

VIRTUAL MEETING OF THE BIRMINGHAM PLANNING BOARD
WEDNESDAY, OCTOBER 14, 2020
7:30 PM

<https://zoom.us/j/111656967> or dial: 877-853-5247 Toll-Free, Meeting Code: 111656967

- A. Roll Call
- B. Review and Approval of the Minutes of the regular meeting of **September 23, 2020**
- C. Chairpersons' Comments
- D. Review of the Agenda

- E. Study Session Items
Rules of Procedure for Study Sessions: Site Plan and Design Review, Special Land Use Permit Review and other review decisions will not be made during study sessions; Each person (member of the public) will be allowed to speak at the end of the study session; Each person will be allowed to speak only once; The length of time for each person to speak will be decided by the Chairman at the beginning of the meeting; Board members may seek information from the public at any time during the meeting.
 - 1. **Review of First Draft of 2040 Birmingham Plan**
 - **Mixed Use Districts**
 - **Haynes Square** (pages 200 - 207)
 - **S. Woodward Gateway** (pages 208 - 223)
 - **Rail District** (pages 224 – 232)

- F. Miscellaneous Business and Communications:
 - a. Communications
 - b. Administrative Approval Correspondence
 - c. Draft Agenda for the next Regular Planning Board Meeting (**October 28, 2020**)
 - d. Other Business

- G. Planning Division Action Items
 - a. Staff Report on Previous Requests
 - b. Additional Items from tonight's meeting

- H. Adjournment

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(Title VI of the Civil Rights Act of 1964).

CITY OF BIRMINGHAM
REGULAR MEETING OF THE PLANNING BOARD
WEDNESDAY, SEPTEMBER 23, 2020

Held Remotely Via Zoom And Telephone Access

Minutes of the regular meeting of the City of Birmingham Planning Board held on September 23, 2020. Chairman Scott Clein convened the meeting at 7:32 p.m.

A. Roll Call

Present: Chairman Scott Clein; Board Members Robin Boyle, Stuart Jeffares, Bert Koseck, Daniel Share, Bryan Williams; Alternate Board Members Jason Emerine, Nasseem Ramin

Absent: Board Member Janelle Whipple-Boyce; Student Representatives Rachel Hester, June Lee

Administration: Jana Ecker, Planning Director
Jamil Alawadi, IT Staff
Nicholas Dupuis, City Planner
Laura Eichenhorn, Transcriptionist

09-116-20

B. Approval Of The Minutes Of The Regular Planning Board Meeting of September 9, 2020

Mr. Williams and Mr. Share determined that 'Park' needed to be added after 'Booth' on page four of the minutes.

Mr. Share also recommended that additional context be added on page six to the fourth full paragraph down. He suggested the beginning of that paragraph be changed to "In reply to Planning Director Ecker concerning whether an alternate Planning Board member can serve on an ad hoc committee for a project that will come before the Planning Board," with the rest of the paragraph remaining as-is.

Motion by Mr. Share

Seconded by Mr. Williams to approve the minutes of the Regular Planning Board Meeting of September 9, 2020 as amended.

Motion carried, 5-0.

ROLL CALL VOTE

Yeas: Share, Williams, Clein, Jeffares, Emerine

Nays: None

Abstain: Boyle, Koseck

09-117-20

C. Chairperson's Comments

Chairman Clein welcomed everyone to the meeting and reminded everyone that the meeting was being held under the guidance of the City Attorney and City administration to ensure compliance with Governor Whitmer's executive orders. Chairman Clein then reviewed procedures for the meeting.

09-118-20

D. Review Of The Agenda

There were no changes to the agenda.

09-119-20

E. Special Land Use Permit Amendment and Final Site Plan & Design Review

1. 768 N. Old Woodward, The French Lady (Former LY Sushi) – Request for Special Land Use Permit Amendment to operate a food and drink establishment (no alcohol) in an O2 zone.

2. 768 N. Old Woodward, The French Lady (Former LY Sushi) – Request for Final Site Plan & Design Review for design changes to operate a food and drink establishment (no alcohol) in an O2 zone.

City Planner Dupuis presented the item.

In reply to Chairman Clein, City Planner Dupuis stated that 768 N. Old Woodward is not adjacent to any residences.

Claude Pellerin, owner, was present on behalf of the application. Ms. Pellerin confirmed she had a written agreement with the owner of the dumpster in the rear parking lot that permits use by her restaurant. She said her staff would access the dumpster by exiting the restaurant using the rear stairs.

