a process of public participation and are built upon the goals and objectives of the following preceding plans:

- General Village Plan (1929)
- Birmingham Design Plan (1963)
- Urban Design Plan (1993)
- Downtown Birmingham 2016 Plan (1996)
- Eton Road Corridor Plan (1999)







Triangle District Urban Design Plan

## Development Plan Summary

Infill development and redevelopment is recommended to create a distinct character for the Triangle District while complementing the Downtown and surrounding neighborhoods. Redevelopment of the Triangle should create an urban environment that is inviting and walkable. There should be mixed-uses within buildings to create a strong synergy of multiple uses with 24-hour/7-day-a-week activity.

The area should become a self-sufficient neighborhood with mutually supportive residential and commercial uses. While commercial uses along Woodward Avenue could be more general, community service, commercial uses in the heart of the Triangle and along Adams should be oriented more towards serving the immediate neighborhood. Residences and offices should be located in the upper floors above the shops and offices at street-level. Attached single-family, live-work, and other residential uses should also comprise a portion of street-level uses, especially along Elm Street and adjacent to existing single family residences. First-floor retail, especially restaurants, bistros, and cafés, should be encouraged but not required in the heart of the District.

**Building Design and Placement.** Buildings should be designed in a contemporary style and oriented toward their primary street. Designs should incorporate sustainable building elements for the site and the structures. Scale, and size should be compatible with adjacent structures, and facades and rooflines should vary to create relief from continuous surfaces. Pedestrian friendly features should be incorporated.

**Building Height.** Varied building heights are recommended to properly frame the streets and provide the massing necessary to relate to the scale of the streetscapes. The hierarchy of height ranges from taller mixed-use buildings along Woodward Avenue that are seven stories and higher, medium height mixed-use buildings of 4-5 stories in the District's interior

and along Maple to create a more intimate urban neighborhood, and structures at a smaller scale of three stories when abutting existing residential neighborhoods. Buildings should step back from the street at the higher stories.

**Public Open Space.** Opportunities are created for integrating public plazas and open space as part of any redevelopment. This includes small plazas on individual sites and larger open spaces that serve as neighborhood focal points. Recommended realignment of Worth Street creates the opportunity for a triangular plaza, referred to as “Worth Plaza,” as the primary focal point for the redevelopment of the Triangle.

**Identity and Wayfinding.** Architectural designs will differentiate the Triangle from the rest of the City. A coordinated system of public and private signs will uniquely identify and direct visitors around the District. Signs will complement the City’s established Signage and Wayfinding Program.

**Circulation.** Improvements to streets and intersections highlighted in this plan will help to reduce speeds on local streets, improve safety for vehicles and pedestrians, and ensure proper access to residences and business.

**Parking.** Parking needs to be provided more efficiently than the current configuration of disjointed surface parking lots. Redevelopment should incorporate multi-level parking structures and maximize the use of on-street parking. More efficient use of shared parking facilities will allow for redevelopment that is more pedestrian oriented and less dominated by parking lots.

The development plan is a long-term vision for the Triangle District; the pace and order of which is dependent on a variety of factors. To facilitate the orderly and successful implementation of the plan, a phasing plan has been developed. (See the Implementation section.)

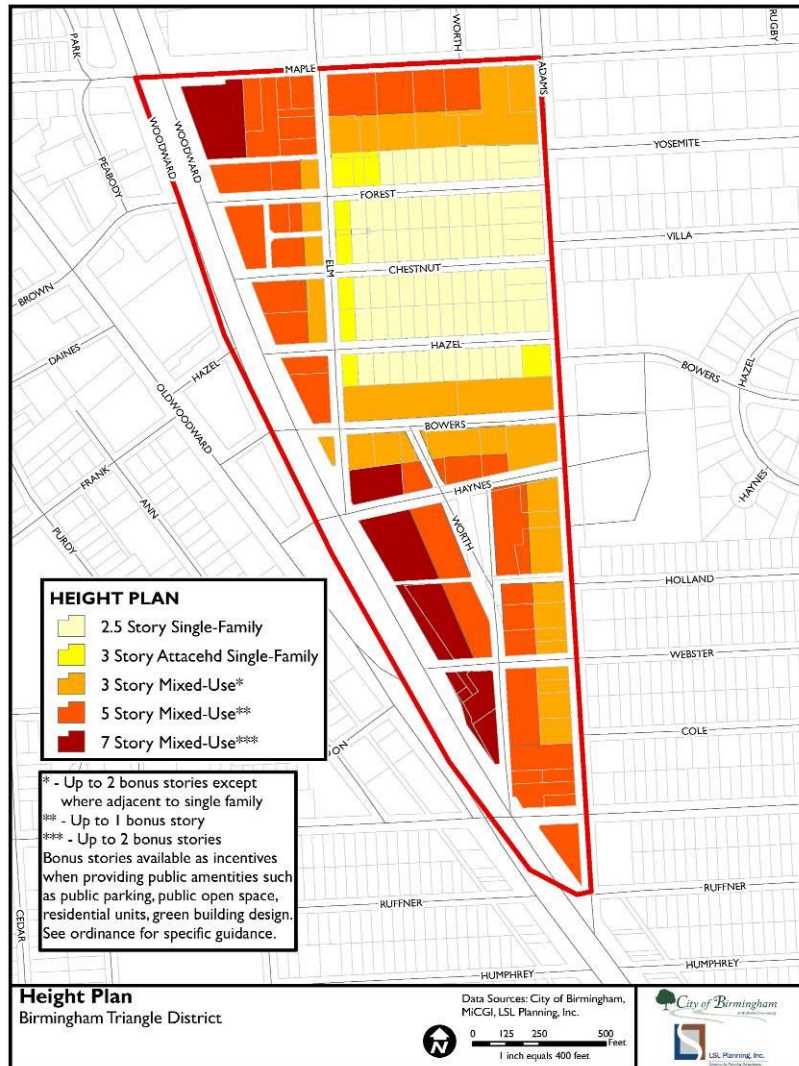


Sample Building Design



Sample Townhouse District





Triangle District Height Plan

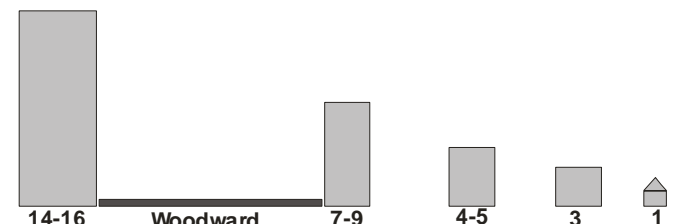
## Building Heights

A hierarchy of heights is recommended between Woodward Avenue and the adjacent single-family residential neighborhoods. Taller buildings at least seven stories are needed to properly define the scale of Woodward Avenue's wide right-of-way and the taller buildings on the west side of the road. Building height should then step down to 4-5 stories in the interior of the Triangle District along the narrower streets. Buildings adjacent to single-family residential neighborhoods should be limited to three stories.

Height bonuses of up to an additional two stories will be allowed for developments that offer certain public amenities. These could include making public parking available in private parking structures, providing public open spaces, improvements to the public streetscape or incorporating energy-efficient green building design into structures. Payments to an escrow account designated for off-site amenities should be accepted in lieu of providing them.

New construction should create architectural variety by stepping back upper floors and varying the massing of buildings. Taller building should also be setback from nearby residential neighborhoods.

In order for the Triangle District to efficiently redevelop, parking will need to be provided with multi-level parking structures. The largest public parking structure will be required in the vicinity of Worth Plaza and should be located between the plaza and Woodward to take advantage of the highest allowable heights and best access.



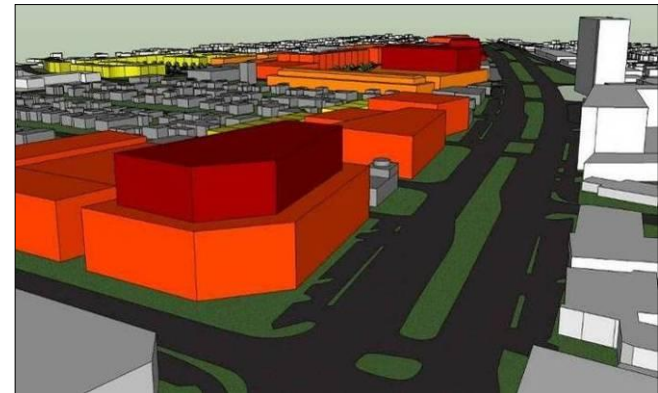
Conceptual Height Cross-Section

## Height Defines Streetscape

Recommended building heights will help to define streetscapes and create a strong sense of enclosure. This enclosure is a vital component to creating a more human-scale environment that is inviting to pedestrians and induces automobile traffic to slow down.

Currently, automobiles dominate Woodward Avenue, with its wide right-of-way of approximately 200 feet. This vast expanse of highway is open and uninviting to the pedestrian. The buildings on the west side of Woodward are taller, with the tallest being the 555 building at 15 stories. The plan recommends taller buildings on the east side of Woodward Avenue to create a better sense of enclosure. Buildings should range from between five and nine stories. With the tallest buildings ranging in height between 90 and 114 feet, this is half the distance across Woodward Avenue, which is an appropriate scale to create the desired sense of enclosure.

With the tallest buildings along Woodward Avenue, the heights will transition down to a level more compatible with the single-family residential neighborhoods and more appropriate to create the desired sense of enclosure for the narrower rights-of-way of the Triangle District's internal streets. In most cases, buildings in the interior should range between three and five stories. Those buildings within a minimum distance to existing single-family residential homes are limited to three stories. Shorter building heights are appropriate to frame the smaller scale of single-family residential streets.



Height/Massing Model – South down Woodward



Height/Massing Model – North down Woodward/Adams



Height/Massing Model – West down Bowers

## Changing the Conversation: **from Building Heights to Place Making:**

- *Walter Chambers*

Discussions about building height limits often turn into a discussion about “human scale”. As the discussion goes, tall buildings are impersonal. Short buildings are more “human”.

To be clear, this discussion is about the buildings that line our streets, and the experience one has when walking down the street. Although people may not know it, the discussion about building heights is really about the way one **FEELS** when experiencing the street. Everyone wants to feel good on the street -- safe, protected, happy, and engaged. When streets feel good, people like to be on them, and having people on the street makes places feel lively, interesting and safe – and that attracts even more people.

Unfortunately, short buildings are no guarantee that a street will feel good. Neither are tall buildings.

So how do you make a street **FEEL** good? By creating a good **Sense of Place**. Streets feel good when there is a strong Sense of Place.

Streets are like rooms. They have a floor, walls, and ceiling. And like a room, they can feel good or bad, depending on their proportions and detail. Have you ever walked into a banquet hall or room with low, tile ceiling? Feels awful doesn't it?

Or how about being the first one to a wedding reception held in a large hotel ball room. The room looks lovely, but you still feel exposed and awkward until the other guests arrive and start filling the space.

A Street requires the same good proportions as any room to make it feel good. It is the “walls” of the street that are key to creating good proportions and a sense of place. The buildings on either side of the street form the walls of the street “room”, and as such are called the “Street Wall”.

So what makes a good street wall? Several factors go into making a good street wall\*, but for this conversation about building heights, the focus will be on **Height to Width Proportion**, or HWP.

HWP is the ratio of the Height of the street wall, to Width of the street. For example, if the buildings that form the street wall are 30 feet tall, and the street is 60 feet wide, then the HWP is 1:2.  $30:60 = 1:2$ . If the buildings (street wall) are 180 feet tall and your street is 60 feet wide, then the HWP is 3:1.  $180:60 = 3:1$ .

Why does HWP matter? Different HWP ratios invoke different feelings and a different sense of Place. A 3:1 ratio (think major urban downtown) feels different than a 1:4 ratio (think suburban retail strip).



Typically, if an HWP is too low, the street will not have a good sense of place. People will not want to be on that street.. And in urban settings it is people we want to attract. People are the ones who create lively, exciting streets, who fill the sidewalk cafes and stores, and that help trigger economic growth. To quote famous urbanist William H. Whyte, "What attracts people most, it would appear, is other people."

That's why low building heights might work on some streets, but not on all streets. If a community is demanding limits on all building heights in its district, then some streets are being set up for failure. And if limits are excessively low (or too high) then the entire district may be set up for failure.

When the conversation changes from building heights to place making, the chance of creating good urban spaces is greatly enhanced. Good place making also triggers economic growth. Talking solely about building heights is to ignore the environment that surrounds the buildings. It is irresponsible. The following real life case demonstrates how focusing on place making is different (and more important) than focusing on building height limits.

**Case Study:** 301 University – University Avenue @ 3<sup>rd</sup> Avenue.  
The street at University Avenue and 3<sup>rd</sup> is approximately 40-45 feet wide (two lanes wide, with parallel parking on either side). A proposed new 12 story condominium tower met fierce

community opposition, and perhaps with good reason. At a HWP ratio of 3:1, this building begins to create a sense of place that feels very much like a downtown high-rise urban area. That is not in keeping of the character of the neighborhood. Perhaps a better HWP for this area would be 3:2 (mid-rise urban) or 1:1. A 4-5 story building would create an inviting sense of place, and would be a better height in this location.

However, a just few blocks further east, University Avenue widens significantly. At Richmond Street, University Avenue is approximately 90-100 feet wide (four lanes, center median, and parking either side). Would a 4-5 story building create a good sense of place here? Probably not. At this location, the wide street can easily handle an 8-9 story building without the street looking or feeling overwhelmingly urban. In this location, a 3:2 or 1:1 HWP would also create a good sense of place, and would feel most comfortable to the people on the street.

For University Avenue, a single building height limit is not appropriate. What works at 3<sup>rd</sup> Avenue, does not work a few blocks away at Richmond Street. That is the reason building height must be based on Place Making, and not on some arbitrarily assigned number applied over an entire district.

In order to achieve good place making, one must start with good walls that are the right height for the "Room". Below is a sampling of Height to Width Ratios and the sense of place they tend to

create. Many thanks and great appreciation to the St Louis Great Streets Initiative from which the below descriptions have come. I urge you to visit their website and read more: <http://www.greatstreets-stl.org/content/view/417/400/>

### **HWP Raito and Place making**

**3:1** or higher: Height to Width Ratio

Sense of spatial definition: strong; may feel like a “concrete canyon” in some settings.

Often seen in larger downtown, urban cores.

**3:2** Height to Width Ratio

Sense of spatial definition: strong; clear sense of enclosure.

A good HWP for Medium sized urban downtown, or urban core residential

**1:1** Height to Width Ratio

Sense of spatial definition: Excellent. Strong place making potential. May be strongest ratio for good place making. Encouraged minimum for all urban areas, including residential.

**1:2** height to Width Ratio

Sense of spatial definition: Good; Sufficient for place making. Considered a minimum for good urban street place making.

**1:3** or lower

Sense of spatial definition: Weak; Place making potential is low.

This ratio is often seen in suburban areas where wide streets are lined with 1-2 story retail stores or strip malls. No sense of place to the street.

*\*Of course, as mentioned earlier in this article, other factors are essential in creating a good Street wall, and those must be taken into consideration.*

*Elements of a good street wall include:*

- *HWP*
- *Architectural Diversity (old & new, short & tall, frequency of façade changes)*
- *Building should be built to the sidewalk for consistent wall face.*
- *Buildings and the architecture must be engaging and interesting to people at street level and second floor (Including human scale building elements, active engagement such as storefronts or sidewalk cafes, and experience of other people).*
- *Landscaping*





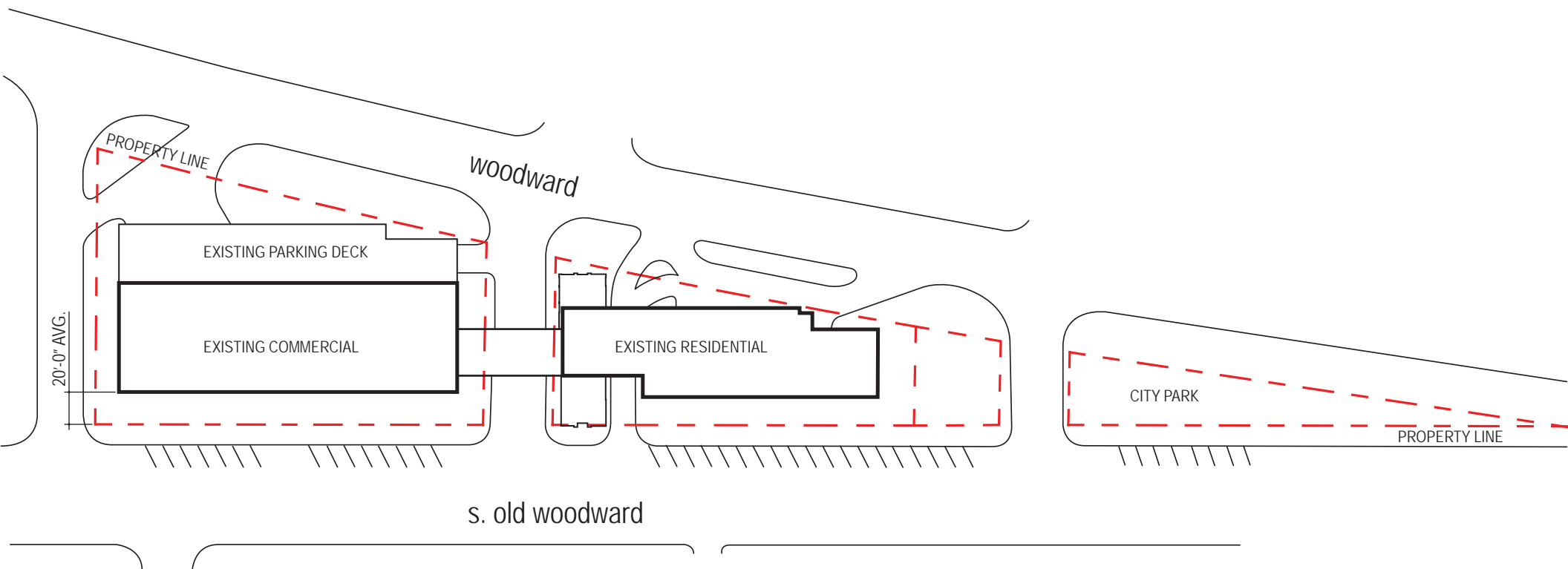
555 S. OLD WOODWARD







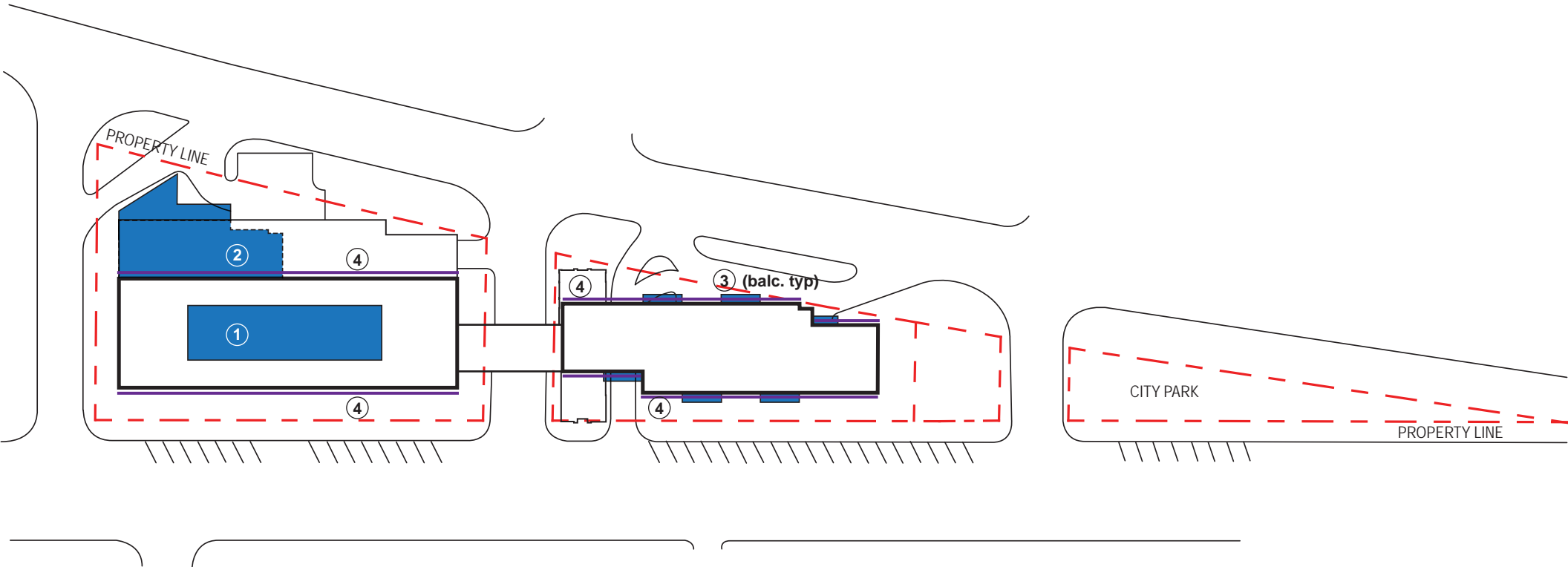
EXISTING





## PHASE I

- 1. Roof-top Restaurant
- 2. Commercial/retail
- 3. New Balconies
- 4. New Curtain Walls



**Administrative Approval Application**  
**Planning Division**

AGENDA

Form will not be processed until it is completely filled out

**1. Applicant**

Name: SCOTT BARBAT  
 Address: 33477 WOODWARD STE 800  
BIRMINGHAM, MI 48009  
 Phone Number: 248-255-3508  
 Fax Number: 248-594-3316  
 Email: SCOTT.BARBAT@GMAIL.COM

**Property Owner**

Name: BS INVESTMENTS, LLC  
- SAME -  
 Address: 33477 WOODWARD STE 800  
BIRMINGHAM, MI 48009  
 Phone Number: 248-255-3508  
 Fax Number: 248-255-3316  
 Email: SCOTT.BARBAT@GMAIL.COM

**2. Applicant's Attorney/Contact Person**

Name: - SAME -  
 Address: \_\_\_\_\_  
 Phone Number: \_\_\_\_\_  
 Fax Number: \_\_\_\_\_  
 Email: \_\_\_\_\_

**Project Designer**

Name: POW ANDERMAN  
 Address: \_\_\_\_\_  
 Phone Number: \_\_\_\_\_  
 Fax Number: \_\_\_\_\_  
 Email: \_\_\_\_\_

**3. Project Information**

Address/Location of Property: 33588 WOODWARD  
 Name of Development: SHELL/DUNKIN  
 Parcel ID #: \_\_\_\_\_  
 Current Use: \_\_\_\_\_  
 Area in Acres: \_\_\_\_\_  
 Current Zoning: \_\_\_\_\_

Name of Historic District site is in, if any: \_\_\_\_\_  
 Date of HDC Approval, if any: \_\_\_\_\_  
 Date of Application for Preliminary Site Plan: \_\_\_\_\_  
 Date of Preliminary Site Plan Approval: \_\_\_\_\_  
 Date of Application for Final Site Plan: \_\_\_\_\_  
 Date of Final Site Plan Approval: \_\_\_\_\_  
 Date of Revised Final Site Plan Approval: \_\_\_\_\_

**4. Attachments**

- Warranty Deed with legal description of property
- Authorization from Owner(s) (if applicant is not owner)
- Completed Checklist
- Material Samples/Specification Sheets
- Digital Copy of plans

- Two (2) folded copies of plans including an itemized list of all changes for which administrative approval is requested, with the changes marked in color on all elevations

**5. Details of the Request for Administrative Approval**

- ① - UNDER DECK OF CANOPY (LOW) ALUMINUM FACIA (NO AZEK).
- ② - CANOPY COLUMNS NOW PAINTED STEEL (NO AZEK).
- ③ - REMOVE WALL ADJACENT TO JIMMY JOHNS.
- ④ - BLOCK PAINT COLOR CHANGE.