Seeing no further questions from the Board for Ms. Pellerin, Chairman Clein invited public comment.

Seeing no public comment, Chairman Clein invited Board discussion of the item.

Mr. Jeffares stated he visited the site during the mid-afternoon of the day of the present meeting. He said he appreciated that the restaurant was not immediately adjacent to another restaurant, that it was a cuisine Birmingham did not currently have, and that the size of was appropriate for the intent of the restaurant. He also said that if there was ever outdoor seating on the rear deck that it would be the best restaurant seating in all of Birmingham because of the view. Mr. Jeffares said he was not presently concerned about the dumpster being insufficiently screened, especially

since the owner was a third party. He recommended the Board keep the issue in mind for the future with an aim towards getting it screened. He also noted that public access to the rear of the building is so limited that very few members of the public would ever have an opportunity to see the unscreened dumpster. Mr. Jeffares concluded by saying he could not see the rooftop mechanical units from the ground level.

Motion by Mr. Jeffares

Seconded by Mr. Williams to recommend approval to the City Commission for the Final Site Plan and Design Review for 768 N. Old Woodward – The French Lady.

Motion carried, 6-0.

ROLL CALL VOTE

Yeas: Williams, Clein, Jeffares, Emerine, Boyle, Koseck

Nays: None

Absent: Share (lost connection during vote), Whipple-Boyce

Motion by Mr. Jeffares

Seconded by Mr. Williams to recommend approval to the City Commission for the Special Land Use Permit for 768 N. Old Woodward – The French Lady.

In Mr. Share's absence, Ms. Ramin voted in the matter of the Special Land Use Permit for The French Lady.

Motion carried, 7-0.

ROLL CALL VOTE

Yeas: Williams, Clein, Jeffares, Emerine, Boyle, Koseck, Ramin

Nays: None

Absent: Share (lost connection during vote), Whipple-Boyce

Mr. Boyle commented that the Board should consider lengthening the time before a legal non-conforming use lapses from six months to a year in light of the Covid-19 pandemic. He noted that doing so would reduce costs and bureaucratic requirements for small businesses during a time when the City is trying to offer small businesses financial support.

Planning Director Ecker explained that the Zoning Ordinance would have to be amended to lengthen the time from six months to a year.

Chairman Clein noted that Mr. Boyle's comment would be minuted. He proposed that the City Commission consider taking Mr. Boyle's recommendation under advisement.

Mr. Williams suggested that Mr. Boyle's proposal could be one of the items discussed at the upcoming joint City Commission-Planning Board meeting.

09-120-20

F. Final Site Plan & Design Review

1. 469 – 479 S. Old Woodward, Project M1 (Former Mountain King and Talmer Bank) – Request for Final Site Plan & Design Review for new five story mixed use building.

Mr. Share was able to rejoin the meeting at the beginning of this item.

City Planner Dupuis presented the item.

Since there was a possibility the owner would pursue a variance from the Board of Zoning Appeals (BZA) for a reduced setback along Hazel, Mr. Williams said it would be important for the BZA to understand that 469-479 S. Old Woodward is the only parcel in the City zoned D4 and not included in the Parking Assessment District (PAD). He said this fact represents a hardship for the applicant.

Mr. Emerine noted that the building also has three frontages, which also represents a hardship.

Mr. Williams agreed with Mr. Emerine.

Mr. Jeffares said he agreed with Mr. Williams and Mr. Emerine as well. He added that if one stood on the property of 469-479 S. Old Woodward and looked across Hazel, all one would see is a concrete and stone wall. Positing that the Hazel frontage was unlikely to attract a retailer because of the view, Mr. Jeffares recommended that the BZA consider this an additional reason for granting a variance for a lesser setback along Hazel.

Duraid Markus, owner, Chris Longe, architect for the project, and Rick Rattner, attorney for the project, were present.

Mr. Longe said the project team was considering putting in storage on the second floor with residential units along the perimeter, a community room, and a workout center. He said this would be the best use of space since the north side of the building faces a wall.

A number of members of the Board stated they were not comfortable with an either/or approval for residential or commercial use on the second floor.

Mr. Markus stated the final site plan and design review should then proceed using the residential schematic for the second floor.