The undersigned states the above information is true and correct, and understands that it is the responsibility of the applicant to advise the Planning Division and / or Building Division of any additional changes to the approved site plan.

Signature of Applicant: [Signature] Date: 5-6-15

Application #: <u>AA15-0036</u>	Office Use Only Date Received: <u>5/6/15</u>	<b>DENIED</b> Fee: <u>\$100</u>
Date of Approval: _____	Date of Denial: <u>5/13/15</u>	Reviewed by: <u>M.B.H.</u>

- ⑤ - CANOPY LIGHT FIXTURES TO HANG  $1\frac{3}{8}$ " BELOW DECK RAIS. (SEE CANOPY PRINTS).

AA15-0036  
118151



Project:

SHELL GAS STATION & DUNKIN' DONUTS

Seal:



Issue Date:

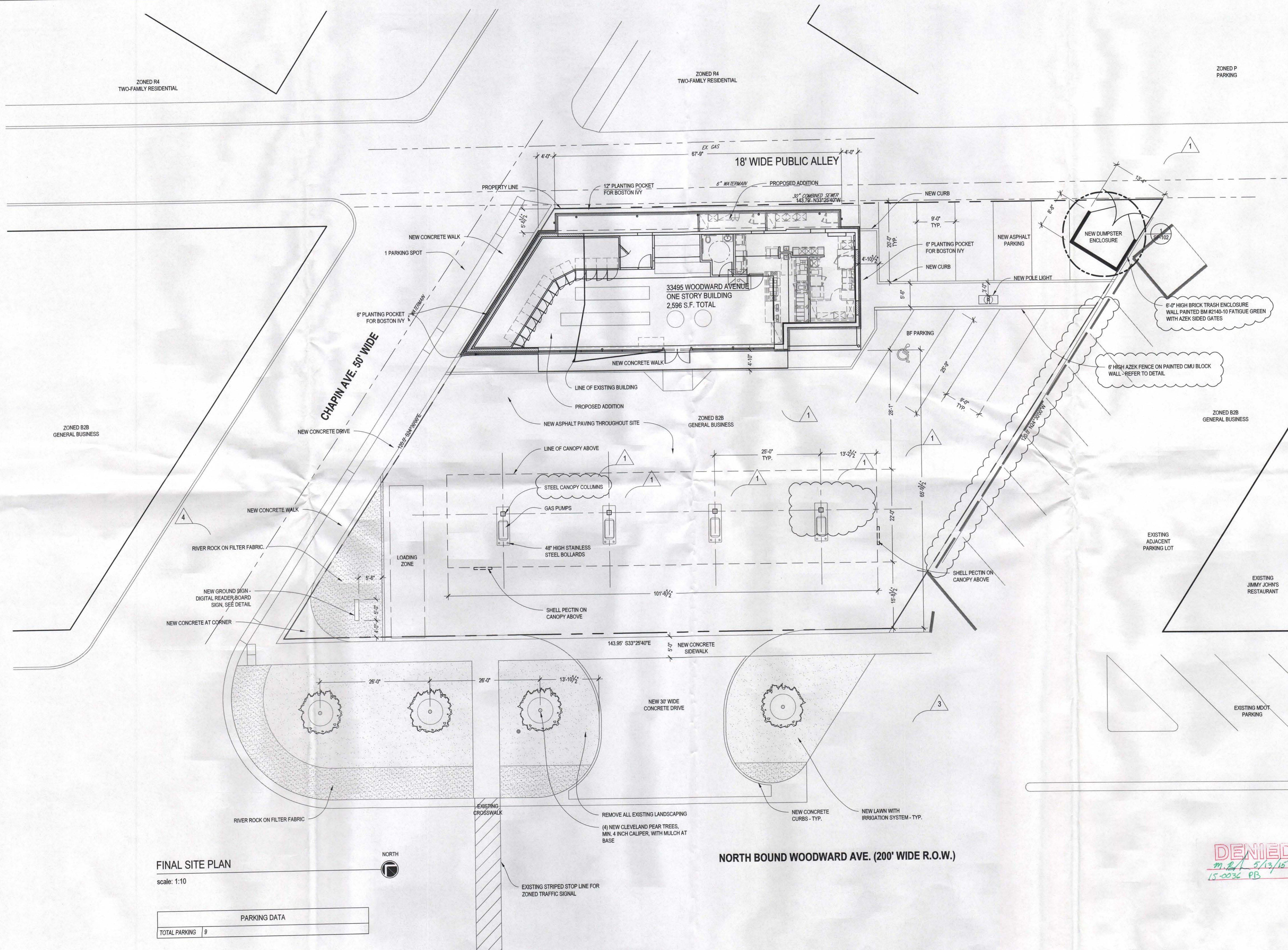
SITE PLAN REVIEW	
REVISION	
FINAL SITE PLAN REVIEW	
BUILDING PERMIT	
REVISION 1	
REVISION 2	
REVISION 3	
REVISION 4	
LATEST	

Sheet Title:

SITE PL

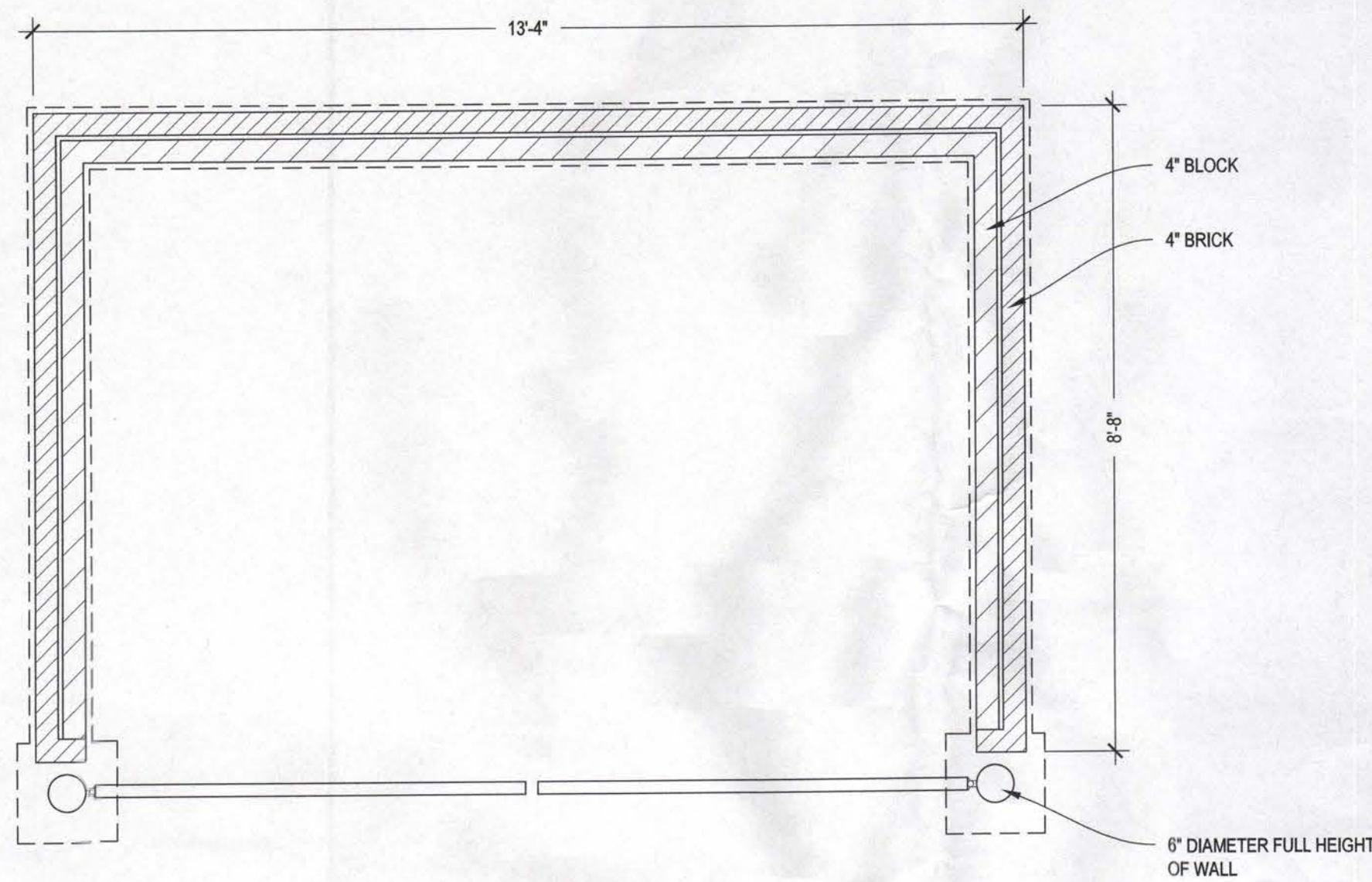
Sheet Number:

SP10

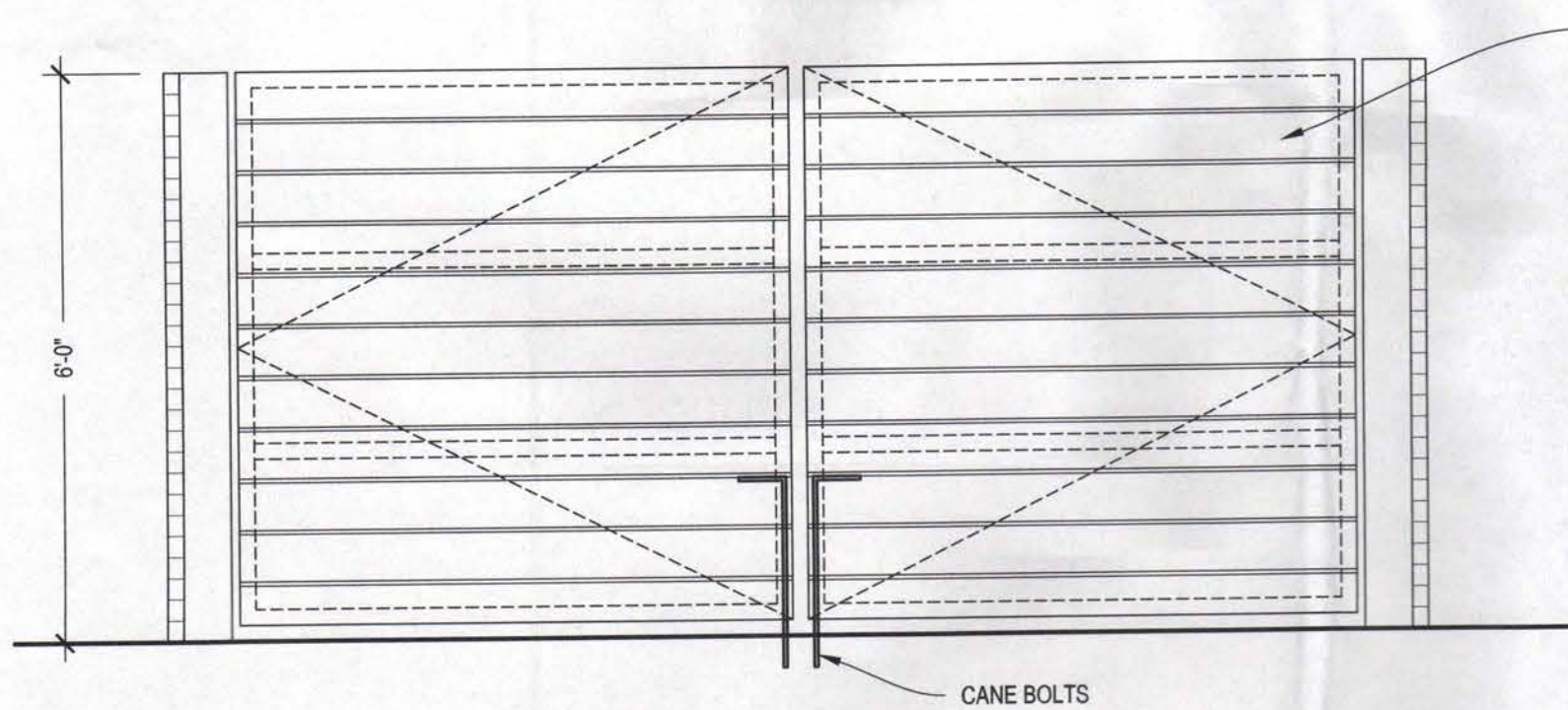


**DENIED**  
m.r. 5/13/15  
15-0036 PB

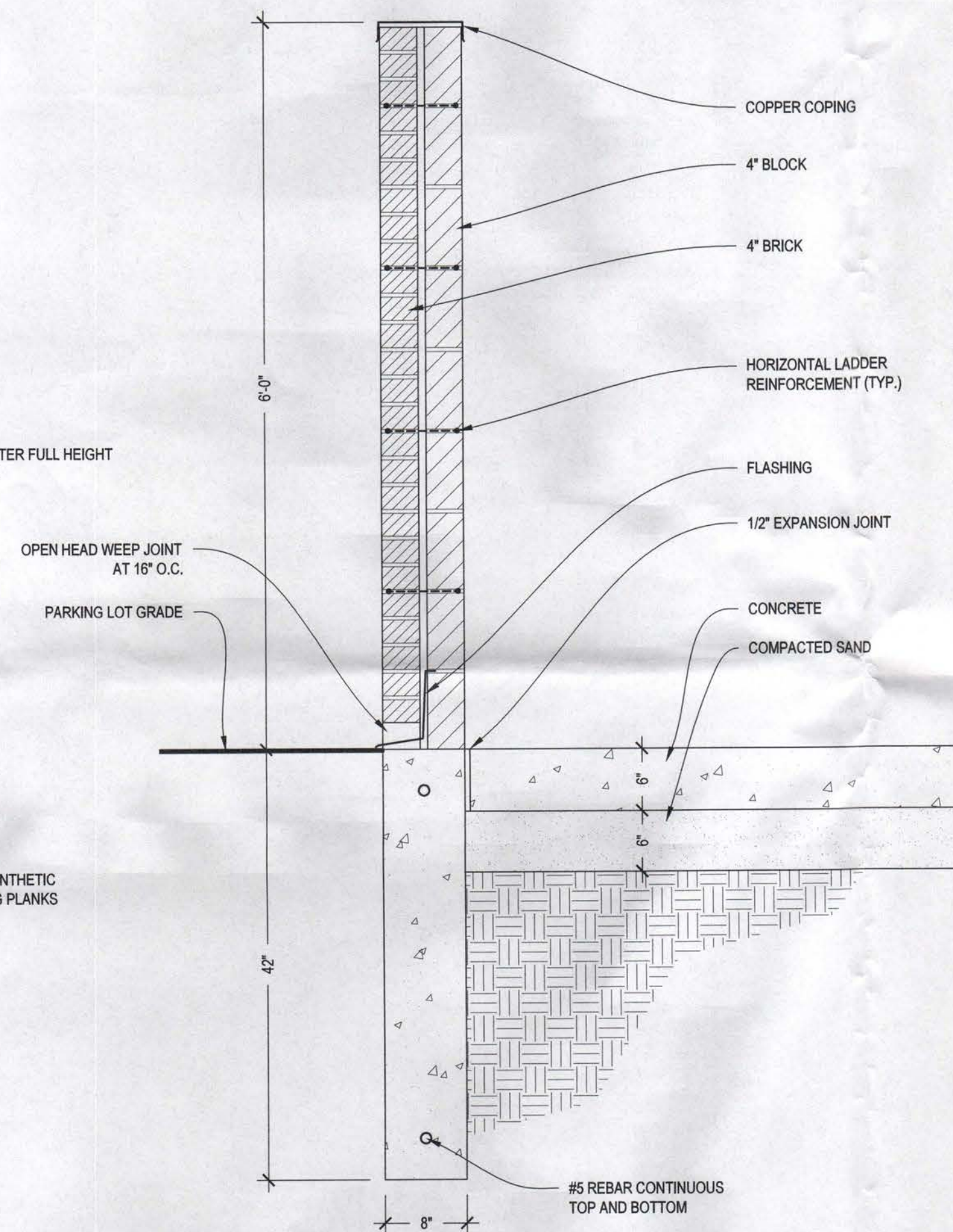




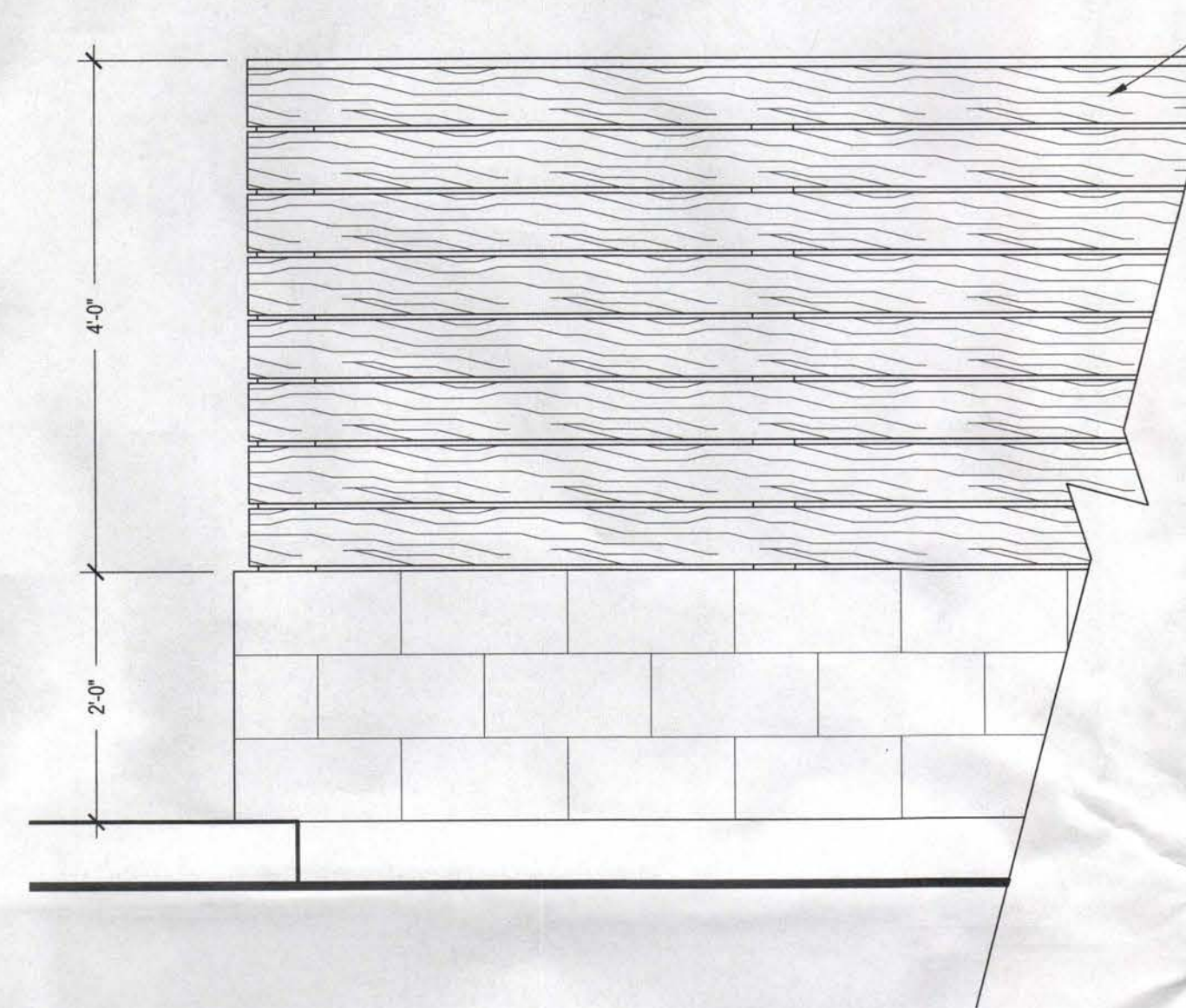
1 Dumpster Enclosure Plan  
scale: 1/2" = 1'-0"



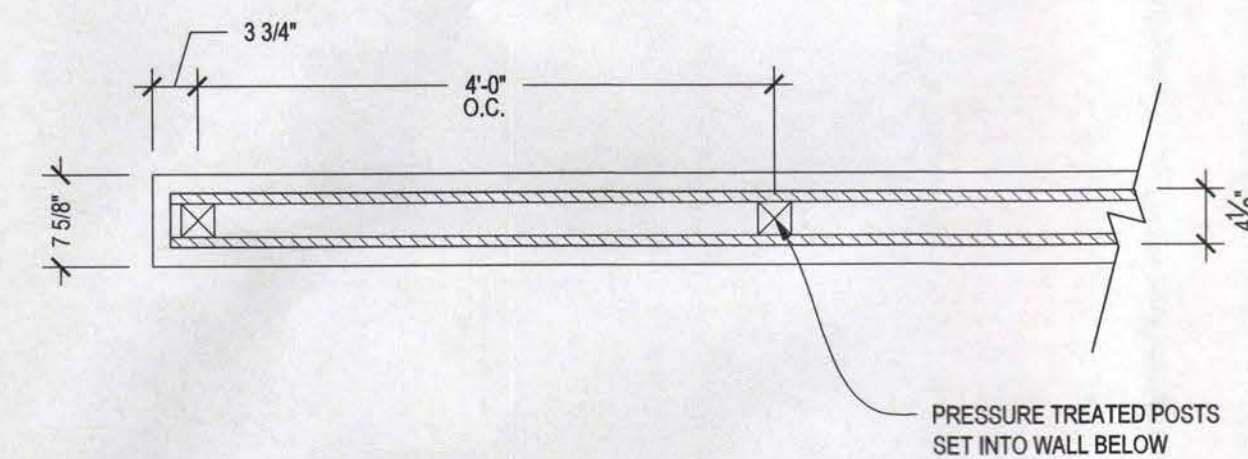
2 Dumpster Enclosure Elevation  
scale: 1/2" = 1'-0"