In reply to Mr. Markus, Chairman Clein confirmed that the project could change its second floor use from residential to office through an administrative approval in the future if need be. Chairman Clein explained that would be permissible since both uses are allowed in the D4 zone. He said that, if the use were to change, only attendant changes to the exterior or issues with ordinance compliance would require further Board review.

Mr. Longe said the loading area on the Woodward side of the building has a bay that is 18 feet wide and 40 feet deep. He said the area should be more than sufficient for the anticipated loading

needs of the residences in the building. Mr. Longe acknowledged that the project would need a variance from the BZA for the loading area.

In reply to Chairman Clein, Mr. Longe agreed with Mr. Jeffares' assessment that the Hazel side of the building would be a poor location for retail frontage. The concrete wall across Hazel prevents the street from being activated, and Mr. Longe opined that no amount of retail within their building along Hazel would change that. He noted that since the building is not part of the PAD all parking must be provided for on-site. If they were required to add more retail, they would be required to provide even more parking which would be difficult for the project. Mr. Longe emphasized that the project team wants to provide parking that is easily accessible for the retail customers. He confirmed that the project would be able to screen the parking along Hazel if the City requires it.

In response to a Board inquiry about the canopies, Mr. Longe confirmed that the proposed canopies meet the ordinance. He stated the specifications for the canopies were provided to City Planner Dupuis the day prior to the present meeting.

Mr. Longe told Mr. Koseck that the project team did intend to pursue admittance to the PAD.

In reply to Mr. Williams, Mr. Longe stated the transformer would be screened with metal coated to match the color of the building. He said the project team was also amenable to providing the required benches and trash receptacles.

Mr. Boyle recommended that the developer consider moving the residents' gym from the second floor to the ground floor on Hazel. Since the underground deck has parking in excess of its residential requirements, the parking on Hazel could potentially be removed for a more activating ground-floor use. He ventured that most customers looking to visit the building's retail would park on-street and that the extra spaces below ground could be counted towards the retail parking requirements. He concluded that if the project team pursued his recommended configuration then they would also be able to avoid seeking a variance for Hazel.

Mr. Markus said Mr. Boyle made an interesting point, and said it was one his team had considered. The drawback to Mr. Boyle's proposal was that enacting it would put retail patrons in the otherwise residential underground parking deck, which the developers wanted to avoid to maintain the residents' feeling of security.

Mr. Boyle said that while he understood Mr. Markus' concern, most people visiting retailers in Birmingham endeavor to park on the street even if accessible underground parking is available. Because of this, Mr. Boyle explained that the excess parking spaces underground could count towards the retail requirements even though retail customers would rarely use those spaces. He said he just wanted the developers to have more flexibility.

Mr. Markus stated the underground deck was ten cars overparked, and that if the Hazel Street parking was eliminated then the project would be four cars under-parked. He also said that the Hazel Street parking would be a draw to any retailer looking to occupy the other available retail space.

Mr. Boyle said he would not continue pressing his point, and that he just wanted to raise the possibility with the project team.

The Board concurred that they were comfortable with the proposed plans for the Hazel side and agreed that there would be no adverse land planning effects if the BZA were to grant a variance for a reduced setback on Hazel.

Mr. Share said it was important to note that the Board was comfortable with the proposed Hazel variance due to the unique location of the project and the unusual three sides with surrounding facilities.

Since the applicant submitted the requirements for conditions one, four and six of the motion to the Planning Department subsequent to the motion's writing, and since City Planner Dupuis had the opportunity to review those submissions, the Board agreed those items could be approved administratively instead of requiring Board review and approval at a later date.

Motion by Mr. Williams

Seconded by Mr. Boyle to approve the Final Site Plan and Design Review for 469-479 S. Old Woodward – Project M1 – subject to the following conditions:

- 1. The applicant's revised and submitted plans clarifying the projection of all terraces and the dimensions of the sidewalk to ensure the canopies meet the projection standards and shall be approved administratively by the Planning Division;**
- 2. The applicant must provide a 20 ft. minimum setback for the parking facility located on the first floor along the Hazel frontage, or obtain a variance from the Board of Zoning Appeals;**
- 3. The applicant must submit revised plans showing 2 off-street loading spaces measuring 40 feet long, 12 feet wide and 14 feet high and in compliance with Section 4.25 of the Zoning Ordinance or obtain a variance from the Board of Zoning Appeals;**
- 4. The applicant's submitted specifications on the types and placement of all proposed light fixtures shall be approved administratively by the Planning Division;**
- 5. The applicant must submit revised elevations and glazing calculations that show 70% glazing on each first floor facade, a maximum of 35% glazing on the upper floors, as well as material specifications for all new glass, or obtain a variance from the Board of Zoning Appeals;**
- 6. The applicant's Existing Conditions Plan and material specifications shall be approved administratively by the Planning Division;**
- 7. That the second floor use be designated as residential, per the submitted plans, with the appropriate parking requirements; and,**
- 8. The applicant must comply with the requests of all City departments.**