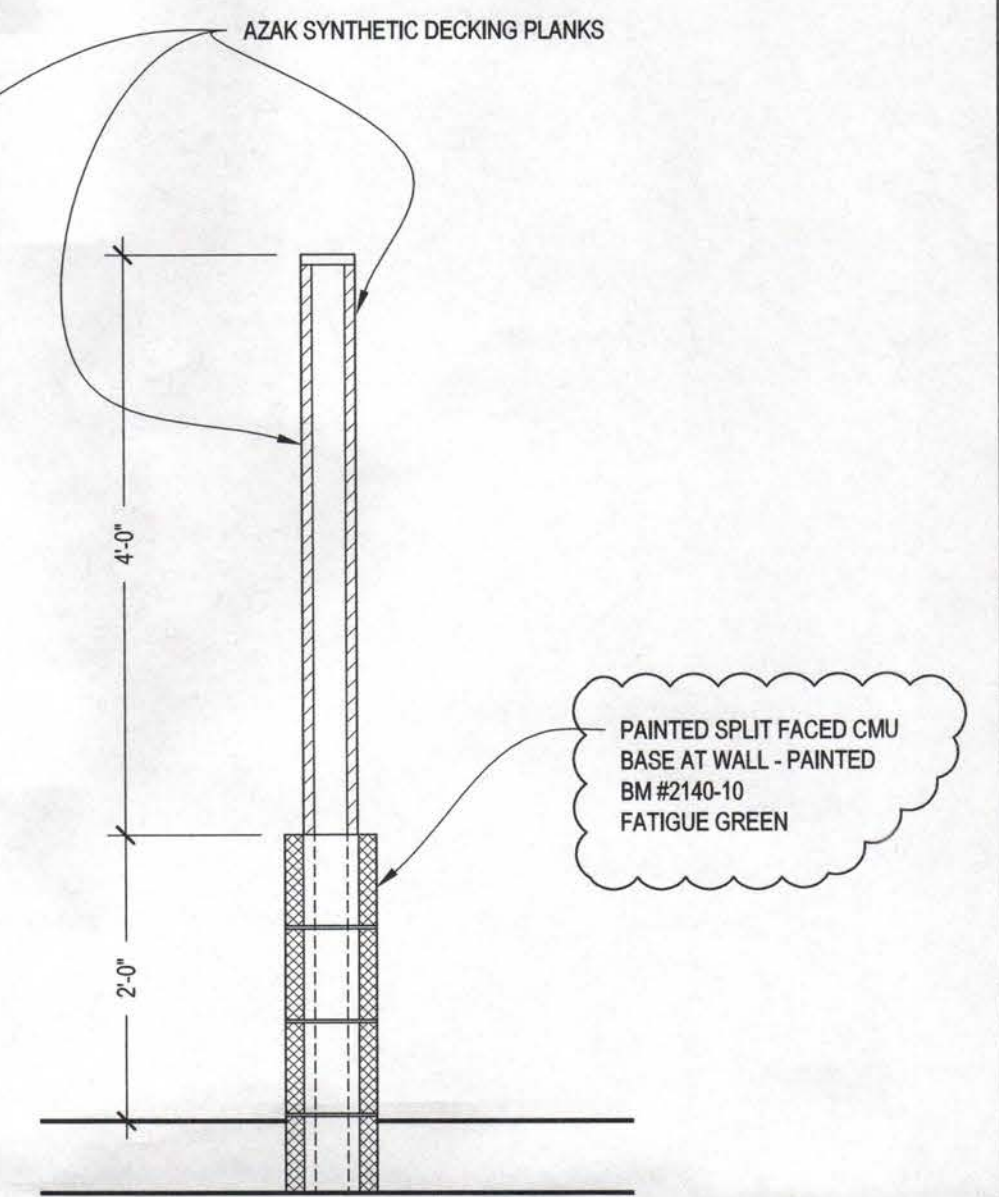
3 Dumpster Enclosure Section  
scale: 1" = 1'-0"



4 Side View at Fence  
scale: 3/4" = 1'-0"



5 Fence Plan Detail  
scale: 3/4" = 1'-0"



6 Section Thru Fence  
scale: 3/4" = 1'-0"

Project:

SHELL GAS STATION & DUNKIN' DONUTS  
NEW BUILDING  
33568 Woodward Avenue Birmingham MI 48009

Seal:



Issue Date:

SITE PLAN REVIEW	
REVISION	
FINAL SITE PLAN REVIEW	
BUILDING PERMIT	1
REVISION 1	2
REVISION 2	3
REVISION 3	3
REVISION 4	3
LATEST	

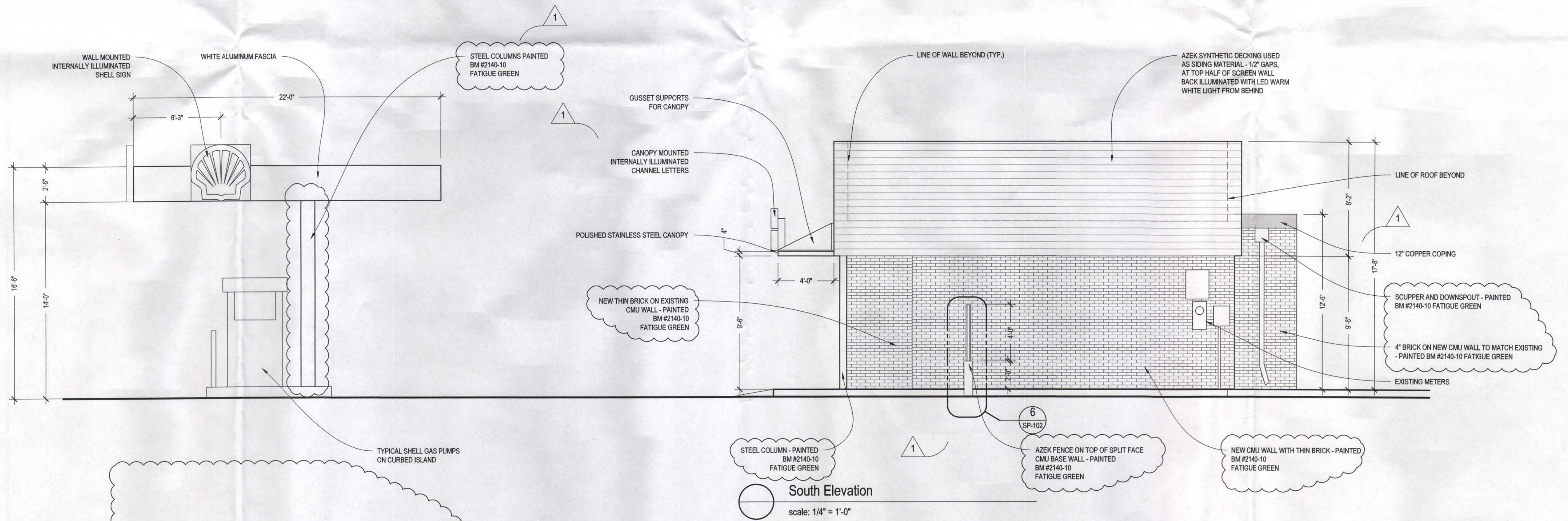
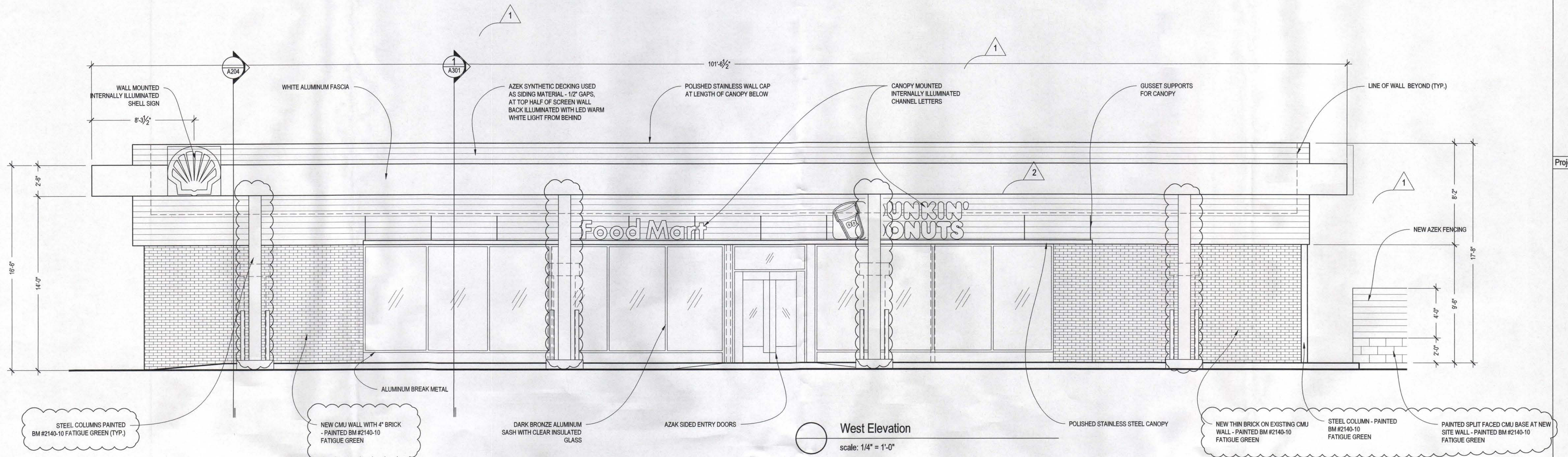
Sheet Title:

SITE PLAN DE

Sheet Number:

SP102





Project:

**SHELL GAS STATION & DUNKIN' DONUTS**  
NEW BUILDING  
33588 Woodward Avenue, Birmingham, MI 48009

Seal:



Issue Date:

SITE PLAN REVIEW	6/1/2024
REVISION	8/1/2024
FINAL SITE PLAN REVIEW	8/1/2024
BUILDING PERMIT	12/1/2024
REVISION 1	2/1/2025
REVISION 2	3/1/2025
REVISION 3	3/1/2025
REVISION 4	3/1/2025
LATEST	5/1/2025

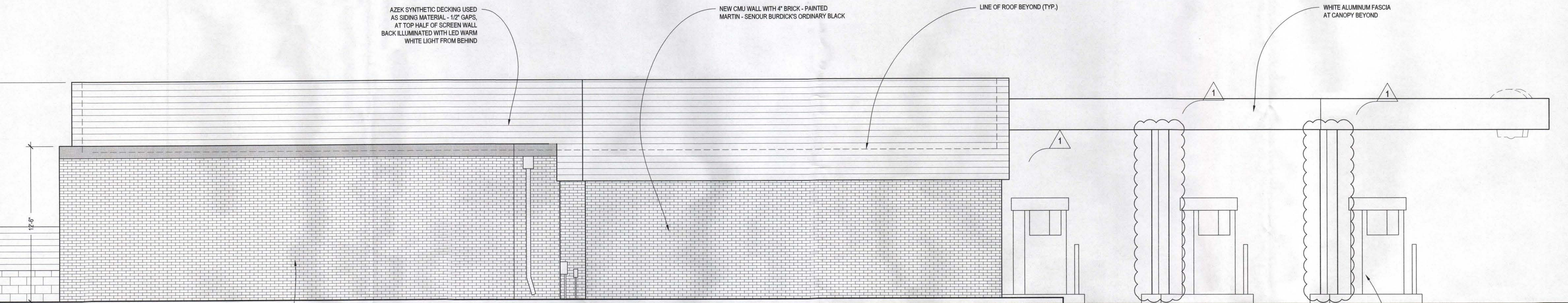
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EXTERIOR  
ELEVATION

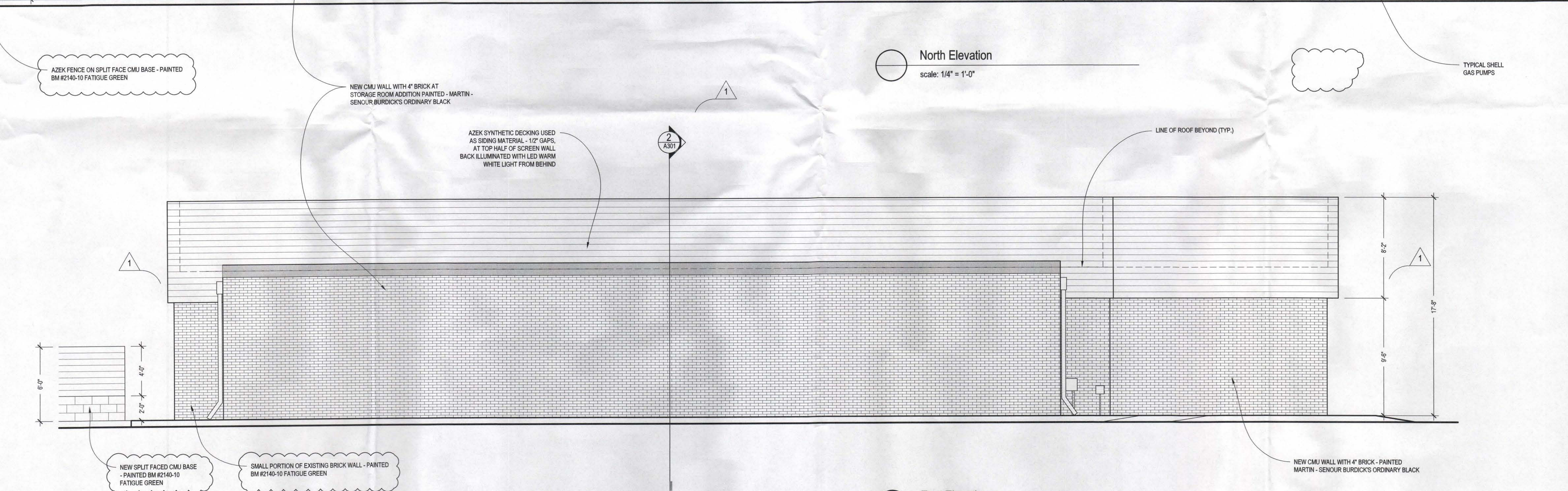
Sheet Number:

A201





North Elevation  
scale: 1/4" = 1'-0"



East Elevation  
scale: 1/4" = 1'-0"

Project:

SHELL GAS STATION & DUNKIN' DONUTS  
NEW BUILDING  
33568 Woodward Avenue, Birmingham, MI 48009

Seal:



Issue Date:

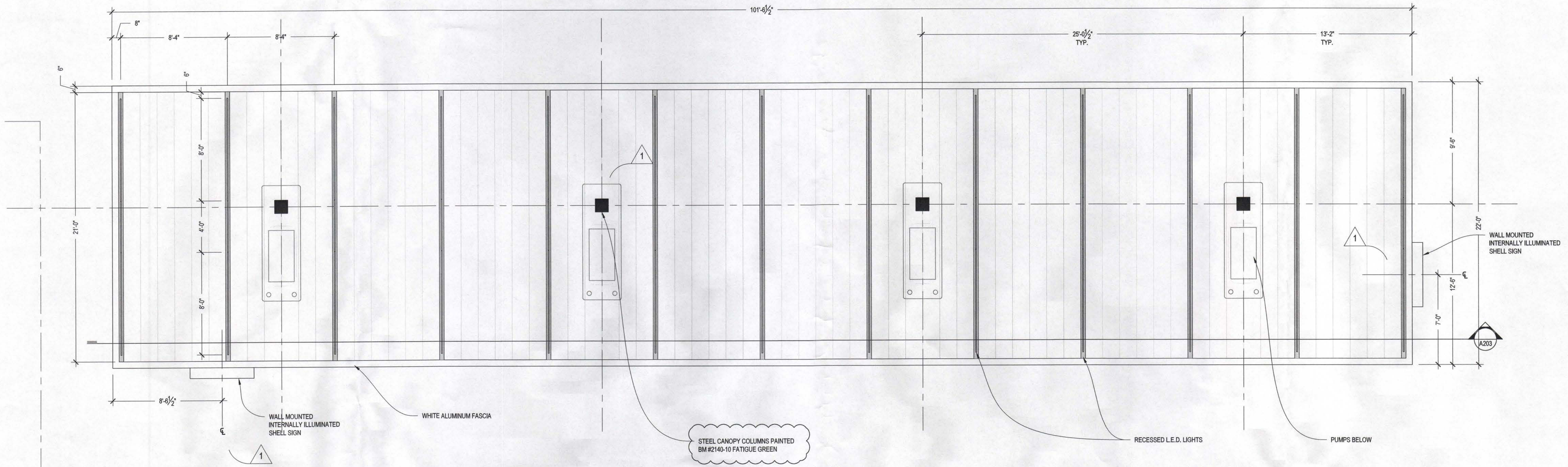
SITE PLAN REVIEW	8
REVISION	8
FINAL SITE PLAN REVIEW	8
BUILDING PERMIT	12
REVISION 1	2
REVISION 2	3
REVISION 3	3
REVISION 4	3
LATEST	5

Sheet Title:

EXTERIOR  
ELEVATION

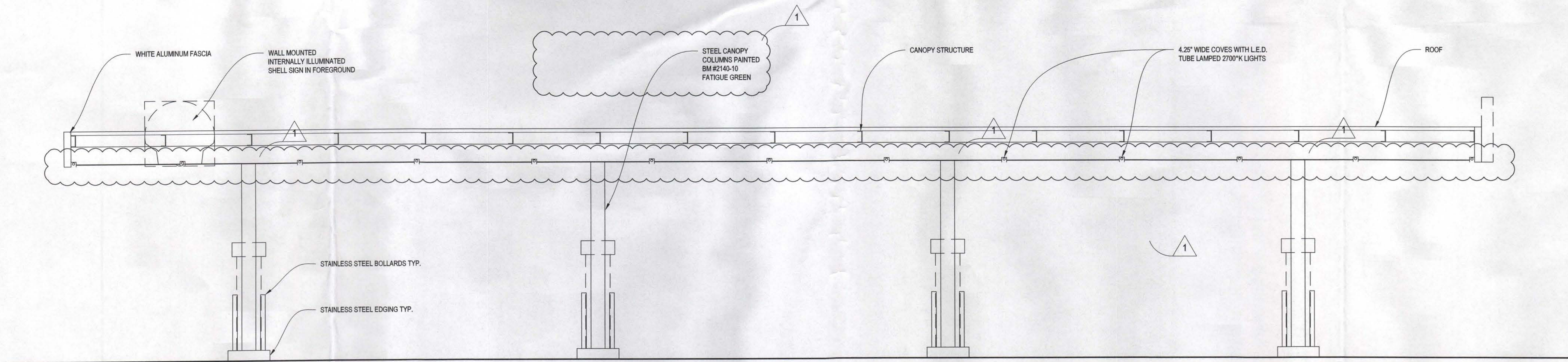
Sheet Number:





Reflected Ceiling Plan at Canopy

scale: 1/4" = 1'-0"



Canopy Section

scale: 1/4" = 1'-0"

Project:

SHELL GAS STATION & DUNKIN' DONUTS  
NEW BUILDING  
33588 Woodward Avenue, Birmingham, MI 48009

Seal:



Issue Date:

SITE PLAN REVIEW	8
REVISION	8
FINAL SITE PLAN REVIEW	8
BUILDING PERMIT	12
REVISION 1	2
REVISION 2	3
REVISION 3	3
REVISION 4	3
LATEST	5

Sheet Title:

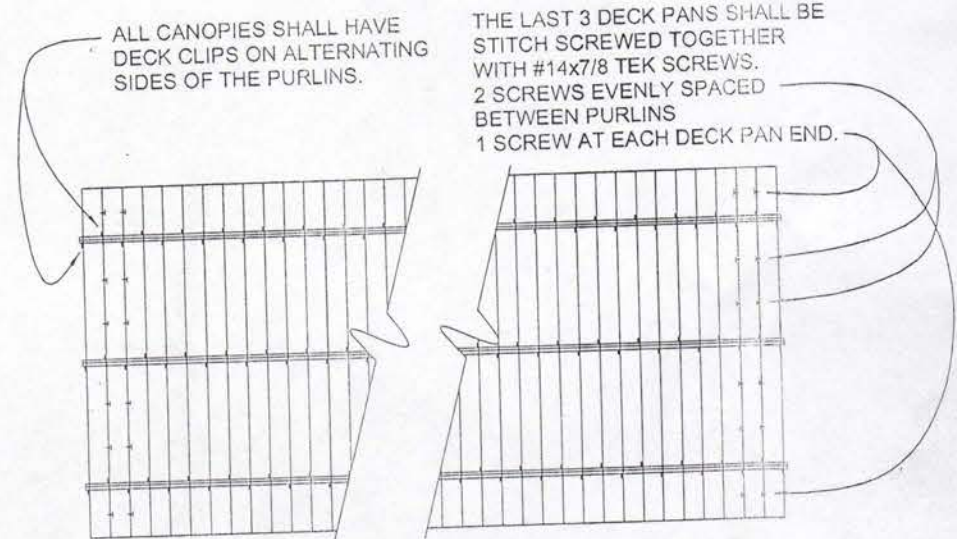
REFLECTED CEILING  
PLAN AND CANOPY  
SECTION

Sheet Number:

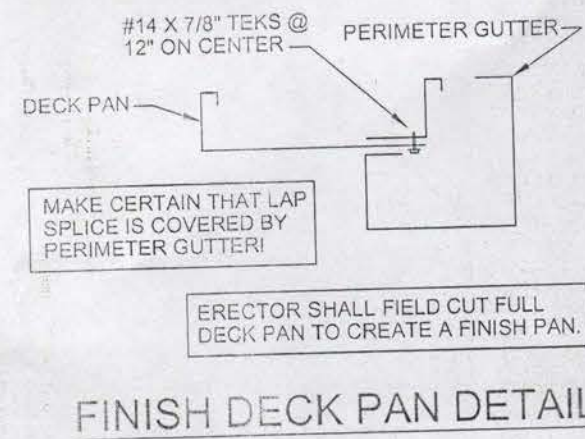




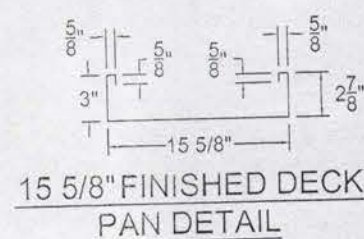




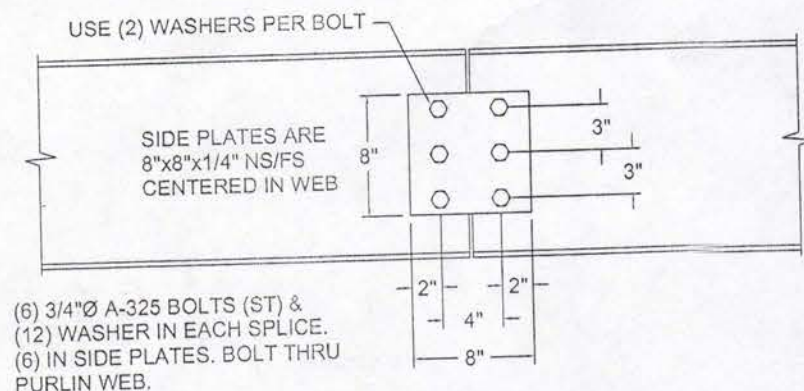
DECK ATTACHMENT DETAIL



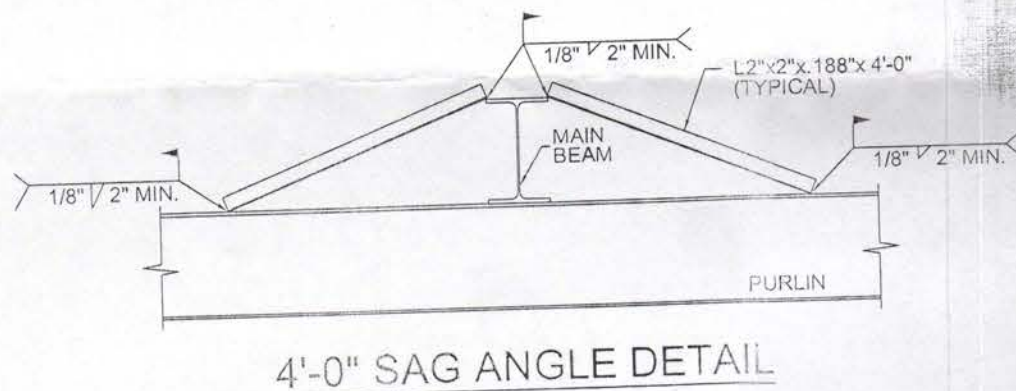
FINISH DECK PAN DETAIL



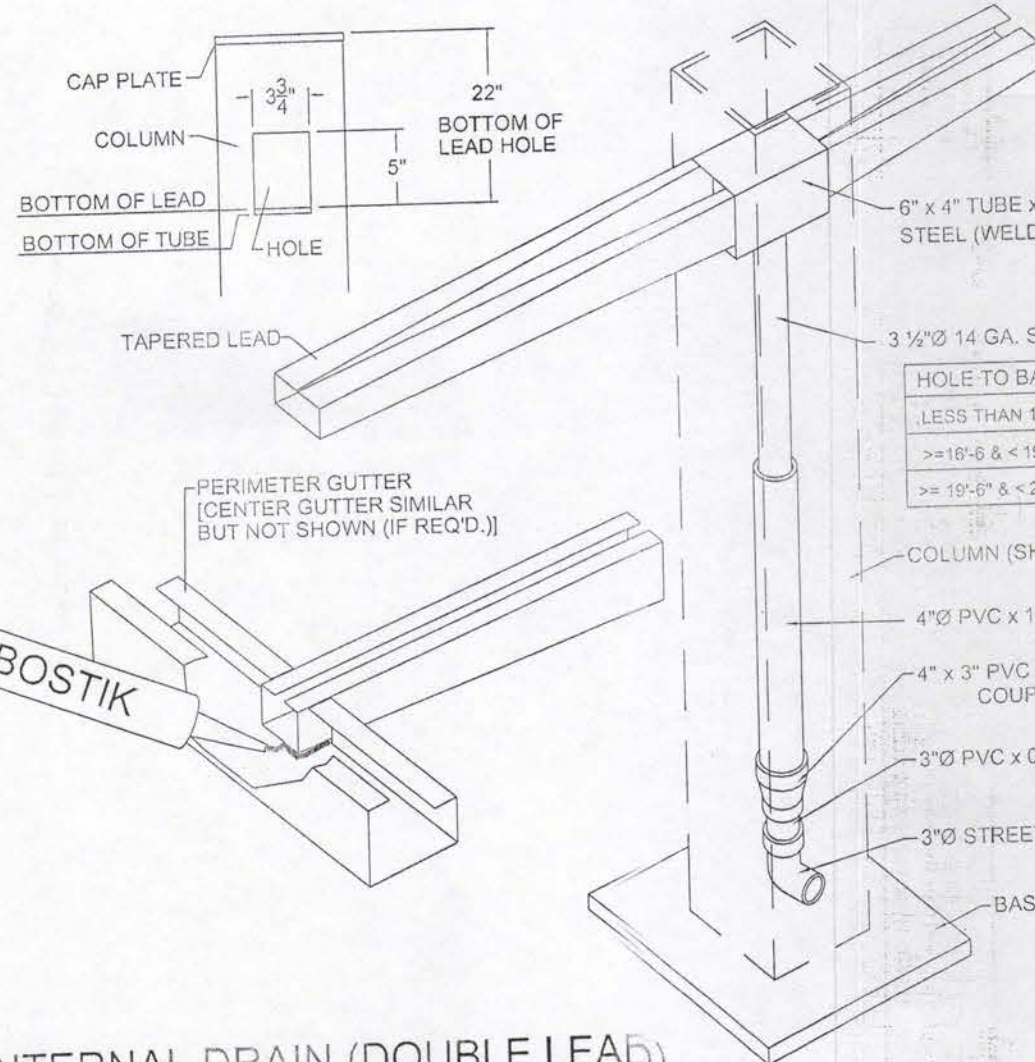
15 5/8" FINISHED DECK PAN DETAIL



PURLIN WEB SPLICE DETAIL  
SPLB



4'-0" SAG ANGLE DETAIL



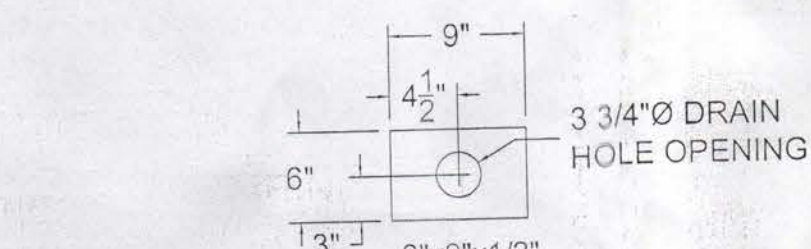
INTERNAL DRAIN (DOUBLE LEAD)

NOTES

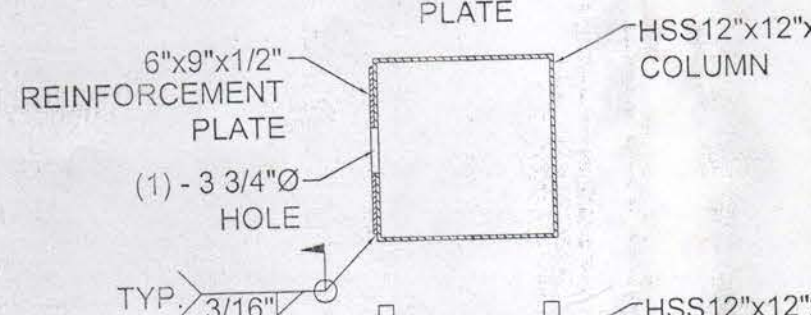
1. CAULK ENTIRE PERIMETER WATER-TIGHT.
- 2 a. FASCIA BRACES AT 48" O.C.
- b. FASCIA BRACES EXTEND 4" PAST END OF DECK PANS (TYP. FOR 6" WIDE GUTTER).
- c. FASTEN FASCIA BRACES WITH (3) TEK SCREWS INTO RIB OF DECK PANS.
- d. FASCIA BRACES ARE 2 1/2" WIDE WITH 1 1/8" LEGS X 22 GA. (MIN.)
3. ACM FASCIA - TABS BENT OVER ON ALL FOUR SIDES. ATTACH SIDE TABS TOGETHER USING (3) #14x7/8 TEK SCREWS. ATTACH TOP & BOTTOM TABS USING #8 PAN HEAD SCREWS 24" O.C.
4. ADJUST TOP L-BRACE UP OR DOWN FOR SNUG FIT OF ACM FASCIA.
5. SILICONE CAULK @ EACH VERTICAL ACM JOINT TO KEEP ANY LIGHT FROM SHINING THROUGH.

#14 x 7/8" TEK SCREW  
#8 x 3/4" PAN HEAD SCREW

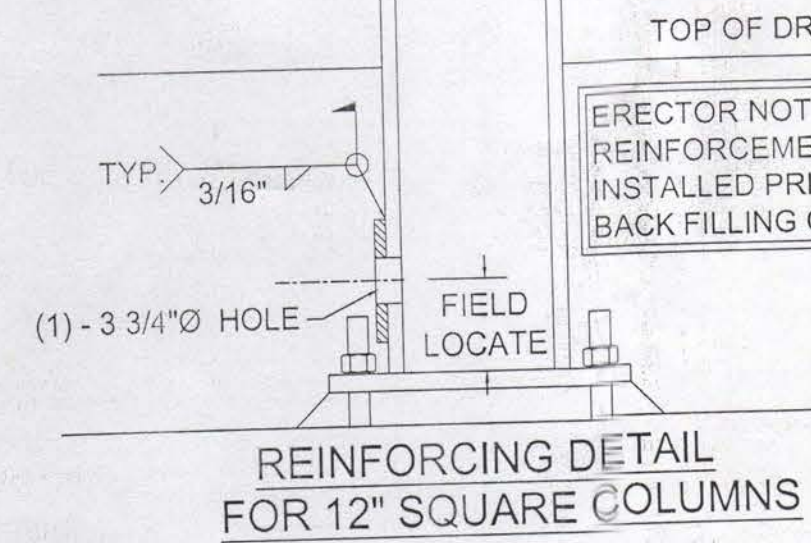
30" ACM FASCIA



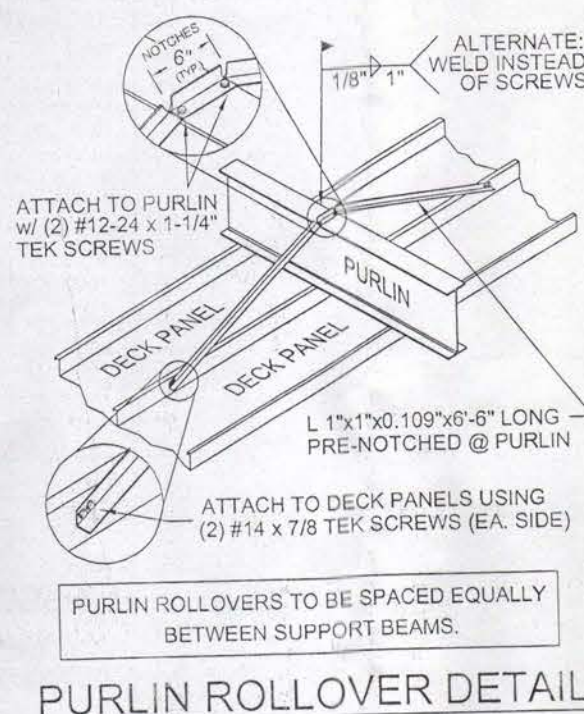
3 3/4" DRAIN HOLE OPENING



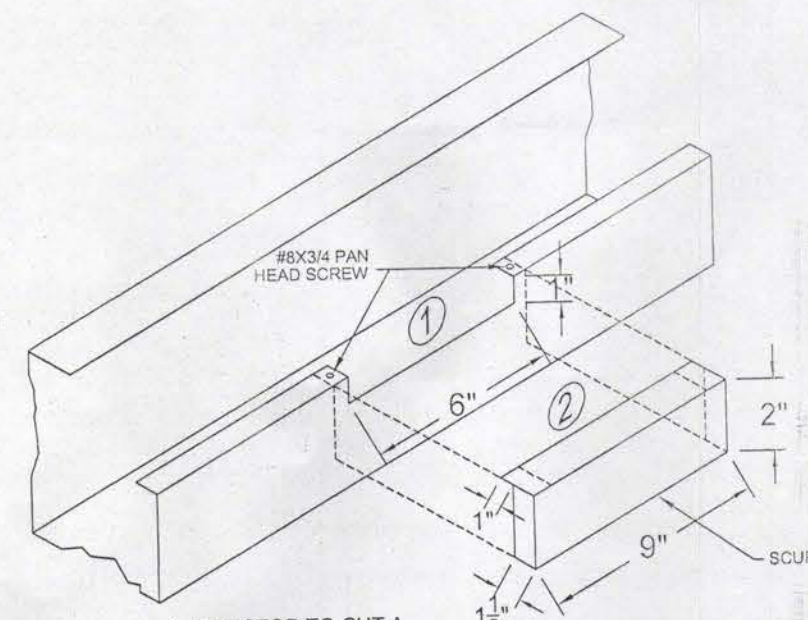
REINFORCING DETAIL FOR 12" SQUARE COLUMNS



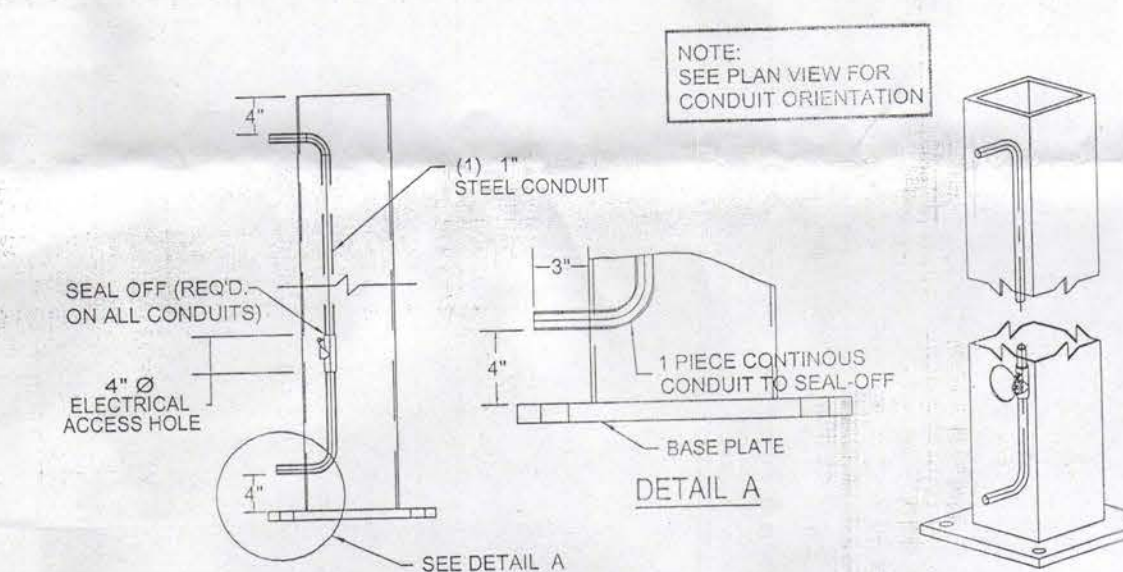
REINFORCING DETAIL FOR 12" SQUARE COLUMNS



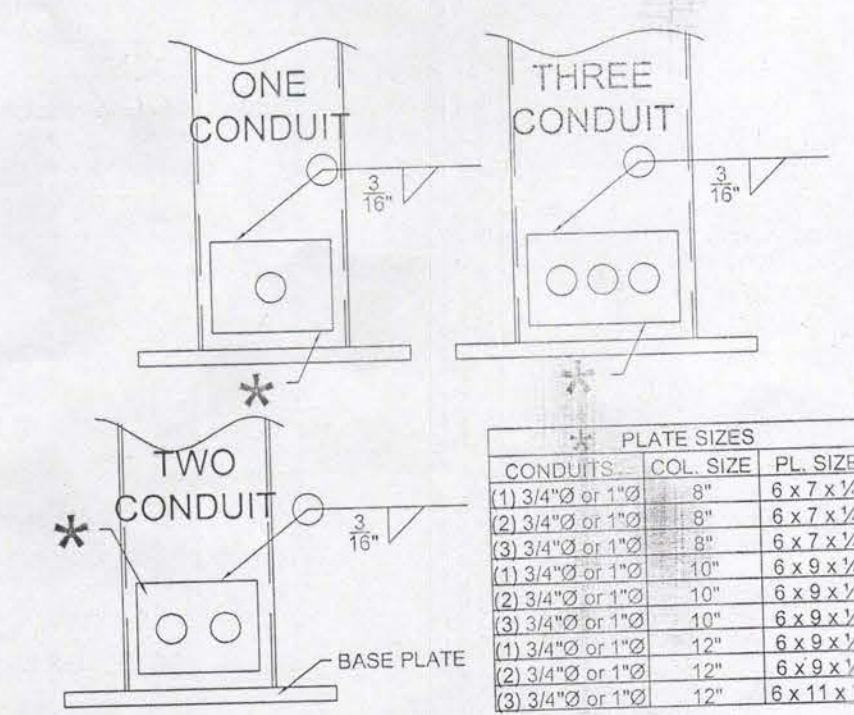
PURLIN ROLLOVER DETAIL



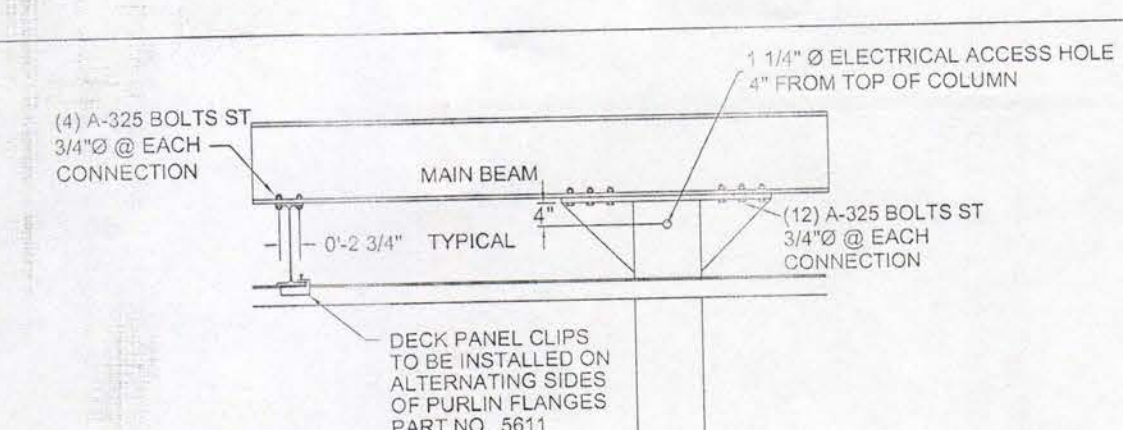
OVERFLOW SCUPPER



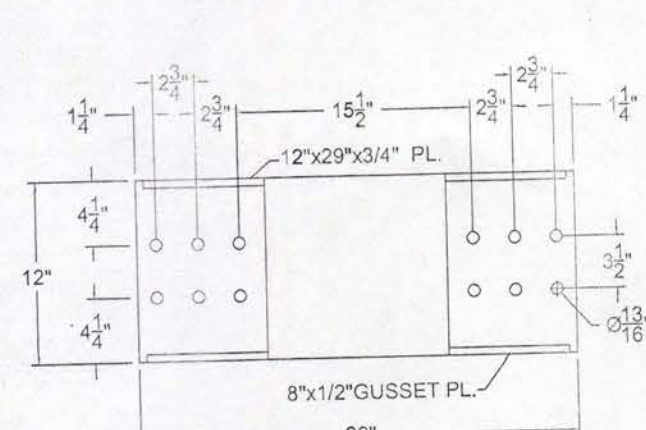
INTERNAL CONDUIT DETAIL  
COLUMN # C2 & C3



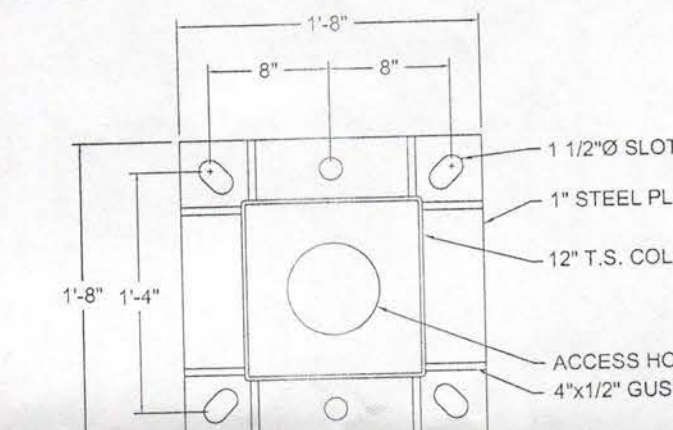
REINFORCEMENT DETAIL



STRUCTURAL CONNECTIONS  
DETAIL \"SCDSS\"



CAP PLATE DETAIL  
CPSS12



BASE PLATE DETAIL  
BP6N12-1"

CERTIFIED BY:



REV.	DESCRIPTION	DATE
1	FOOTER SIZE AND FASCIA MADE	4/15/15
2	CHANGED STEEL BACK TO ORIGINAL DESIGN, ADDED SHELL RECTANGS	4/15/15

ERECTOR CONSTR STEEL JOB FIL

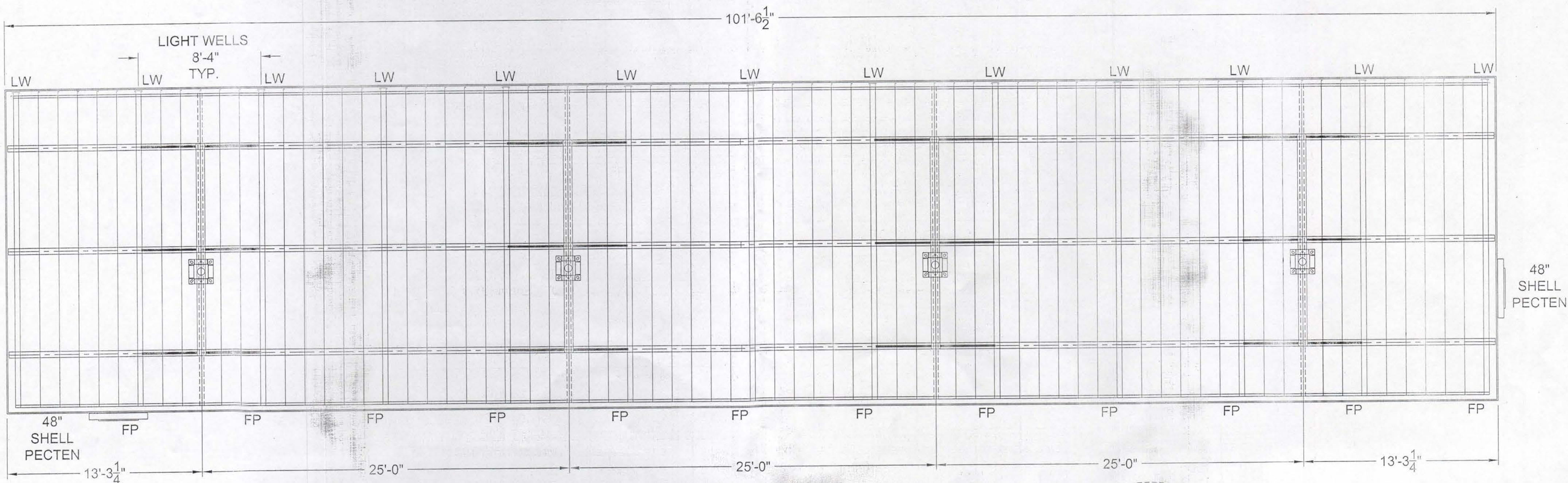


TFC CANOPY  
1107 N. TAYLOR RD.  
GARRETT, IN 46738  
PH. (260) 357-6665 FAX (260) 357-6533

JOB # 699407

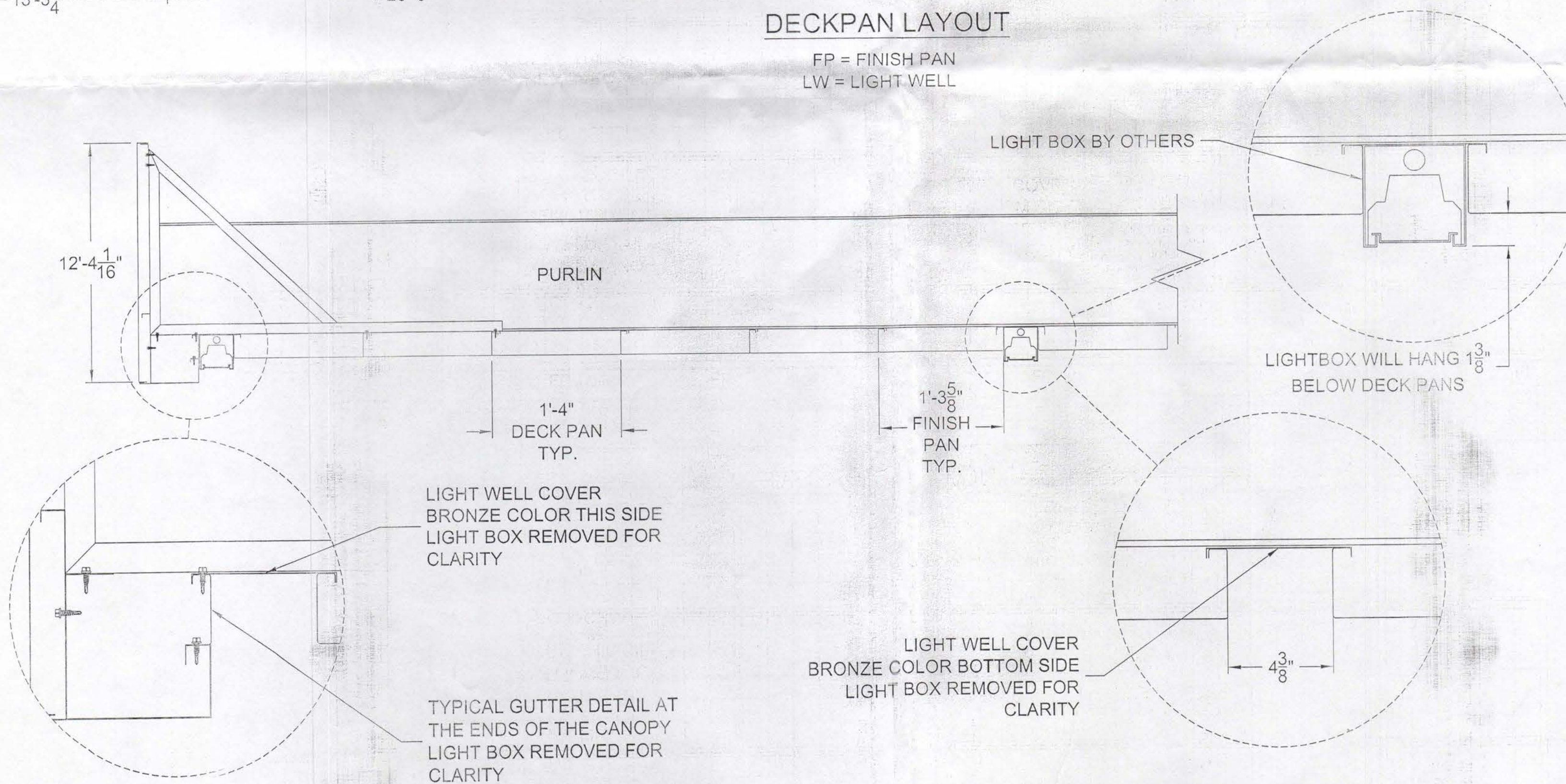
QUOTE #	N/A	PRINT #	N/A
22'-0" x 101'-8 1/2" -- 4 Cols.			
LOCATION:	33588 Woodward Ave. (Barbat Pet. Shell) Birmingham, MI		
CUSTOMER:	R.W. Mercer Jackson, MI 49204		
SCALE:	DO NOT SCALE	ENGINEER:	KED
DATE:	Nov-19-2014	DRAWN BY:	BTD
			DRAWING 2 OF 3





### DECKPAN LAYOUT

FP = FINISH PAN  
LW = LIGHT WELL



CERTIFIED BY:

*Kevin S. [Signature]* 4-15-15

STATE OF MICHIGAN  
KEVIN S. [Signature]  
ENGINEER  
No. 41939

FOOTER SIZE AND PANDA NAME	4/15/15	D.
CHANGED STEEL BACK TO ORIGINAL DESIGN, ADDED SHELL PECTENS	4/15/15	9
REV. DESCRIPTION	DATE	9

☐ ERECTOR ☐ CONSTR ☐ STEEL ☐ JOB FILE

**TFC**

TFC CANOPY  
1107 N. TAYLOR RD.  
GARRETT, IN 46738  
PH. (260) 357-6665 FAX (260) 357-6533

**JOB # 699407**

QUOTE # N/A PRINT # N/A

22'-0" x 101'-8 1/2" -- 4 Cols.

LOCATION: 33588 Woodward Ave. (Barbat Pet. Shell)  
Birmingham, MI

CUSTOMER: R.W. Mercer  
Jackson, MI 49204

SCALE: DO NOT SCALE	ENGINEER: KED	DRAWING NO. 3 OF 3
DATE: Nov-19-2014	DRAWN BY: BTB	



# Administrative Approval Application

## Planning Division

Form will not be processed until it is completely filled out

### 1. Applicant

Name: TOM PLOOM  
Address: 203 PIERCE  
Phone Number: 248.320.4511  
Fax Number: \_\_\_\_\_  
Email: \_\_\_\_\_

### Property Owner

Name: Diana Scheren  
Address: \_\_\_\_\_  
Phone Number: 248 765 3032  
Fax Number: \_\_\_\_\_  
Email: dspink2@yahoo.com

### 2. Applicant's Attorney/Contact Person

Name: \_\_\_\_\_  
Address: \_\_\_\_\_  
Phone Number: \_\_\_\_\_  
Fax Number: \_\_\_\_\_  
Email: \_\_\_\_\_

### Project Designer

Name: CHAS LONGE, AIA  
Address: 124 PEARBODY  
Phone Number: 248.256.6940  
Fax Number: \_\_\_\_\_  
Email: calonge@calongearia.com

### 3. Project Information

Address/Location of Property: 203 PIERCE  
Name of Development: TOAST  
Parcel ID #: 19.36.201.020  
Current Use: COMMERCIAL / B5140  
Area in Acres: \_\_\_\_\_  
Current Zoning: B4

Name of Historic District site is in, if any: \_\_\_\_\_  
Date of HDC Approval, if any: \_\_\_\_\_  
Date of Application for Preliminary Site Plan: \_\_\_\_\_  
Date of Preliminary Site Plan Approval: \_\_\_\_\_  
Date of Application for Final Site Plan: \_\_\_\_\_  
Date of Final Site Plan Approval: \_\_\_\_\_  
Date of Revised Final Site Plan Approval: \_\_\_\_\_

### 4. Attachments

- Warranty Deed with legal description of property
- Authorization from Owner(s) (if applicant is not owner)
- Completed Checklist
- Material Samples
- Digital Copy of plans

- Two (2) folded copies of plans including an itemized list of all changes for which administrative approval is requested, with the changes marked in color on all elevations



### 5. Details of the Request for Administrative Approval

- 1) AWNINGS FOR PLATFORM DINING AREA
- 2) WALL MOUNTED AWNINGS & OUTDOOR SEATING / W/ HEATERS
- 3) APPROVAL OF PLATFORM SIZE PER CITY COMMISSION

The undersigned states the above information is true and correct, and understands that it is the responsibility of the applicant to advise the Planning Division and / or Building Division of any additional changes to the approved site plan.

Signature of Applicant: *[Signature]*

Date: 6.2.15

Application #: <u>AA15-0044</u>	Office Use Only Date Received: <u>6/2/15</u>	Fee: <u>\$ 100.00</u>
Date of Approval: <u>6/3/15</u>	Date of Denial: _____	Reviewed by: <u><i>[Signature]</i></u>

15-0043  
118788



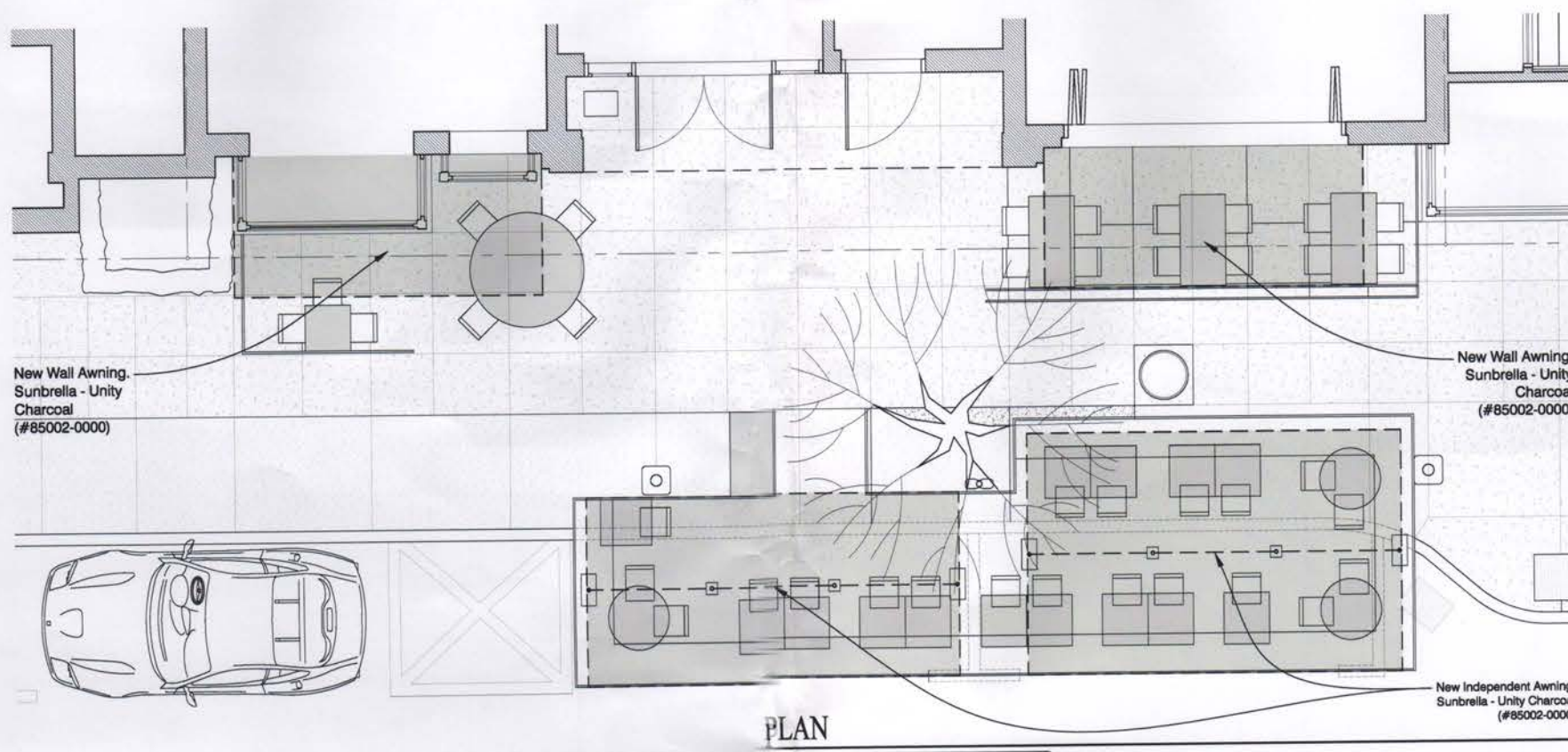
TOAST  
203 Pierce Street  
Birmingham, MI 48009

205 TIE  
APPROVED

6/3/15  
AA-15-044



ELEVATION



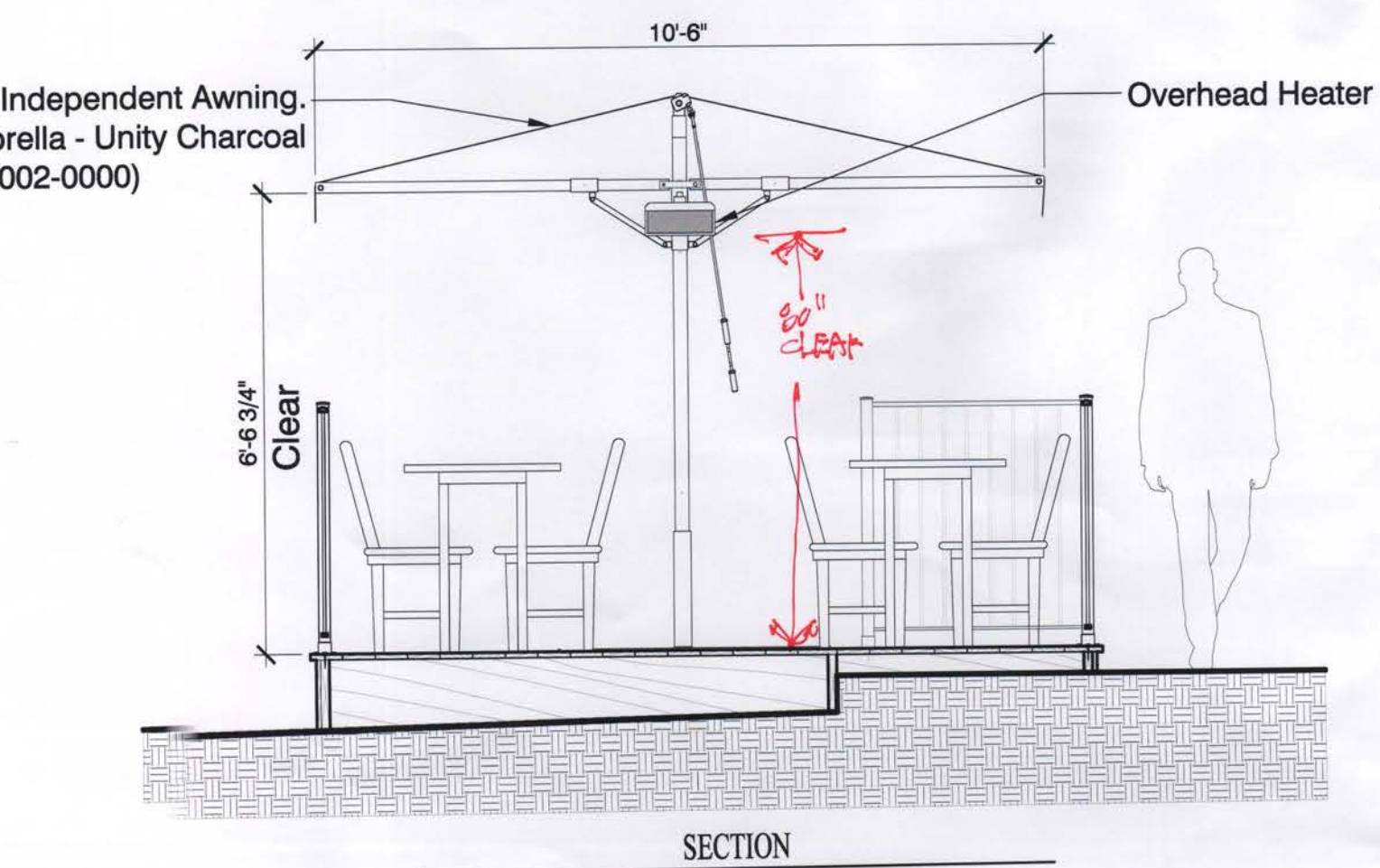
PLAN

CHRISTOPHER J. LONGE AIA  
ARCHITECTURE  
INTERIORS  
124 Peabody, Birmingham, Michigan 48009 248.258.6940

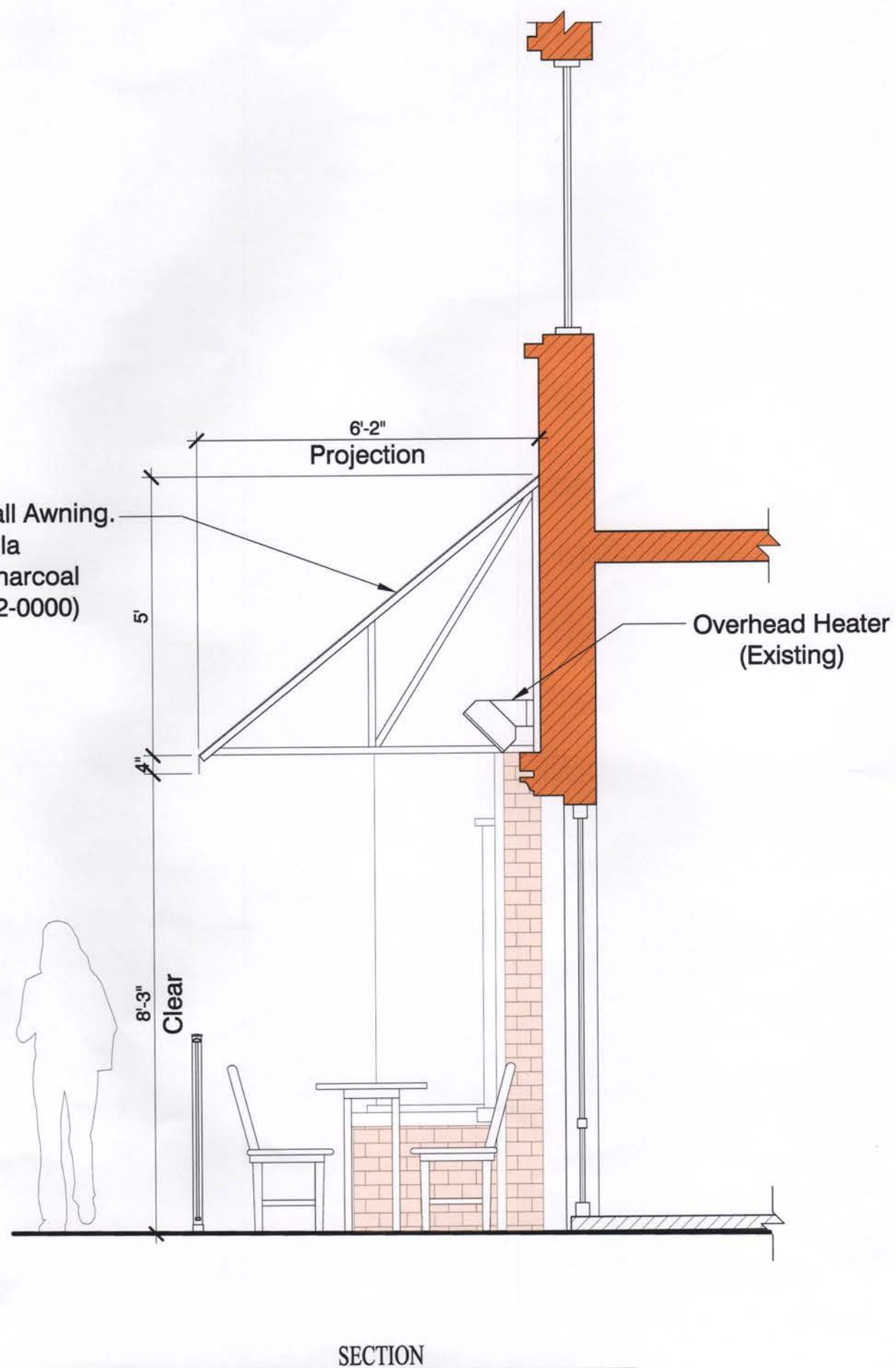


TOAST  
203 Pierce Street  
Birmingham, MI 48009

APPROVED



New Wall Awning.  
Sunbrella  
Unity Charcoal  
(#85002-0000)



CHRISTOPHER J. LONGE AIA  
ARCHITECTURE  
INTERIORS  
124 Peabody, Birmingham, Michigan 48009 248.258.6940



APPR  
7/6/15  
AA-15

203 Pierce  
To

CHRISTOPHER J LONGE AIA  
ARCHITECTURE  
INTERIORS

124 Peabody  
Birmingham, Michigan 48003  
T. 248.258.6940  
F. 248.258.5588

Project  
**Toast Restaurant**  
**203 Pierce Street**  
**Birmingham, Michigan**

preliminary    bids/permits    construction

Drawing Issue Log		
mark	date	issued for
	05.28.15	Contractor

Seal  
Christopher J. Longe A.I.A. Architect No. 31781

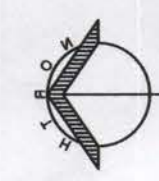
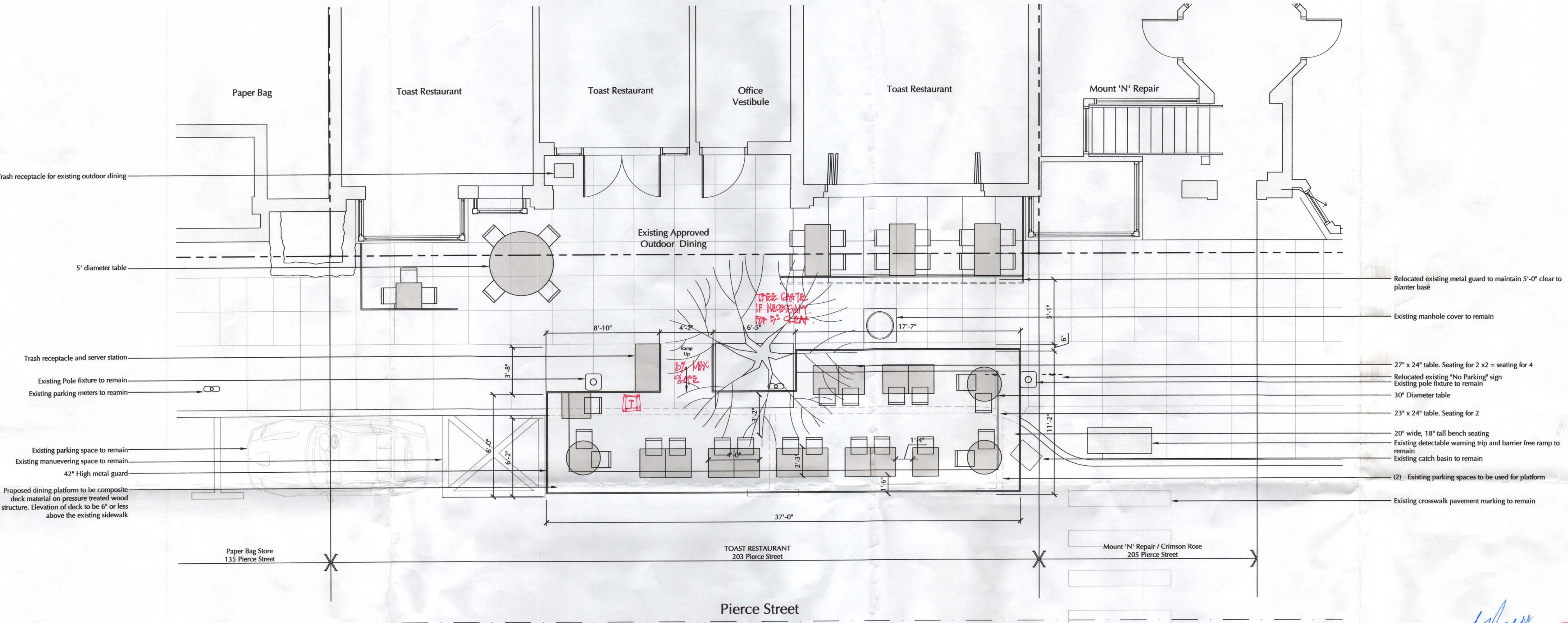
DRAWN BY: P. MARCANTONIO  
CHK'D BY: C. LONGE

Sheet Title  
**Outdoor Dining Platform**

Sheet

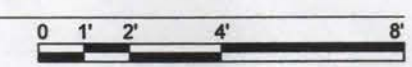
A-102

Project No. 1326  
DESIGN COPY WRITTEN 2015  
BY  
CHRISTOPHER J. LONGE A.I.A.  
ARCHITECTURE LLC



Plan

SCALE: 1/4" = 1'-0"