Mr. Koseck expressed appreciation for the project and said it aligned well with the goals of the 2016 Plan. He said it would be an asset to its location. Mentioning the variance issues, Mr. Koseck said he hoped the BZA would recognize the unique circumstances associated with the project. He also said that if the project gains admittance to the PAD then the currently proposed first-floor

parking could be changed into either more retail or more street-activating common spaces for the residents as per Mr. Boyle's prior suggestion. Mr. Koseck stated that everyone involved in bringing the project to fruition should be very proud, and said he looked forward to seeing the project built.

Motion carried, 7-0.

ROLL CALL VOTE

Yeas: Williams, Clein, Jeffares, Emerine, Boyle, Koseck, Share

Nays: None

Chairman Clein thanked Mr. Markus, Mr. Longe, Mr. Rattner and the project team for all their work on the project. He thanked the residents of the neighboring buildings for their long-term efforts towards ensuring that this project would be one that would benefit the community.

Mr. Markus told Mr. Boyle he was considering Mr. Boyle's recommendation. He said that if he could maintain his required number of parking spaces he would be interested in replacing some of the spots on Hazel with a more street-activating use.

The project team thanked the Board and signed off.

09-121-20

G. Revised Preliminary Site Plan Review

1. 35001 Woodward, The Maple (Vacant lot, Hunter House) – Request for Revised Preliminary Site Plan Review to revise first floor of new 5 story mixed use building to include surface parking.

Planning Director Ecker presented the item. She explained that City Attorney Currier advised that:

- If the Board were to approve the revised preliminary site plan they would have to specify that it should supersede the previously approved Preliminary Site Plan in their motion; and,
- If they were to deny the Revised Preliminary Site Plan, the previously approved Preliminary Site Plan would stand.

Mr. Williams said he could not recall a project located in the PAD ever receiving approval for surface parking within 20' of the building facades.

Kevin Biddison, architect, was present on behalf of the project.

In reply to Mr. Koseck, Mr. Biddison confirmed the project team looked into providing parking in the MDOT right-of-way, but determined there was not enough depth along the road due to the right-hand turn lane.

Seeing no further Board questions for the applicant, Chairman Clein invited public comment.

Kelly Cobb, owner of Hunter House Hamburgers, said he was aware that some members of the Planning Board felt that Hunter House is causing undue difficulty for the developers of The Maple. He said he was present to reiterate that he gets no pleasure from the difficulty he is encountering with the developers of The Maple, and wanted to see it resolved as expediently as everyone else. He explained that The Maple's developers need to offer solutions that would adequately address his issues with food truck parking, lack of sufficient interior storage for food, poor condition of the gravel lot, and Hunter House's current ADA and health code non-compliance. He said that while he appreciated The Maple's team's most recent efforts to provide enough parking for Hunter House, it still remained a problem that The Maple's development team continues to submit plans to the City without Hunter House's review or approval of the plans. Mr. Cobb noted that Hunter House's review and approval for plan submissions is required per the private agreement between the developers of The Maple and Hunter House.

Mr. Cobb said the Maple's most recently submitted plans have inadequate space allotted for the Hunter House. He noted that his private agreement with the development team of The Maple requires that Hunter House be provided enough space to continue their operations. He said the proposed plans do not include an adequate number of parking spaces for Hunter House. He said the proposed plans also do not conform to the use of the property as required by deed restrictions. Mr. Cobb said he is working tirelessly to find a solution that would work for both Hunter House and The Maple, but opined that neither the presently proposed site plan revision nor the previously approved site plan met those criteria. He said that given these issues, the Planning Board's work on reviewing this project will remain moot for as long as there is disagreement between himself and The Maple's developers. Mr. Cobb apologized for that fact, and thanked the Board members for their dedication and time.