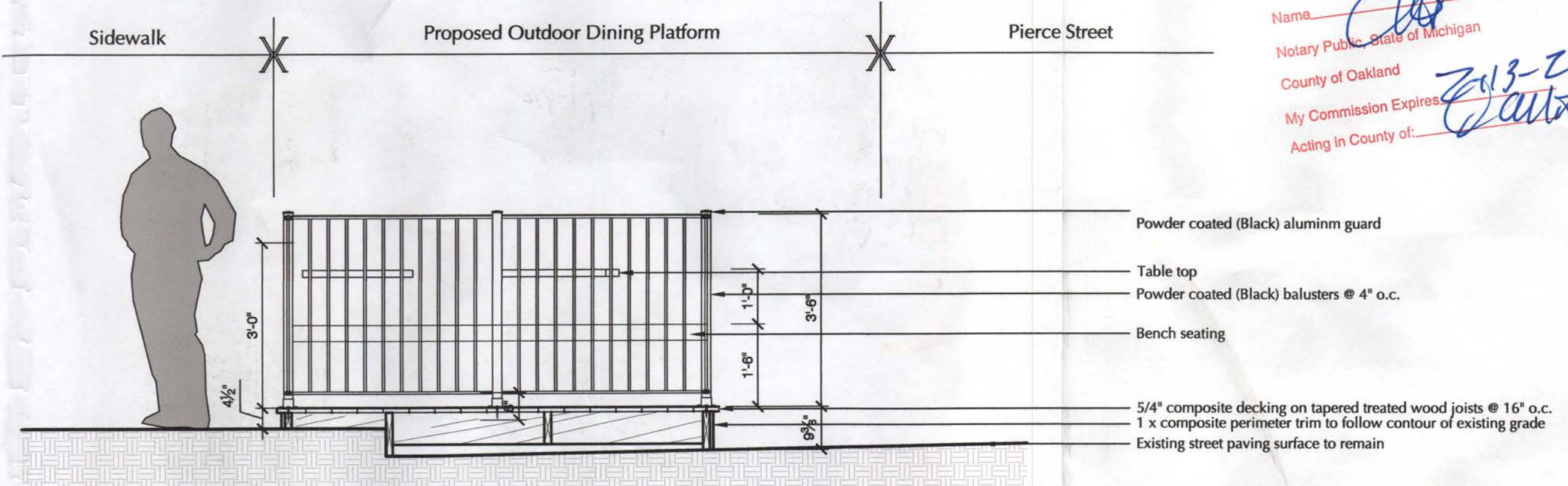
Existing / Proposed Chair

Aluminum Wicker  
Chair  
Black  
By Nesbit  
Part #5050984091254



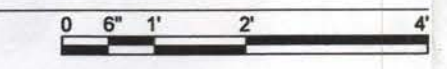
Existing Outdoor Dining Sets

To be replicated at Proposed Dining Platform



Section

SCALE: 1/2" = 1'-0"





# Administrative Approval Application

## Planning Division

Form will not be processed until it is completely filled out

### 1. Applicant

Name: Harvey Weiss, Woodward Brown Associates, LLC  
 Address: 32820 Woodward Ave, Suite 200  
Royal Oak, MI 48073  
 Phone Number: 248-549-3600, Ext 14  
 Fax Number: 248-549-9960  
 Email: hweiss@samonaweiss.com

### Property Owner

Name: Same as Applicant  
 Address: \_\_\_\_\_  
 Phone Number: \_\_\_\_\_  
 Fax Number: \_\_\_\_\_  
 Email: \_\_\_\_\_

### 2. Applicant's Attorney/Contact Person

Name: \_\_\_\_\_  
 Address: \_\_\_\_\_  
 Phone Number: \_\_\_\_\_  
 Fax Number: \_\_\_\_\_  
 Email: \_\_\_\_\_

### Project Designer

Thom Phillips, VP  
 Name: HOBBS + BLACK ASSOCIATES, INC.  
 Address: 100 NORTH STATE STREET,  
ANN ARBOR, MI 48104  
 Phone Number: 734-663-4189  
 Fax Number: 734-663-1770  
 Email: tphillips@hobbs-black.com

### 3. Project Information

Address/Location of Property: \_\_\_\_\_  
34901 & 34953 Woodward, Birmingham MI  
 Name of Development: The Balmoral  
 Parcel ID #: \_\_\_\_\_  
 Current Use: Vacant  
 Area in Acres: 0.5 Acres  
 Current Zoning: D4

Name of Historic District site is in, if any: NA  
 Date of HDC Approval, if any: NA  
 Date of Application for Preliminary Site Plan: 1/31/2007  
 Date of Preliminary Site Plan Approval: \_\_\_\_\_  
 Date of Application for Final Site Plan: 12/12/2007  
 Date of Final Site Plan Approval: 11/12/2010  
 Date of Revised Final Site Plan Approval: 12/05/2013

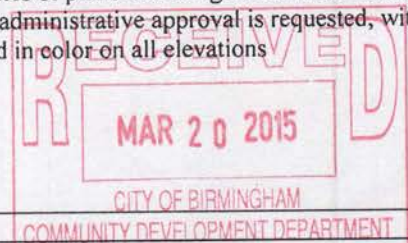
### 4. Attachments

- Warranty Deed with legal description of property
- Authorization from Owner(s) (if applicant is not owner)
- Completed Checklist
- Material Samples
- Digital Copy of plans

- Two (2) folded copies of plans including an itemized list of all changes for which administrative approval is requested, with the changes marked in color on all elevations

### 5. Details of the Request for Administrative Approval

Please see attached letter.



The undersigned states the above information is true and correct, and understands that it is the responsibility of the applicant to advise the Planning Division and / or Building Division of any additional changes to the approved site plan.

Signature of Applicant: Harvey Weiss - ASSOC. OWNER WOODWARD BROWN  
 Date: 3-18-15

Application #: <u>15-0024</u>	Office Use Only Date Received: <u>3/20/15</u>	Fee: <u>\$100</u>
Date of Approval: <u>4/23/15</u>	Date of Denial: _____	Reviewed by: <u>M.B.L.</u>

RECEIVED  
 MAR 20 2015  
 CITY OF BIRMINGHAM  
 COMMUNITY DEVELOPMENT DEPARTMENT

APPROVED

Ref 00116990  
 Date 03/31/2015 3:14:59 PM  
 Amount \$100.00  
 Receipt # 255017

JAA15-0009

PAA15-0024

00116990



# BALMORAL PLACE

## Birmingham, Michigan

### Submittal: **AMENDED FINAL SITE PLAN**

**AMENDED FINAL SITE PLAN REVIEW: 12/05/2013**



**APPROVED**  
M. By 4/23/15  
15-00241 PB

#### SUBMITTED BY:

WOODWARD BROWN ASSOCIATES, LLC  
BY: WEISS SAMONA LAND DEVELOPMENT  
C/O: WEISS PROPERTIES, LLC  
32820, WOODWARD AVE, SUITE 200  
ROYAL OAK, MI 48073  
PHONE: (248) 549-3600 FAX: (248) 549-9960

#### ARCHITECT:

HOBBS & BLACK ASSOCIATES, INC.  
100 N. STATE STREET  
ANN ARBOR, MI 48104  
PHONE: (734) 663-4189  
FAX: (734) 663-1770

#### CIVIL ENGINEERING, SURVEYOR LANDSCAPE ARCHITECT:

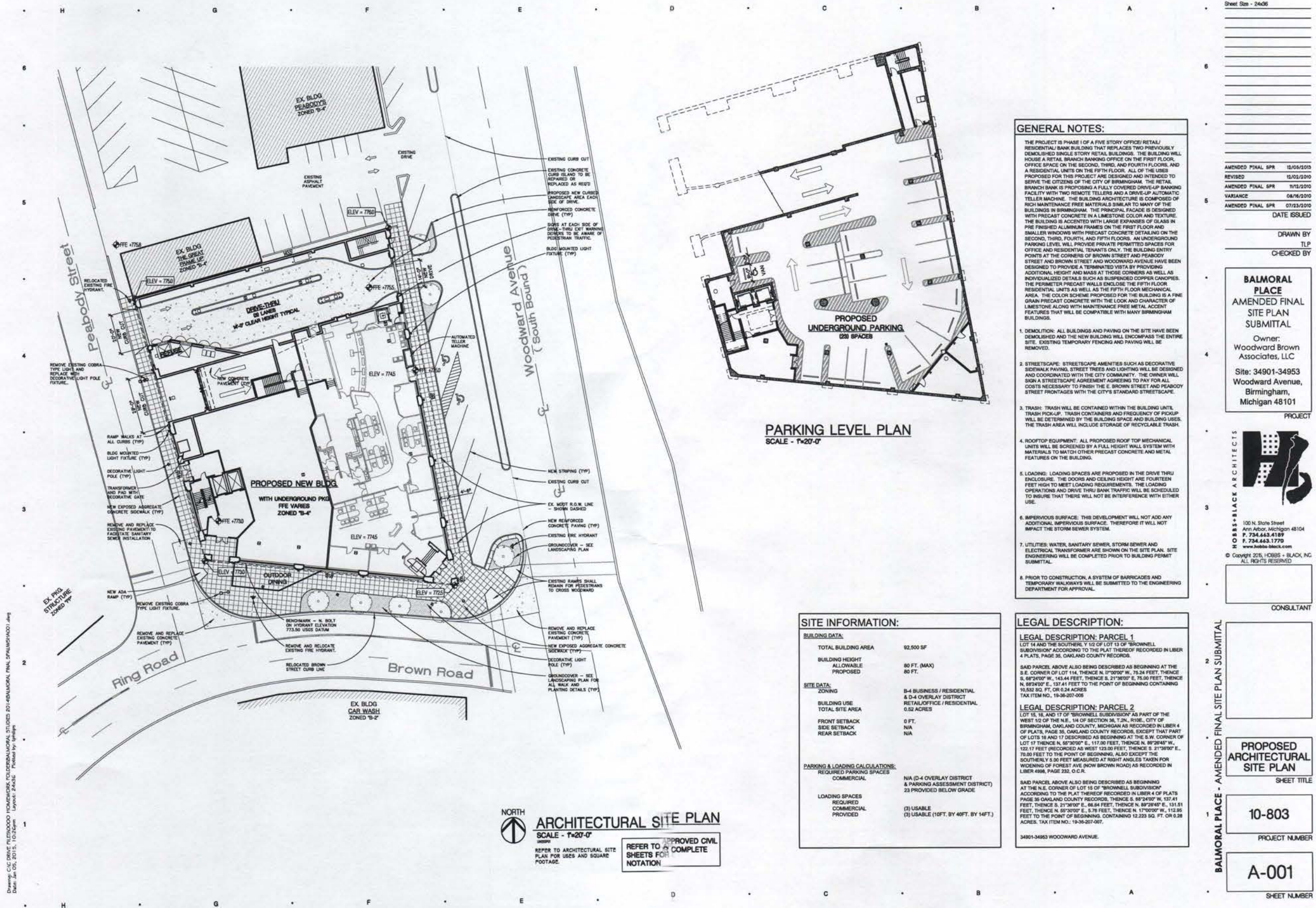
NOWAK & FRAUS  
1310 N. STEPHENSON HIGHWAY  
ROYAL OAK, MI 48067-1508  
PHONE: (248) 399-0886  
FAX: (248) 399-0805

#### SHEET INDEX :

A-000 COVER SHEET  
A-001 ARCHITECTURAL SITE PLAN AND LOWER LEVEL PARKING  
A-002 FIRST FLOOR PLAN  
A-003 2ND, 3RD, 4TH, 5TH FLOOR PLANS AND ROOF PLAN  
A-004 BUILDING ELEVATIONS  
A-005 BUILDING ELEVATIONS  
A-006 BUILDING SECTIONS  
A-007 PHOTOMETRIC PLAN  
A-008 MECHANICAL UNIT CUT SHEETS

Sheet Size - 24x36	
AMENDED FINAL SPR 12/05/2013	
REVISED 12/02/2010	
AMENDED FINAL SPR 11/12/2010	
VARIANCE 06/16/2010	
AMENDED FINAL SPR 07/23/2010	
DATE ISSUED	
DRAWN BY	
TLP	
CHECKED BY	
<b>BALMORAL PLACE</b>	
AMENDED FINAL	
SITE PLAN	
SUBMITTAL	
Owner:	
Woodward Brown Associates, LLC	
Site: 34901-34953	
Woodward Avenue,	
Birmingham,	
Michigan 48101	
PROJECT	
<b>HOBBS &amp; BLACK ARCHITECTS</b>	
100 N. State Street	
Ann Arbor, Michigan 48104	
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BALMORAL PLACE - AMENDED FINAL SITE PLAN SUBMITTAL	
TITLE SHEET	
10-803	
PROJECT NUMBER	
A-000	
SHEET NUMBER	





**GENERAL NOTES:**

THE PROJECT IS PHASE I OF A FIVE STORY OFFICE/RETAIL/RESIDENTIAL BANK BUILDING THAT REPLACES TWO PREVIOUSLY DEMOLISHED SINGLE STORY RETAIL BUILDINGS. THE BUILDING WILL HOUSE A RETAIL BRANCH BANKING OFFICE ON THE FIRST FLOOR, OFFICE SPACE ON THE SECOND, THIRD, AND FOURTH FLOORS, AND A RESIDENTIAL UNIT ON THE FIFTH FLOOR. ALL OF THE USES PROPOSED FOR THIS PROJECT ARE DESIGNED AND INTENDED TO SERVE THE CITIZENS OF THE CITY OF BIRMINGHAM. THE RETAIL BRANCH BANK IS PROPOSING A FULLY COVERED DRIVE-UP BANKING FACILITY WITH TWO REMOTE TELLERS AND A DRIVE-UP AUTOMATIC TELLER MACHINE. THE BUILDING ARCHITECTURE IS COMPOSED OF RICH MAINTENANCE FREE MATERIALS SIMILAR TO MANY OF THE BUILDINGS IN BIRMINGHAM. THE PRINCIPAL FACADE IS DESIGNED WITH PRECAST CONCRETE IN A LIMESTONE COLOR AND TEXTURE. THE BUILDING IS ACCENTED WITH LARGE EXPANSES OF GLASS IN PRE FINISHED ALUMINUM FRAMES ON THE FIRST FLOOR AND SMALLER WINDOWS WITH PRECAST CONCRETE DETAILING ON THE SECOND, THIRD, FOURTH, AND FIFTH FLOORS. AN UNDERGROUND PARKING LEVEL WILL PROVIDE PRIVATE PERMITTED SPACES FOR OFFICE AND RESIDENTIAL TENANTS ONLY. THE BUILDING ENTRY POINTS AT THE CORNERS OF BROWN STREET AND PEABODY STREET AND BROWN STREET AND WOODWARD AVENUE HAVE BEEN DESIGNED TO PROVIDE A TERMINATED VISTA BY PROVIDING ADDITIONAL HEIGHT AND MASS AT THOSE CORNERS AS WELL AS INDIVIDUALIZED DETAILS SUCH AS SUSPENDED COPPER CANOPIES. THE PERIMETER PRECAST WALLS ENCLOSE THE FIFTH FLOOR RESIDENTIAL UNITS AS WELL AS THE FIFTH FLOOR MECHANICAL AREA. THE COLOR SCHEME PROPOSED FOR THE BUILDING IS A FINE GRAIN PRECAST CONCRETE WITH THE LOOK AND CHARACTER OF LIMESTONE ALONG WITH WATERED WHITE METAL ACCENT FEATURES THAT WILL BE COMPATIBLE WITH MANY BIRMINGHAM BUILDINGS.

1. DEMOLITION: ALL BUILDINGS AND PAVING ON THE SITE HAVE BEEN DEMOLISHED AND THE NEW BUILDING WILL ENCOMPASS THE ENTIRE SITE. EXISTING TEMPORARY FENCING AND PAVING WILL BE REMOVED.

2. STREETSCAPE: STREETSCAPE AMENITIES SUCH AS DECORATIVE SIDEWALK PAVING, STREET TREES AND LIGHTING WILL BE DESIGNED AND COORDINATED WITH THE CITY COMMUNITY. THE OWNER WILL SIGN A STREETSCAPE AGREEMENT AGREEING TO PAY FOR ALL COSTS NECESSARY TO FINISH THE E. BROWN STREET AND PEABODY STREET FRONTAGES WITH THE CITY'S STANDARD STREETSCAPE.

3. TRASH: TRASH WILL BE CONTAINED WITHIN THE BUILDING UNTIL TRASH PICK-UP. TRASH CONTAINERS AND FREQUENCY OF PICKUP WILL BE DETERMINED BY THE BUILDING SPACE AND BUILDING USES. THE TRASH AREA WILL INCLUDE STORAGE OF RECYCLABLE TRASH.

4. ROOFTOP EQUIPMENT: ALL PROPOSED ROOF TOP MECHANICAL UNITS WILL BE SCREENED BY A RAIL HEIGHT WALL SYSTEM WITH MATERIALS TO MATCH OTHER PRECAST CONCRETE AND METAL FEATURES ON THE BUILDING.

5. LOADING: LOADING SPACES ARE PROPOSED IN THE DRIVE THRU ENCLOSURE. THE DOORS AND CEILING HEIGHT ARE FOURTEEN FEET HIGH TO MEET LOADING REQUIREMENTS. THE LOADING OPERATIONS AND DRIVE THRU BANK TRAFFIC WILL BE SCHEDULED TO INSURE THAT THERE WILL NOT BE INTERFERENCE WITH EITHER USE.

6. IMPERVIOUS SURFACE: THIS DEVELOPMENT WILL NOT ADD ANY ADDITIONAL IMPERVIOUS SURFACE. THEREFORE IT WILL NOT IMPACT THE STORM SEWER SYSTEM.

7. UTILITIES: WATER, SANITARY SEWER, STORM SEWER AND ELECTRICAL TRANSFORMER ARE SHOWN ON THE SITE PLAN. SITE ENGINEERING WILL BE COMPLETED PRIOR TO BUILDING PERMIT SUBMITTAL.

8. PRIOR TO CONSTRUCTION, A SYSTEM OF BARRICADES AND TEMPORARY WALKWAYS WILL BE SUBMITTED TO THE ENGINEERING DEPARTMENT FOR APPROVAL.

**PARKING LEVEL PLAN**  
SCALE - 1/4"=1'-0"

SITE INFORMATION:	
<b>BUILDING DATA:</b>	
TOTAL BUILDING AREA	92,500 SF
BUILDING HEIGHT ALLOWABLE PROPOSED	80 FT. (MAX) 80 FT.
<b>SITE DATA:</b>	
ZONING	B-4 BUSINESS / RESIDENTIAL & D-4 OVERLAY DISTRICT
BUILDING USE	RETAIL/OFFICE / RESIDENTIAL
TOTAL SITE AREA	0.52 ACRES
FRONT SETBACK	0 FT.
SIDE SETBACK	N/A
REAR SETBACK	N/A
<b>PARKING &amp; LOADING CALCULATIONS:</b>	
REQUIRED PARKING SPACES COMMERCIAL	N/A (D-4 OVERLAY DISTRICT & PARKING ASSESSMENT DISTRICT) 23 PROVIDED BELOW GRADE
LOADING SPACES REQUIRED COMMERCIAL PROVIDED	(3) USABLE (3) USABLE (10FT. BY 40FT. BY 14FT.)

**LEGAL DESCRIPTION:**

**LEGAL DESCRIPTION: PARCEL 1**  
LOT 14 AND THE SOUTHERLY 1/2 OF LOT 13 OF "BROWNELL SUBDIVISION" ACCORDING TO THE PLAT THEREOF RECORDED IN LIBER 4 OF PLATS, PAGE 35, OKLAHOMA COUNTY RECORDS.

SAID PARCEL ABOVE ALSO BEING DESCRIBED AS BEGINNING AT THE S.E. CORNER OF LOT 114, THENCE N. 17°00'00" W. 75.24 FEET, THENCE S. 89°24'00" W. 143.44 FEET, THENCE S. 21°36'00" E. 75.00 FEET, THENCE N. 86°24'00" E. 127.41 FEET TO THE POINT OF BEGINNING CONTAINING 10.532 SQ. FT. OR 0.24 ACRES  
TAX ITEM NO. 19-36-207-008

**LEGAL DESCRIPTION: PARCEL 2**  
LOT 15, 16, AND 17 OF "BROWNELL SUBDIVISION" AS PART OF THE WEST 1/2 OF THE N.E. 1/4 OF SECTION 36, 12N., 10E., CITY OF BIRMINGHAM, OKLAHOMA COUNTY, MICHIGAN AS RECORDED IN LIBER 4 OF PLATS, PAGE 35, OKLAHOMA COUNTY RECORDS, EXCEPT THAT PART OF LOTS 16 AND 17 DESCRIBED AS BEGINNING AT THE S.W. CORNER OF LOT 17 THENCE N. 89°30'00" E. 117.00 FEET, THENCE N. 89°26'45" W. 122.17 FEET RECORDED AS WEST 123.00 FEET, THENCE S. 21°36'00" E. 75.00 FEET TO THE POINT OF BEGINNING, ALSO EXCEPT THE SOUTHERLY 5.00 FEET MEASURED AT RIGHT ANGLES TAKEN FOR WIDENING OF FOREST AVE (NOW BROWN ROAD) AS RECORDED IN LIBER 488, PAGE 232, O.C.R.

SAID PARCEL ABOVE ALSO BEING DESCRIBED AS BEGINNING AT THE N.E. CORNER OF LOT 15 OF "BROWNELL SUBDIVISION" ACCORDING TO THE PLAT THEREOF RECORDED IN LIBER 4 OF PLATS, PAGE 35, OKLAHOMA COUNTY RECORDS, THENCE S. 89°24'00" W. 137.41 FEET, THENCE S. 21°36'00" E. 66.64 FEET, THENCE N. 89°26'45" W. 131.51 FEET, THENCE N. 89°30'00" E. 5.78 FEET, THENCE N. 17°00'00" W. 112.85 FEET TO THE POINT OF BEGINNING, CONTAINING 12.225 SQ. FT. OR 0.28 ACRES. TAX ITEM NO. 19-36-207-007.

34901-34963 WOODWARD AVENUE.

**ARCHITECTURAL SITE PLAN**  
SCALE - 1/4"=1'-0"  
REFER TO ARCHITECTURAL SITE PLAN FOR USES AND SQUARE FOOTAGE.  
REFER TO APPROVED CIVIL SHEETS FOR COMPLETE NOTATION.

Sheet Size - 24x36

AMENDED FINAL SPR	12/05/2015
REVISED	12/02/2010
AMENDED FINAL SPR	11/12/2010
VARIANCE	08/16/2010
AMENDED FINAL SPR	07/23/2010
DATE ISSUED	

DRAWN BY  
TLP  
CHECKED BY

**BALMORAL PLACE**  
AMENDED FINAL  
SITE PLAN  
SUBMITTAL  
Owner:  
Woodward Brown  
Associates, LLC  
Site: 34901-34953  
Woodward Avenue,  
Birmingham,  
Michigan 48101

PROJECT

**JOHN + BLACK ARCHITECTS**  
100 N. State Street  
Ann Arbor, Michigan 48104  
P. 734.443.4189  
F. 734.443.1770  
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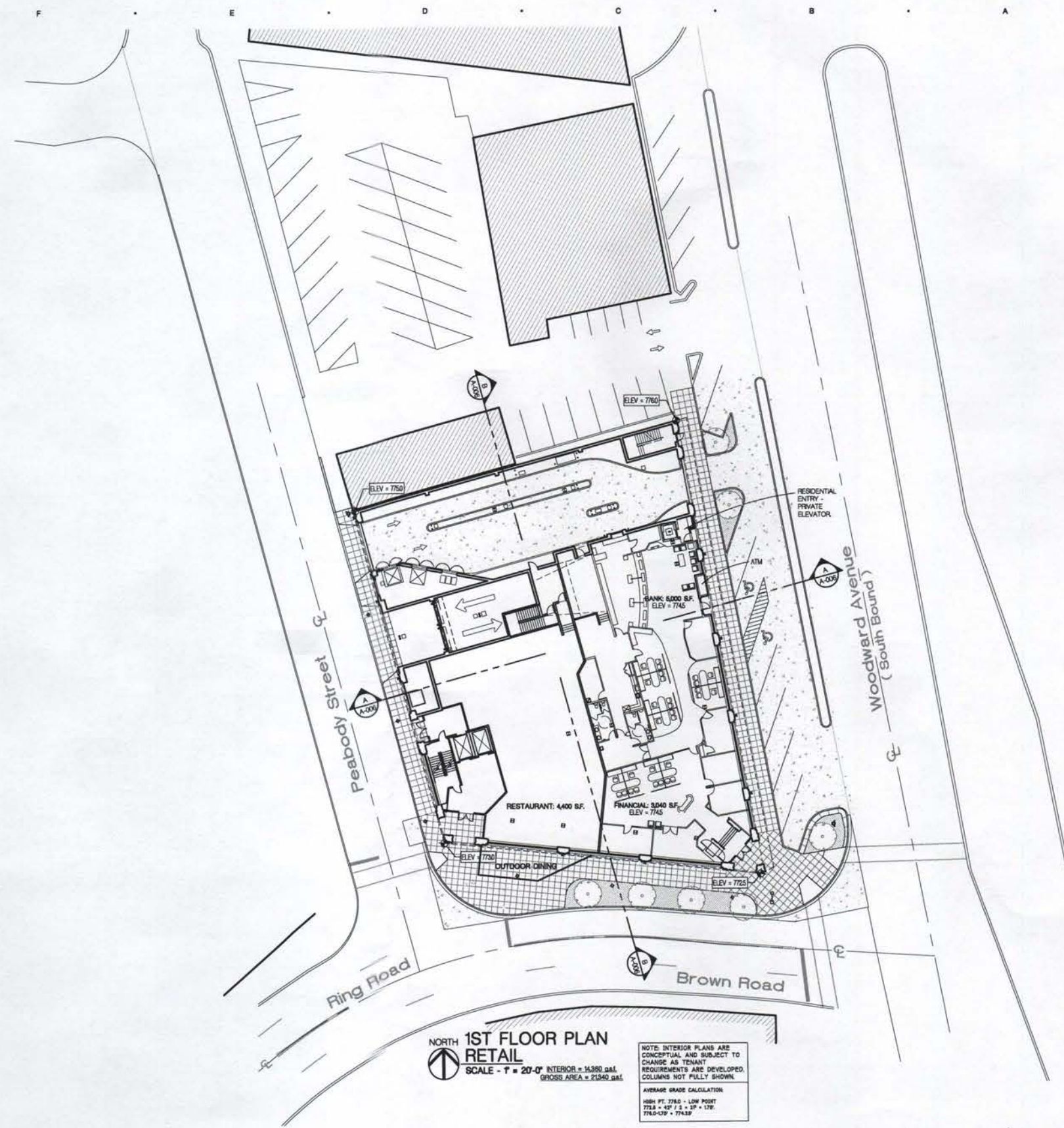
**PROPOSED ARCHITECTURAL SITE PLAN**  
SHEET TITLE

**10-803**  
PROJECT NUMBER

**A-001**  
SHEET NUMBER



Drawing: C:\C Drive\PLANS\2000\TOWN\WORK\FOUR\BIRMINGHAM\STUDIOS 201\BIRMINGHAM FINAL SPANISH\A002.dwg  
Date: Jan 05, 2015, 10:30pm Layout: 24x36 Plotted by: gphs



**1ST FLOOR PLAN**  
**RETAIL**  
SCALE - 1" = 20'-0" INTERIOR = 14,380 sq.ft.  
GROSS AREA = 21,340 sq.ft.

NOTE: INTERIOR PLANS ARE CONCEPTUAL AND SUBJECT TO CHANGE AS TENANT REQUIREMENTS ARE DEVELOPED. COLUMNS NOT FULLY SHOWN.

AVERAGE GRADE CALCULATION  
HIGH PT. 776.0 - LOW POINT  
772.5 - 40' / 3" = 3" = 178'  
776.0 - 178' = 774.3'

Sheet Size - 24x36

AMENDED FINAL SPR	12/05/2015
REVISED	12/02/2010
AMENDED FINAL SPR	11/12/2010
VARIANCE	08/16/2010
AMENDED FINAL SPR	07/23/2010
DATE ISSUED	

DRAWN BY  
TLP  
CHECKED BY

**BALMORAL PLACE**  
AMENDED FINAL  
SITE PLAN  
SUBMITTAL

Owner:  
Woodward Brown  
Associates, LLC

Site: 34901-34953  
Woodward Avenue,  
Birmingham,  
Michigan 48101

PROJECT

**HOBBS+BLACK ARCHITECTS**

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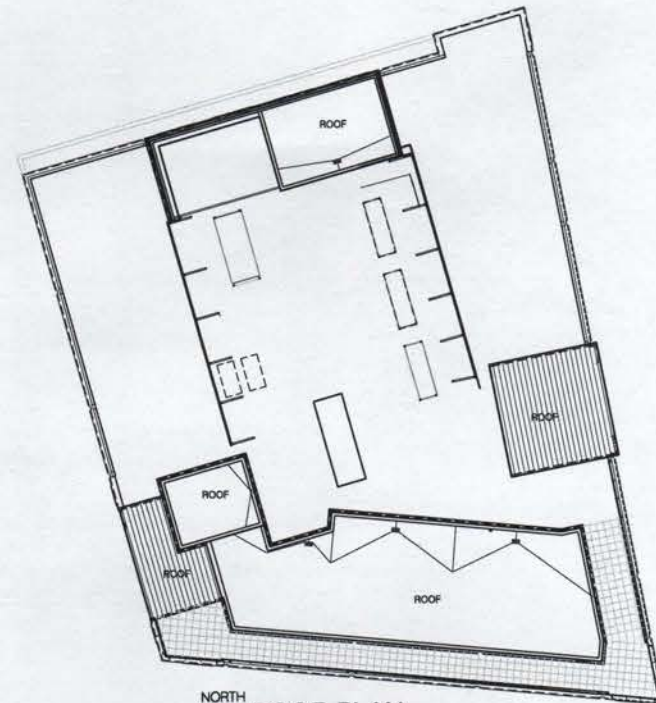
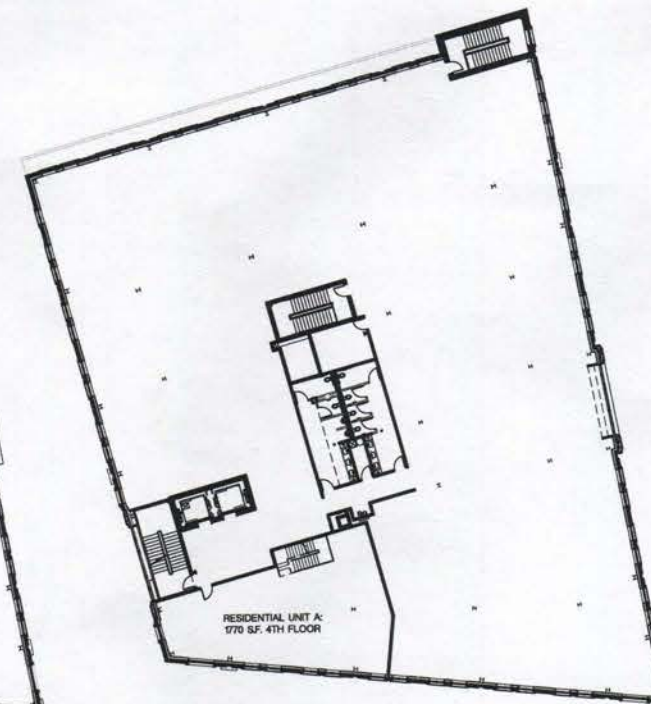
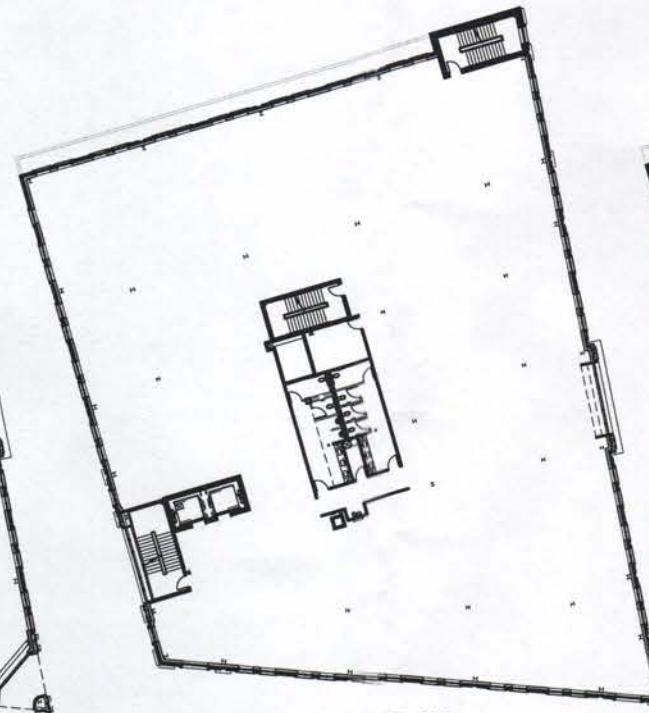
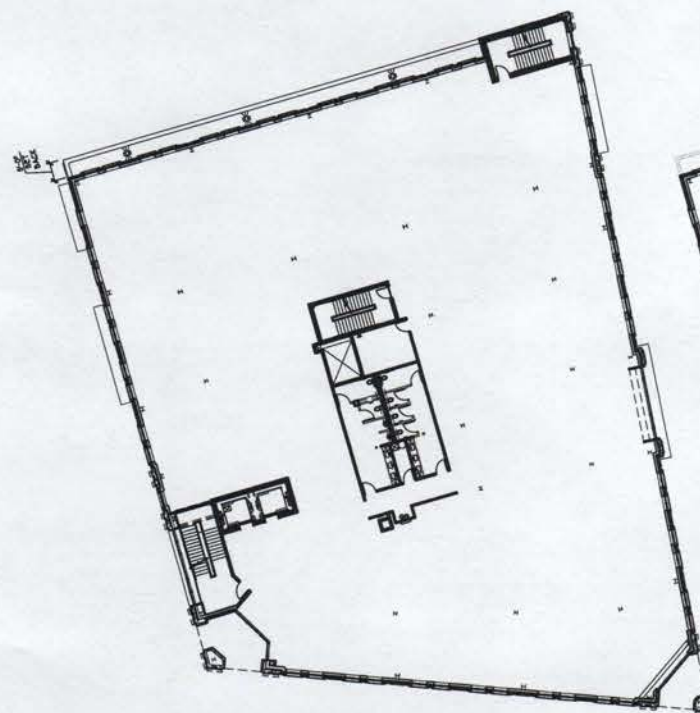
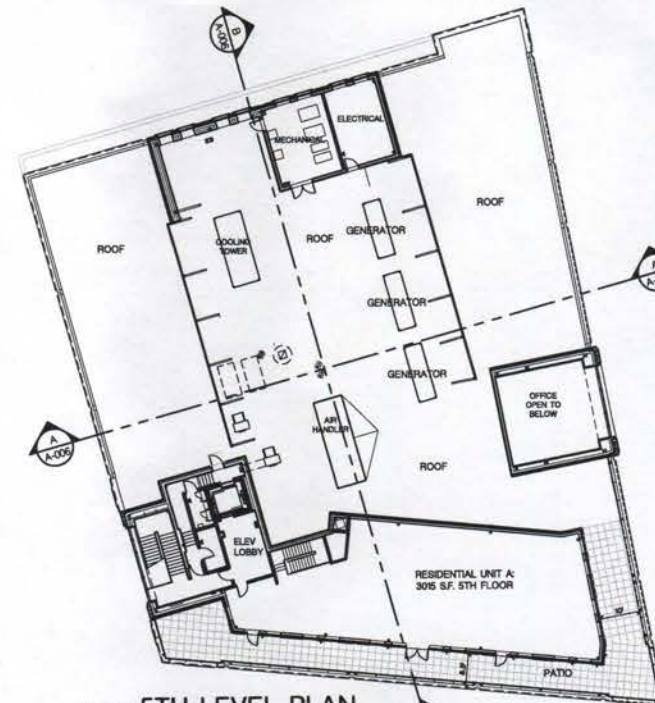
**BALMORAL PLACE - AMENDED FINAL SITE PLAN SUBMITTAL**

**FLOOR PLANS**  
SHEET TITLE

**10-803**  
PROJECT NUMBER

**A-002**  
SHEET NUMBER



[illegible]

Sheet Size - 24x36

AMENDED FINAL SPR	12/05/2013
REVISED	12/02/2010
AMENDED FINAL SPR	11/12/2010
VARIANCE	08/18/2010
AMENDED FINAL SPR	07/23/2010
DATE ISSUED	

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**BALMORAL  
PLACE**  
AMENDED FINAL  
SITE PLAN  
SUBMITTAL

Owner:  
Woodward Brown  
Associates, LLC  
Site: 34901-34953  
Woodward Avenue,  
Birmingham,  
Michigan 48101

PROJECT

BLACK ARCHITECTS

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[www.hobbs-block.com](http://www.hobbs-block.com)

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## FLOOR PLANS

SHEET TITLE

10-803

PROJECT NUMBER

A-003

SHEET NUMBER

1 2



TRANSPARENT MATERIAL TABULATIONS FOR EAST ELEVATION

TRANSPARENT AREAS	1ST LEVEL	2ND THRU 5TH LEVELS
TRANSPARENT AREA	890	2,177
WALL AREA	421	1,530
TOTAL AREA	1,411	7,439
PERCENTAGE	76.1%	29.3%

• EACH ELEVATION IS TABULATED INDEPENDENTLY.  
• FIRST LEVEL FLOOR AREA IS TABULATED FROM 1'-0" TO 8'-0" ABOVE GRADE.

- DECORATIVE PRECAST RAIL SET BACK FROM EAVE LINE
- EAVE LINE
- CAST STONE SMOOTH
- PREFIN. ALUM. WINDOW W/ TINTED LOW-E GLASS
- PREFIN. DECORATIVE METAL GUARDRAIL
- PREFIN. ALUM. WINDOW W/ TINTED LOW-E GLASS
- PREFIN. DECORATIVE METAL GUARDRAIL
- TENANT SIGN BAND LOCATION - SIZED TO MEET ORDINANCE REQUIREMENTS (TYP)
- CAST STONE SMOOTH
- PREFIN. ALUM. WINDOW W/ TINTED LOW-E GLASS
- PREFIN. SUN SHADE STRUCTURE
- PREFIN. STOREFRONT GLAZING SYSTEM W/ CLEAR LOW-E GLASS
- MATCHING PRECAST CONC. PIERS
- CAST STONE OR GRANITE BASE



**EAST (WOODWARD AVENUE) ELEVATION**  
SCALE - 1/16" = 1'-0"

TRANSPARENT MATERIAL TABULATIONS FOR WEST ELEVATION

TRANSPARENT AREAS	1ST LEVEL	2ND THRU 5TH LEVELS
TRANSPARENT AREA	660	1,280
WALL AREA	280	970
TOTAL AREA	940	7050
PERCENTAGE	70%	17.9%

• EACH ELEVATION IS TABULATED INDEPENDENTLY.  
• FIRST LEVEL FLOOR AREA IS TABULATED FROM 1'-0" TO 8'-0" ABOVE GRADE.

- CAST STONE SMOOTH
- PREFIN. ALUM. WINDOW W/ TINTED LOW-E GLASS
- PREFIN. DECORATIVE METAL GUARDRAIL
- PREFIN. ALUM. WINDOW W/ TINTED LOW-E GLASS
- PREFIN. DECORATIVE METAL GUARDRAIL
- CAST STONE SMOOTH PANELS AND ACCENT SHAPES
- PREFIN. CANOPY STRUCTURE
- PREFINISHED METAL DOORS AND FRAME AT TRASH ROOM



**WEST (PEABODY STREET) ELEVATION**  
SCALE - 1/16" = 1'-0"

Sheet Size - 24x36

AMENDED FINAL SPR 12/06/2013  
REVISED 12/02/2010  
AMENDED FINAL SPR 11/12/2010  
VARIANCE 08/16/2010  
AMENDED FINAL SPR 07/23/2010  
DATE ISSUED

DRAWN BY TLP  
CHECKED BY

**BALMORAL PLACE**  
AMENDED FINAL  
SITE PLAN  
SUBMITTAL

Owner:  
Woodward Brown  
Associates, LLC

Site: 34901-34953  
Woodward Avenue,  
Birmingham,  
Michigan 48101

PROJECT

**HOBBBS+BLACK ARCHITECTS**

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**BALMORAL PLACE - AMENDED FINAL SITE PLAN SUBMITTAL**

**ELEVATIONS**

SHEET TITLE

**10-803**

PROJECT NUMBER

**A-004**

SHEET NUMBER

Drawing: C:\C DRIVE FILES\WOODWORK\TOWERS\BALMORAL STUDIOS 201\BALMORAL FINAL SPAN\SHOOTS.dwg  
Date: Jan 12, 2015, 1:00:00pm  
Layout: 24x36  
Printed by: jphillips



TRANSPARENT MATERIAL TABULATIONS FOR SOUTH ELEVATION

TRANSPARENT AREAS	1ST LEVEL	2ND THRU 4TH LEVELS	5TH LEVEL
TRANSPARENT AREA	584	1463	439
WALL AREA	238	4410	625
TOTAL AREA	622	5873	1064
PERCENTAGE	7%	67%	13%

• EACH ELEVATION IS TABULATED INDEPENDENTLY  
• FIRST LEVEL FLOOR AREA IS TABULATED FROM 1'-0" TO 8'-0" ABOVE GRADE

PREFIN. METAL TUBE GUARDRAIL ON CAST STONE BASE BEYOND. NO GLASS WILL BE USED IN GUARD RAIL CONSTRUCTION.

CAST STONE SMOOTH

DECORATIVE IRON GRILLES IN RECESSED CAST STONE PANELS

BUILDING SIGN BAND LOCATION - SIZED TO MEET ORDINANCE REQUIREMENTS

CAST STONE SMOOTH

DECORATIVE IRON GRILLES IN RECESSED CAST STONE PANELS

CAST STONE OR GRANITE BASE

**G3 NORTH ELEVATION**  
SCALE - 1/16" = 1'-0"

TRANSPARENT MATERIAL TABULATIONS FOR SOUTH ELEVATION

TRANSPARENT AREAS	1ST LEVEL	2ND THRU 4TH LEVELS
TRANSPARENT AREA	584	1463
WALL AREA	238	4410
TOTAL AREA	622	5873
PERCENTAGE	7%	67%

• EACH ELEVATION IS TABULATED INDEPENDENTLY  
• FIRST LEVEL FLOOR AREA IS TABULATED FROM 1'-0" TO 8'-0" ABOVE GRADE

CAST STONE SMOOTH

PREFIN. ALUM. WINDOW W/ TINTED LOW-E GLASS

PREFIN. METAL GUARDRAIL

CAST STONE ACCENTS - SMOOTH

TENANT SIGN BAND LOCATION - SIZED TO MEET ORDINANCE REQUIREMENTS (TYP.)

PREFIN. METAL CANOPY (TYP.)

PREFIN. ALUM. WINDOW W/ CLEAR LOW-E GLASS

**G1 SOUTH (BROWN ROAD) ELEVATION**  
SCALE - 1/16" = 1'-0"

PREFIN. DECORATIVE METAL FENCE AT RESTAURANT PATIO  
WALL SPOONIE LUMINAIRE (TYP.)  
TENANT SIGN BAND LOCATION - SIZED TO MEET ORDINANCE REQUIREMENTS (TYP.)

NOTE: CAST STONE WALL IS SHOWN COMPOSED OF 24" WIDE X 12" HIGH STONE PANELS IN A RUNNING BOND PATTERN. TYPICAL.

Sheet Size - 24x36

AMENDED FINAL SPR 12/08/2015  
REVISED 12/02/2010  
AMENDED FINAL SPR 11/13/2010  
VARIANCE 08/16/2010  
AMENDED FINAL SPR 07/23/2010  
DATE ISSUED

DRAWN BY TLP  
CHECKED BY

**BALMORAL PLACE**  
AMENDED FINAL SITE PLAN SUBMITTAL  
Owner: Woodward Brown Associates, LLC  
Site: 34901-34953 Woodward Avenue, Birmingham, Michigan 48101

**HOBBES + BLACK ARCHITECTS**  
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**ELEVATIONS**  
SHEET TITLE

**10-803**  
PROJECT NUMBER

**A-005**  
SHEET NUMBER

BALMORAL PLACE - AMENDED FINAL SITE PLAN SUBMITTAL



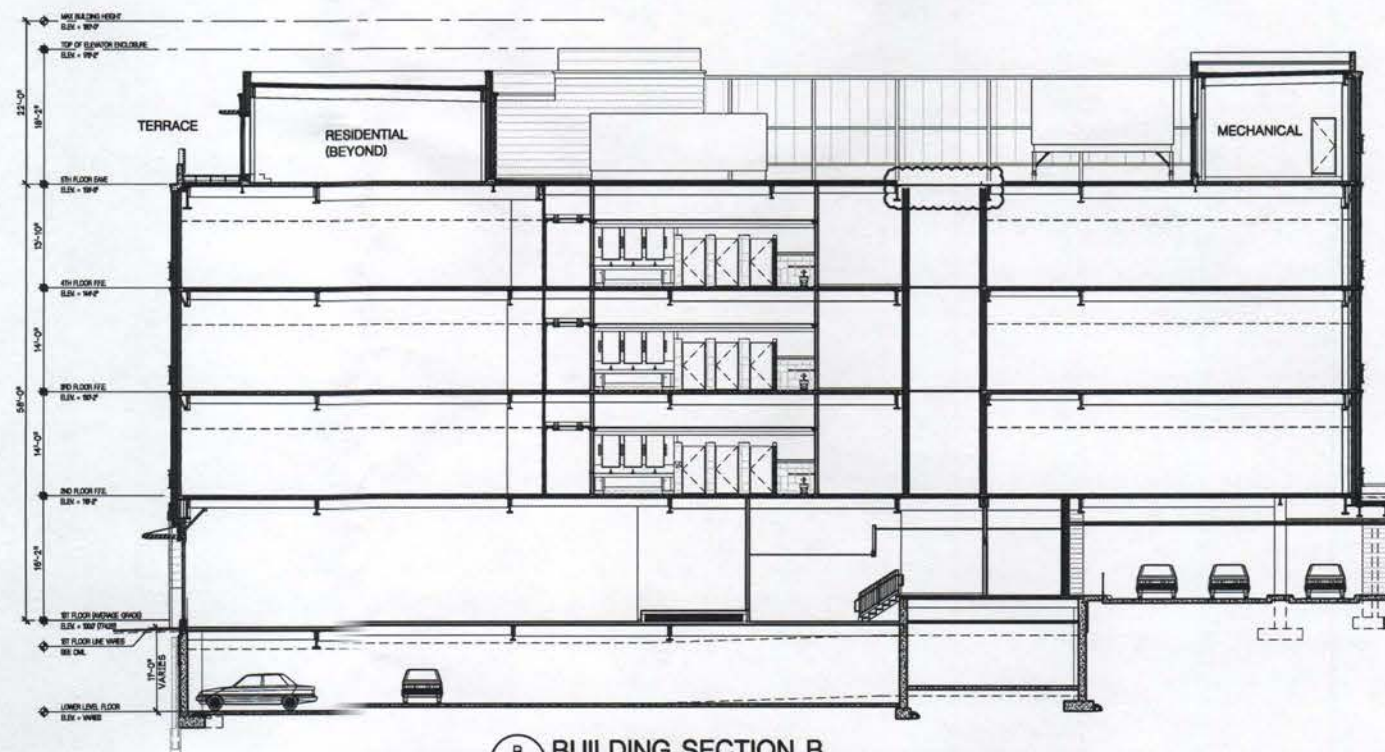
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**BALMORAL  
PLACE**  
AMENDED FINAL  
SITE PLAN  
SUBMITTAL

Owner:  
Woodward Brown  
Associates, LLC  
Site: 34901-34953  
Woodward Avenue,  
Birmingham,  
Michigan 48101

PROJECT

**BUILDING SECTION A**  
SCALE - 1/10" = 1'-0"



**B BUILDING SECTION B**  
SCALE - 1/10" = 1'-0"



CK ARCHITECTS

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Ann Arbor, Michigan 48104  
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CONSULTANT

1 BALMORAL PLACE - AMENDED FINAL SITE PLAN SUBMITTAL 2

## BUILDING SECTIONS

SHEET TITLE

10-803

PROJECT NUMBER

A-006

SHEET NUMBER



Drawing: C:\C:\DRIVE FILES\WOODWORK\WOODWORK\BALMORAL STUDIES 20\BALMORAL FINAL SITE PLAN\WOODWORK.Dwg  
Date: Jan 12, 2015, 5:11 PM  
Layout: 24x36  
Printed by: jgibbs

#### NOTES

- SEE MH COLUMN OF LUMINAIRE LOCATIONS FOR MOUNTING HEIGHTS.
- SEE LUMINAIRE SCHEDULE FOR LIGHT LOSS FACTORS.
- CALCULATIONS ARE SHOWN IN FOOTCANDLES AT GRADE/FLOOR SURFACE.
- ROOM REFLECTANCES ARE 25/25/10.
- CEILING HEIGHT IS 12' AFF IN DRIVE-THRU AREA.