While Chairman Clein thanked Mr. Cobb for his comments, he said he disagreed that any members of the Planning Board regretted Hunter House's involvement with this project. Chairman Clein added that the Board fully looked forward to Hunter House's continued operations. He continued that the Board was only endeavoring to make sure that the project was adhering to all City-required ordinances and standards, and that the Board was specifically taking no position on the private contract between the developers of The Maple and the owners of the Hunter House. Speaking on behalf of the Board, Chairman Clein said they wished both parties nothing but success.

Mr. Cobb thanked Chairman Clein. Adding one more comment, Mr. Cobb stated that in the entire time Hunter House has been in operation he had never heard of an issue with cars egressing the site onto Woodward. He said he respected F&V's analysis, and understood the difference between anecdote and data, but wanted to offer that perspective.

Seeing no further comment from the public, Chairman Clein returned the conversation to the Board.

Mr. Williams stated he would be moving fairly quickly to deny the Revised Preliminary Site Plan.

Mr. Share said his preference would be to postpone rather than deny. He said the key question was whether the Woodward egress, given the expected use by Hunter House customers, would

create an unsafe situation for pedestrians and cars. He said he would want F&V to address the issue of safety specifically before the Board acts on the revised preliminary site plan.

Mr. Koseck expressed frustration and disappointment with the Revised Preliminary Site Plan. He said the Board had spent an immense amount of time on this project, offering many of the same comments repeatedly, only for plans to be re-submitted with those comments seemingly disregarded. He said he did not see any attempt on the part of the project team to creatively work within the constraints of the Zoning Ordinance. Mr. Koseck continued that while he understood the project might not be able to meet every requirement of the zoning ordinance, there was no way he could ever support the plans as currently proposed. Citing his time on the BZA, Mr. Koseck opined that if this revised preliminary site plan had been before them for a review they would have declined to even look at the revised plans due to there being no change of facts. Praising Mr. Biddison's skill as an architect and the potential to have a valuable tenant in the Hunter House, Mr. Koseck said he could not understand how the presently submitted plans seemed to be going backwards in terms of progress. He concluded by saying that he obviously could not support the revised preliminary site plan moving forward.

Mr. Jeffares said that while he understood that Hunter House wanted surface parking provided, the City has dedicatedly avoided allowing surface parking in the downtown area. He noted that there have been other proposed projects delayed to future agendas because the Board has had to review this project again and again. He noted the Board's continued emphasis to applicants that they should resolve their issues with community stakeholders to the absolute best of their ability before submitting a project for Board review, and said the development team on this project has regularly failed to do that. Mr. Jeffares echoed Mr. Koseck's frustration, and said that while he was between postponing the project and denying it he was leaning towards denial.

Citing City Attorney Currier's guidance, Chairman Clein said that if the Board was between a postponement and denial then a denial would procedurally make more sense. He reiterated that a denial would leave the previously approved preliminary site plan standing, while a postponement to a date certain would hold a spot on a future agenda that could otherwise go to reviewing another project. Chairman Clein noted that traditionally the Board avoids denials where possible because of the repercussions for the applicant, but since this applicant already had an approved Preliminary Site Plan no harm would come from a denial in this case.

Chairman Clein continued by stating he would support a denial of the Revised Preliminary Site Plan because the proposed site plan does not meet the requirements of Article 7, Section 7.27 of the Zoning Ordinance, specifically (4) which states that "The site plan, and its relation to streets, driveways and sidewalks, shall be such as to not interfere with or be hazardous to vehicular and pedestrian traffic". He said the proposed revised site plan is completely contrary to the City's planning of its downtown and its 2016 Plan. Chairman Clein echoed Mr. Williams' previous observation that buildings in the PAD are not granted surface parking within 20' of the building's facades. He noted the Revised Preliminary Site Plan would affect three facades of the building in addition to the number of other issues raised by his colleagues on the Board.

Chairman Clein observed that the plan for The Maple includes multiple floors of residential, which will inevitably lead to a significant increase in pedestrian traffic in the area. In addition, with the exception of the Hunter House, the majority of the site has been vacant for years. Because of

these two factors, Chairman Clein stated anecdotal observation of the site cannot provide a good sense of what will result from the impending combined increase in both vehicular and pedestrian traffic. He acknowledged Mr. Cobb's statement that he had not seen any issues arising from vehicles exiting onto Woodward, but explained that the site will be so changed post-development that history cannot give a reliable indication of future safety in this case.

Mr. Emerine said he agreed with all of Chairman Clein's comments. He said the surface parking lot was unnecessary and created another egress point into the MDOT right-of-way which should be also avoided. He said he would not be supporting the Revised Preliminary Site Plan.

Motion by Mr. Williams

Seconded by Mr. Boyle to deny the Revised Preliminary Site Plan for 35001 & 35075 Woodward, The Maple, as the proposed site plan does not meet the requirements of Article 7, Section 7.27, specifically (4) which states that "The site plan, and its relation to streets, driveways and sidewalks, shall be such as to not interfere with or be hazardous to vehicular and pedestrian traffic."

Motion carried, 6-1.

ROLL CALL VOTE

Yeas: Williams, Clein, Jeffares, Emerine, Boyle, Koseck

Nays: Share

Chairman Clein thanked the applicant for their time. He stressed that the Board wants to see both The Maple and Hunter House be successful in Birmingham and encouraged the applicant renew their efforts once again towards resolving the matters between them.

09-122-20

H. Miscellaneous Business and Communications:

a. Communications

b. Administrative Approval Correspondence

Planning Director Ecker presented an administrative approval request for 526 Henrietta. She explained it was before the Board because it was zoned R8 for multi-family even though it is a single family home.

Glenda Meads, architect, was present on behalf of the request.

The applicant's first request was to enclose the second floor's porch with glass to create a 165 sq. ft. three-season room. The second was to convert the garage door into one double door from two single doors with a post in the middle.

In reply to Chairman Clein, Planning Director Ecker said she only brought the matter before the Board because the Board does not usually allow double garage doors on single-family homes. She explained that in the R8 zone, however, there is no ordinance prohibition on double garage doors. She said there were no ordinance issues present in the proposed plans.

Mr. Share noted that the architect for this application, Glenda Meads, was currently working on an addition for his home. He said he would take no part in the discussion.

Mr. Boyle said he was in favor of maintaining consistency across zoning districts for single family homes, and as a result did not think a double garage door should be approved.

Mr. Williams noted that the building is zoned R8, so a double garage door is permitted, and that the building is actually two separate homes with a shared wall. Consequently, he said he disagreed with Mr. Boyle's recommendation.

Mr. Boyle said Mr. Williams had a fair point.

Chairman Clein said that, while he respected Mr. Boyle's perspective, he would have a hard time justifying the enforcement of different zoning requirements across zones.

Mr. Koseck said he thought the request was reasonable and that he trusted Ms. Meads to skillfully handle the changes. He said he would support administrative approval of the requests.

Mr. Emerine and Mr. Williams said they were also fine with an administrative approval for the requests.

In response to Chairman Clein, Mr. Boyle said he was fine with the requests being administratively approved.

Chairman Clein said he was also comfortable with an administrative approval and stated there was Board consensus for the approval to proceed.

c. Draft Agenda for the next Regular Planning Board Meeting (October 14, 2020)

- Continued master plan discussion on mixed use Districts

d. Other Business

Planning Director Ecker let the Board know that the master plan neighborhood packets went out to all of the neighborhoods and that the comment period had commenced.

Mr. Jeffares reported that the map provided in his neighborhood's packet was incorrect. He said he thought Birmingham Farms had the Crestview map, and that the Crestview map had two or three streets that are not currently in the neighborhood and none of the streets that currently are in the neighborhood.

Planning Director Ecker said she would pass Mr. Jeffares' comments along to the master planning team.

09-123-20

I. Planning Division Action Items

a. Staff Report on Previous Requests

b. Additional Items from tonight's meeting

09-124-20

J. Adjournment

No further business being evident, the Chairman thanked all in attendance and adjourned the meeting at 9:57 p.m.

Jana L. Ecker

Planning Director



October 8, 2020

Planning Board Members
City of Birmingham
151 Martin St.
Birmingham, MI 48012

1st Draft Master Plan Workshop, October 14, 2020 Planning Board Meeting

Dear Planning Board Members,

Enclosed you will find the brief presentation concerning the following Master Plan topics our team has assembled for your meeting on October 14th:

- Haynes Square
- South Woodward Gateway
- Rail District

These review topics are based on the approved process that City Commission and Planning Board spent considerable time formulating during the early summer months.

MASTER PLAN THEMES

Please continue to review the subject sections of the draft Master Plan through the lens of the agreed-upon “key themes”, which the Planning Board pared down from 11 to six during your August meeting (below).