THE ENGINEER AND/OR ARCHITECT MUST DETERMINE APPLICABILITY OF THE LAYOUT TO EXISTING / FUTURE FIELD CONDITIONS. THIS LIGHTING LAYOUT REPRESENTS ILLUMINATION LEVELS CALCULATED FROM LABORATORY DATA TAKEN UNDER CONTROLLED CONDITIONS IN ACCORDANCE WITH ILLUMINATING ENGINEERING SOCIETY APPROVED METHODS. ACTUAL PERFORMANCE OF ANY MANUFACTURER'S LUMINAIRE MAY VARY DUE TO VARIATION IN ELECTRICAL VOLTAGE, TOLERANCE IN LAMPS, AND OTHER VARIABLE FIELD CONDITIONS. MOUNTING HEIGHTS INDICATED ARE FROM GRADE AND/OR FLOOR UP. GBA DOES NOT ACT AS THE CIVIL OR STRUCTURAL ENGINEER AND DOES NOT DETERMINE BASE REQUIREMENTS. POLE SPECIFICATIONS ARE NOT INCLUDED WITH EXTERIOR LIGHTING PHOTOMETRIC ANALYSIS. THESE LIGHTING CALCULATIONS ARE NOT A SUBSTITUTE FOR INDEPENDENT ENGINEERING ANALYSIS OF LIGHTING SYSTEM SUITABILITY AND SAFETY. THE ENGINEER AND/OR ARCHITECT IS RESPONSIBLE TO REVIEW FOR MICHIGAN ENERGY CODE AND LIGHTING QUALITY COMPLIANCE.

ABBREVIATION KEY	
1	Length
2	Width
3	Height
4	Area
5	Volume
6	Weight
7	Temperature
8	Pressure
9	Speed
10	Acceleration
11	Force
12	Moment
13	Power
14	Energy
15	Frequency
16	Wavelength
17	Angle
18	Direction
19	Rotation
20	Curvature
21	Radius
22	Diameter
23	Circumference
24	Area
25	Volume
26	Weight
27	Temperature
28	Pressure
29	Speed
30	Acceleration
31	Force
32	Moment
33	Power
34	Energy
35	Frequency
36	Wavelength
37	Angle
38	Direction
39	Rotation
40	Curvature
41	Radius
42	Diameter
43	Circumference
44	Area
45	Volume
46	Weight
47	Temperature
48	Pressure
49	Speed
50	Acceleration
51	Force
52	Moment
53	Power
54	Energy
55	Frequency
56	Wavelength
57	Angle
58	Direction
59	Rotation
60	Curvature
61	Radius
62	Diameter
63	Circumference
64	Area
65	Volume
66	Weight
67	Temperature
68	Pressure
69	Speed
70	Acceleration
71	Force
72	Moment
73	Power
74	Energy
75	Frequency
76	Wavelength
77	Angle
78	Direction
79	Rotation
80	Curvature
81	Radius
82	Diameter
83	Circumference
84	Area
85	Volume
86	Weight
87	Temperature
88	Pressure
89	Speed
90	Acceleration
91	Force
92	Moment
93	Power
94	Energy
95	Frequency
96	Wavelength
97	Angle
98	Direction
99	Rotation
100	Curvature

PHOTOMETRIC and BIM	
1	Complete BIM Building Information Modeling and all associated data for data visualization is downloaded from www.dalighting.com
2	Report
3	Report
4	Report
5	Report
6	Report
7	Report
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97	Report
98	Report
99	Report
100	Report

LUMINAIRE SCHEDULE	
1	Symbol
2	Label
3	Qty
4	Catalog Number
5	Description
6	Lamp
7	File
8	Lumens
9	LLF
10	Watts
11	Symbol
12	Label
13	Qty
14	Catalog Number
15	Description
16	Lamp
17	File
18	Lumens
19	LLF
20	Watts
21	Symbol
22	Label
23	Qty
24	Catalog Number
25	Description
26	Lamp
27	File
28	Lumens
29	LLF
30	Watts
31	Symbol
32	Label
33	Qty
34	Catalog Number
35	Description
36	Lamp
37	File
38	Lumens
39	LLF
40	Watts
41	Symbol
42	Label
43	Qty
44	Catalog Number
45	Description
46	Lamp
47	File
48	Lumens
49	LLF
50	Watts
51	Symbol
52	Label
53	Qty
54	Catalog Number
55	Description
56	Lamp
57	File
58	Lumens
59	LLF
60	Watts
61	Symbol
62	Label
63	Qty
64	Catalog Number
65	Description
66	Lamp
67	File
68	Lumens
69	LLF
70	Watts
71	Symbol
72	Label
73	Qty
74	Catalog Number
75	Description
76	Lamp
77	File
78	Lumens
79	LLF
80	Watts
81	Symbol
82	Label
83	Qty
84	Catalog Number
85	Description
86	Lamp
87	File
88	Lumens
89	LLF
90	Watts
91	Symbol
92	Label
93	Qty
94	Catalog Number
95	Description
96	Lamp
97	File
98	Lumens
99	LLF
100	Watts

STATISTICS	
1	Description
2	Symbol
3	Avg
4	Max
5	Min
6	Max/Min
7	Avg/Min
8	Description
9	Symbol
10	Avg
11	Max
12	Min
13	Max/Min
14	Avg/Min
15	Description
16	Symbol
17	Avg
18	Max
19	Min
20	Max/Min
21	Avg/Min
22	Description
23	Symbol
24	Avg
25	Max
26	Min
27	Max/Min
28	Avg/Min
29	Description
30	Symbol
31	Avg
32	Max
33	Min
34	Max/Min
35	Avg/Min
36	Description
37	Symbol
38	Avg
39	Max
40	Min
41	Max/Min
42	Avg/Min
43	Description
44	Symbol
45	Avg
46	Max
47	Min
48	Max/Min
49	Avg/Min
50	Description
51	Symbol
52	Avg
53	Max
54	Min
55	Max/Min
56	Avg/Min
57	Description
58	Symbol
59	Avg
60	Max
61	Min
62	Max/Min
63	Avg/Min
64	Description
65	Symbol
66	Avg
67	Max
68	Min
69	Max/Min
70	Avg/Min
71	Description
72	Symbol
73	Avg
74	Max
75	Min
76	Max/Min
77	Avg/Min
78	Description
79	Symbol
80	Avg
81	Max
82	Min
83	Max/Min
84	Avg/Min
85	Description
86	Symbol
87	Avg
88	Max
89	Min
90	Max/Min
91	Avg/Min
92	Description
93	Symbol
94	Avg
95	Max
96	Min
97	Max/Min
98	Avg/Min
99	Description
100	Symbol

LUMINAIRE LOCATIONS	
1	No.
2	Label
3	X
4	Y
5	MH
6	Orientation
7	TIR
8	No.
9	Label
10	X
11	Y
12	MH
13	Orientation
14	TIR
15	No.
16	Label
17	X
18	Y
19	MH
20	Orientation
21	TIR
22	No.
23	Label
24	X
25	Y
26	MH
27	Orientation
28	TIR
29	No.
30	Label
31	X
32	Y
33	MH
34	Orientation
35	TIR
36	No.
37	Label
38	X
39	Y
40	MH
41	Orientation
42	TIR
43	No.
44	Label
45	X
46	Y
47	MH
48	Orientation
49	TIR
50	No.
51	Label
52	X
53	Y
54	MH
55	Orientation
56	TIR
57	No.
58	Label
59	X
60	Y
61	MH
62	Orientation
63	TIR
64	No.
65	Label
66	X
67	Y
68	MH
69	Orientation
70	TIR
71	No.
72	Label
73	X
74	Y
75	MH
76	Orientation
77	TIR
78	No.
79	Label
80	X
81	Y
82	MH
83	Orientation
84	TIR
85	No.
86	Label
87	X
88	Y
89	MH
90	Orientation
91	TIR
92	No.
93	Label
94	X
95	Y
96	MH
97	Orientation
98	TIR
99	No.
100	Label

LUMINAIRE SCHEDULE	
1	Symbol
2	Label
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6	Lamp
7	File
8	Lumens
9	LLF
10	Watts
11	Symbol
12	Label
13	Qty
14	Catalog Number
15	Description
16	Lamp
17	File
18	Lumens
19	LLF
20	Watts
21	Symbol
22	Label
23	Qty
24	Catalog Number
25	Description
26	Lamp
27	File
28	Lumens
29	LLF
30	Watts
31	Symbol
32	Label
33	Qty
34	Catalog Number
35	Description
36	Lamp
37	File
38	Lumens
39	LLF
40	Watts
41	Symbol
42	Label
43	Qty
44	Catalog Number
45	Description
46	Lamp
47	File
48	Lumens
49	LLF
50	Watts
51	Symbol
52	Label
53	Qty
54	Catalog Number
55	Description
56	Lamp
57	File
58	Lumens
59	LLF
60	Watts
61	Symbol
62	Label
63	Qty
64	Catalog Number
65	Description
66	Lamp
67	File
68	Lumens
69	LLF
70	Watts
71	Symbol
72	Label
73	Qty
74	Catalog Number
75	Description
76	Lamp
77	File
78	Lumens
79	LLF
80	Watts
81	Symbol
82	Label
83	Qty
84	Catalog Number
85	Description
86	Lamp
87	File
88	Lumens
89	LLF
90	Watts
91	Symbol
92	Label
93	Qty
94	Catalog Number
95	Description
96	Lamp
97	File
98	Lumens
99	LLF
100	Watts

ABBREVIATION KEY	
	Description
1	avg
D	Deg (degrees from min to max of total)
MAC	Max/min (degrees from min to max of total)
Learning Objectives (LOs)	
F20	Find the slope, 2011 base, F.8
F24	Find the slope, 2011 base, F.8
F28	Find the slope, 2011 base, F.8
F32	Find the slope, 2011 base, F.8
F36	Find the slope, 2011 base, F.8
F40	Find the slope, 2011 base, F.8
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F100	Find the slope, 2011 base, F.8
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F468	Find the slope, 2011 base, F.8
F472	Find the slope, 2011 base, F.8
F476	Find the slope, 2011 base, F.8
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F504	Find the slope, 2011 base, F.8
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F572	Find the slope, 2011 base, F.8
F576	Find the slope, 2011 base, F.8
F580	Find the slope, 2011 base, F.8
F584	Find the slope, 2011 base, F.8
F588	Find the slope, 2011 base, F.8
F592	Find the slope, 2011 base, F.8
F596	Find the slope, 2011 base, F.8
F600	Find the slope, 2011 base, F.8
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F612	Find the slope, 2011 base, F.8
F616	Find the slope, 2011 base, F.8
F620	Find the slope, 2011 base, F.8
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F636	Find the slope, 2011 base, F.8
F640	Find the slope, 2011 base, F.8
F644	Find the slope, 2011 base, F.8
F648	Find the slope, 2011 base, F.8
F652	Find the slope, 2011 base, F.8
F656	Find the slope, 2011 base, F.8
F660	Find the slope, 2011 base, F.8
F664	Find the slope, 2011 base, F.8
F668	Find the slope, 2011 base, F.8
F672	Find the slope, 2011 base, F.8
F676	Find the slope, 2011 base, F.8
F680	Find the slope, 2011 base, F.8
F684	Find the slope, 2011 base, F.8
F688	Find the slope, 2011 base, F.8
F692	Find the slope, 2011 base, F.8
F696	Find the slope, 2011 base, F.8
F700	Find the slope, 2011 base, F.8
F704	Find the slope, 2011 base, F.8
F708	Find the slope, 2011 base, F.8
F712	Find the slope, 2011 base, F.8
F716	Find the slope, 2011 base, F.8
F720	Find the slope, 2011 base, F.8
F724	Find the slope, 2011 base, F.8
F728	Find the slope, 2011 base, F.8
F732	Find the slope, 2011 base, F.8
F736	Find the slope, 2011 base, F.8
F740	Find the slope, 2011 base, F.8
F744	Find the slope, 2011 base, F.8
F748	Find the slope, 2011 base, F.8
F752	Find the slope, 2011 base, F.8
F756	Find the slope, 2011 base, F.8
F760	Find the slope, 2011 base, F.8
F764	Find the slope, 2011 base, F.8
F768	Find the slope, 2011 base, F.8
F772	Find the slope, 2011 base, F.8
F776	Find the slope, 2011 base, F.8
F780	Find the slope, 2011 base, F.8
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F788	Find the slope, 2011 base, F.8
F792	Find the slope, 2011 base, F.8
F796	Find the slope, 2011 base, F.8
F800	Find the slope, 2011 base, F.8
F804	Find the slope, 2011 base, F.8
F808	Find the slope, 2011 base, F.8
F812	Find the slope, 2011 base, F.8
F816	Find the slope, 2011 base, F.8
F820	Find the slope, 2011 base, F.8
F824	Find the slope, 2011 base, F.8
F828	Find the slope, 2011 base, F.8
F832	Find the slope, 2011 base, F.8
F836	Find the slope, 2011 base, F.8
F840	Find the slope, 2011 base, F.8
F844	Find the slope, 2011 base, F.8
F848	Find the slope, 2011 base, F.8
F852	Find the slope, 2011 base, F.8
F856	Find the slope, 2011 base, F.8
F860	Find the slope, 2011 base, F.8
F864	Find the slope, 2011 base, F.8
F868	Find the slope, 2011 base, F.8
F872	Find the slope, 2011 base, F.8
F876	Find the slope, 2011 base, F.8
F880	Find the slope, 2011 base, F.8
F884	Find the slope, 2011 base, F.8
F888	Find the slope, 2011 base, F.8
F892	Find the slope, 2011 base, F.8
F896	Find the slope, 2011 base, F.8
F900	Find the slope, 2011 base, F.8
F904	Find the slope, 2011 base, F.8
F908	Find the slope, 2011 base, F.8
F912	Find the slope, 2011 base, F.8
F916	Find the slope, 2011 base, F.8
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F928	Find the slope, 2011 base, F.8
F932	Find the slope, 2011 base, F.8
F936	Find the slope, 2011 base, F.8
F940	Find the slope, 2011 base, F.8
F944	Find the slope, 2011 base, F.8
F948	Find the slope, 2011 base, F.8
F952	Find the slope, 2011 base, F.8
F956	Find the slope, 2011 base, F.8
F960	Find the slope, 2011 base, F.8
F964	Find the slope, 2011 base, F.8
F968	Find the slope, 2011 base, F.8
F972	Find the slope, 2011 base, F.8
F976	Find the slope, 2011 base, F.8
F980	Find the slope, 2011 base, F.8
F984	Find the slope, 2011 base, F.8
F988	Find the slope, 2011 base, F.8
F992	Find the slope, 2011 base, F.8
F996	Find the slope, 2011 base, F.8
F1000	Find the slope, 2011 base, F.8



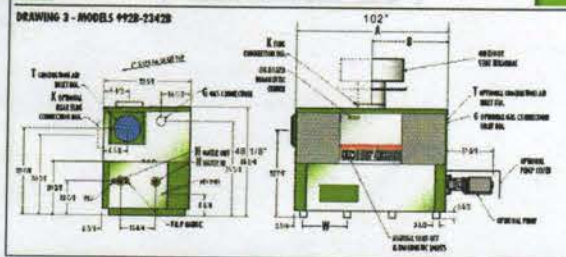
## Engineering Data

### FXV Closed Circuit Cooling Towers



## Engineering Data

- Single Inlet .....1
- Dual Air Inlet .....2
- Structural Support .....5



## Energy Recovery Ventilator with Heating

### Model ERH

- Commercial • Institutional

- 10,000 cfm
- 1.5 in. wg external static pressure

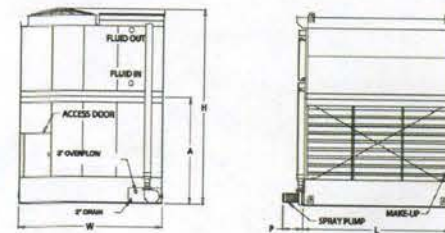


**GREENHECK**  
Building Value in Air

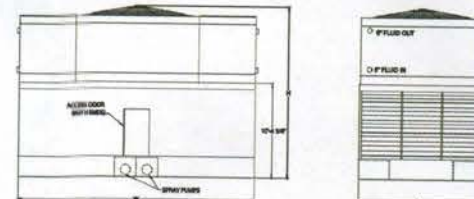
April 2005

## Dimensional Reference Drawings

FXV Engineering Data



Single Air Inlet Models



Dual Air Inlet Models

**Baltimore Aircoil Company**

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## Engineering Data:

### Cold Weather Operation

#### FXV Heat Loss Data (BTU/H)

Model	Heat Loss	Heat Loss	Heat Loss	Heat Loss
FXV-100	17,500	47,700	52,300	52,300
FXV-150	25,200	67,000	75,000	75,000
FXV-200	32,900	87,300	97,000	97,000
FXV-250	40,600	107,600	119,000	119,000
FXV-300	48,300	127,900	142,000	142,000
FXV-350	56,000	148,200	165,000	165,000
FXV-400	63,700	168,500	188,000	188,000
FXV-450	71,400	188,800	211,000	211,000
FXV-500	79,100	209,100	234,000	234,000
FXV-550	86,800	229,400	257,000	257,000
FXV-600	94,500	249,700	280,000	280,000
FXV-650	102,200	270,000	303,000	303,000
FXV-700	110,000	290,300	326,000	326,000
FXV-750	117,700	310,600	349,000	349,000
FXV-800	125,400	330,900	372,000	372,000
FXV-850	133,100	351,200	395,000	395,000
FXV-900	140,800	371,500	418,000	418,000
FXV-950	148,500	391,800	441,000	441,000
FXV-1000	156,200	412,100	464,000	464,000

Notes:  
1. Heat loss based on 50°F entering air and 40°F wet-bulb air.  
2. One inch thick PIR (polyisocyanurate) insulation on both the PCD face and the leaving panels surrounding the coil.

## Dimensional Data of Positive Closure Damper Hood

Model	Width	Height	Weight	Weight	Weight
FXV-100	36"	48"	15.0	15.0	15.0
FXV-150	48"	60"	22.5	22.5	22.5
FXV-200	60"	72"	30.0	30.0	30.0
FXV-250	72"	84"	37.5	37.5	37.5
FXV-300	84"	96"	45.0	45.0	45.0
FXV-350	96"	108"	52.5	52.5	52.5
FXV-400	108"	120"	60.0	60.0	60.0
FXV-450	120"	132"	67.5	67.5	67.5
FXV-500	132"	144"	75.0	75.0	75.0
FXV-550	144"	156"	82.5	82.5	82.5
FXV-600	156"	168"	90.0	90.0	90.0
FXV-650	168"	180"	97.5	97.5	97.5
FXV-700	180"	192"	105.0	105.0	105.0
FXV-750	192"	204"	112.5	112.5	112.5
FXV-800	204"	216"	120.0	120.0	120.0
FXV-850	216"	228"	127.5	127.5	127.5
FXV-900	228"	240"	135.0	135.0	135.0
FXV-950	240"	252"	142.5	142.5	142.5
FXV-1000	252"	264"	150.0	150.0	150.0

Notes:  
1. Hood weight includes shipping and handling weight.

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## Product Features



### Model ERH

#### Energy Recovery Ventilator With Heating

With the ERH, you save money with the total energy wheel and get the convenience of supplemental heating. The ERH provides pre-conditioned air in the summer and space heating air in the winter which is ideal if your climate has prominent summer and winter seasons. For applications requiring cooling, refer to Model ERCH.

Total Energy Wheel recovers energy from the exhaust air stream and transfers it to the supply air stream. The wheel is constructed of a light-weight polymer and a permanently bonded silica gel desiccant. Right wheel segments are easily removable for cleaning.

Energy recovery wheels certified by the Air-Conditioning and Refrigeration Institute (ARI) and the National Air Conditioning Engineers (NACE) are available in the Certified Product Directory of Manufacturers and Suppliers.

Optional Tempering Section provides supplemental heating of the outdoor air after pre-conditioning by the energy wheel. Tempering options include indirect gas, hot water, and electric.



You Get a Product that is Simple to Operate:

- Fresh outdoor air is pre-conditioned by the total energy wheel, recovering a majority of the energy from the exhaust air.
- Saves 3-4 tons of cooling per 1000 cfm.
- Saves 50-60 MBtu of heating per 1000 cfm.
- The heating section further tempers cold outdoor air to desired conditions.

You Get a Product that is Flexible:

- Four housing sizes.
- Airflow capacities up to 10,000 cfm.
- External static pressures up to 1.5 in. wg.



Model ERH shown with optional indirect gas heating, supply return, and exhaust fans.

#### Standard Construction

- g Forward Curved Blowers & Motor Assemblies mounted on isolation bases with response vibration isolators for smooth, quiet operation.
- g A Control Box furnished with motor starters for the supply blower, exhaust blower, and energy wheel motors.
- g Double Wall Housing which provides a galvanized steel-lined interior to prevent exposure of the insulation to the elements.
- g Access Doors to blowers, filters, energy wheel, and heater section to simplify inspection and routine service.
- g Single Point Wiring with a disconnect switch and low voltage terminal strip.



## COOPER-FIN H" BOILER PRODUCT SUMMARY

### (CIN) 400,000 - 2,870,000 BTU/H

Model	Q1-001	Q1-002	Q1-003	Q1-004	Q1-005	Q1-006	Q1-007	Q1-008	Q1-009	Q1-010
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR) (2)	0.6	0.8	0.8	0.8	0.8	0.8	0.8	0.8	0.8	0.8
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
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Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
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Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
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Capacity (COP/HR)	1.5	3.7	1.3	5.5	3.4	3.7	6.1	6.5	6.8	
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