1. Connect the City

Major roadways and the region’s focus on automobiles over other modes of movement have divided many of Birmingham’s neighborhoods. Recent efforts, such as crossings and lane reductions along Maple, have proven success in re-connecting neighborhoods. “Big” Woodward (M-1) is the most substantial of barriers, carving a divide that is extremely unsafe for all roadway users, in effect cutting off eastern neighborhoods from downtown. Yet Big Woodward is not alone;

many smaller barriers exist along busy streets and even neighborhood streets that are missing sidewalks, crossings, and suffer from poor surfaces. Street improvements across the City should be prioritized to establish safe and convenient walking and biking routes, safely connecting Birmingham.

- A. Advocate for and fund (as necessary) a reduction of travel lanes, lane widths, and speeds along Big Woodward
- B. Improve the Big Woodward streetscape, especially in the South Woodward Gateway
- C. Invest in the Triangle district to grow a downtown area that spans Big Woodward
- D. Increase safe crossings of larger roads such as Maple's recent pedestrian refuges and future safe Big Woodward crossings
- E. Prioritize pedestrian and bike improvements along the neighborhood loop, and continue improvements throughout the City, especially to schools and civic buildings
- F. Ensure bike routes have adequate facilities and connect to routes and trails in surrounding communities
- G. Ensure public facilities, spaces, and sidewalks are accessible to people of all abilities
- H. Participate in regional transit planning and programs, and improve existing transit stops

2. Embrace Managed Growth

The region is growing and pressure to grow is felt in Birmingham. How and to what extent that growth is accommodated is a key decision for the City's future. Should little or no new housing be provided, housing costs will surely increase.

Should unrestricted housing development be allowed everywhere, the character of neighborhoods will likely suffer. Well managed growth can preserve neighborhood character and diversity while providing housing opportunities for our teachers, first responders, and other community members who are increasingly priced out. Managed growth can also increase lifestyle choice, expanding housing options as requested by seniors, young families, and singles in the community.

- A. Retain neighborhood character and stability by ensuring new housing is located and designed to enhance neighborhood identity

- B. Retain age, family structure, and income diversity with a variety of housing types and sizes
- C. Concentrate most growth in mixed-use districts, increasing multi-family housing in a downtown-like setting
- D. Carefully add Missing Middle housing along major and secondary thoroughfares
- E. Adjust zoning regulations to retain neighborhood scale and intensity
- F. Incentivize renovation of existing homes as well as additions which retain the neighborhood scale
- G. Allow accessory dwelling units where they align with neighborhood character, adding housing and secondary income
- H. Invest in a high-quality senior center to retain and attract older adults

3. Retain Neighborhood Quality

Neighborhoods are organizational touchstones for the community, helping to orient and relate households, institutions, amenities like parks and schools, and businesses. Together, the fabric of neighborhoods coalesce, making Birmingham a wonderful place to live. Still, improvements are needed across the community, small and large. Residents in each area of Birmingham should have safe and convenient access to parks, schools, and services; walking, biking, and driving routes; and well-maintained streets, sewers, and stormwater systems.

- A. Reinforce neighborhood identity
- B. Focus land-use on neighborhood structure
- C. Evaluate regulations, City programs, and amenities per-neighborhood
- D. Improve streets and sidewalks to encourage walking and biking to neighborhood destinations
- E. Encourage small neighborhood retail destinations
- F. Evaluate flooding and stormwater solutions per-neighborhood
- G. Evaluate streets, sidewalks, and crossings per-neighborhood

4. Invest in Civic Spaces and Programs

Parks, plazas, and civic programs bring the City to life. While many parks are within neighborhoods, they are intended for use by all residents, offering a variety of programming opportunities. Similarly, civic programs connect people from across the City.

All residents should have a park and playground nearby, and easy access to civic functions. The City should invest in parks, provide or help fund inexpensive space for organizations, and host numerous civic events throughout the year, solidifying the community as a whole.

- A. Invest in park programming, accessibility, maintenance, equipment, and plantings
- B. Improve the edges and access through the Rouge River corridor and attached parks
- C. Provide a centrally located senior center with space for use by other local organizations
- D. Support existing civic organizations in the community
- E. Add more civic events like the Hometown Parade and summer concert series

5. Support Mixed-use Areas

Birmingham's downtown is a shining jewel in the region, yet its edges and the other mixed-use areas are underperforming. Each district would benefit from increased housing, street improvements, and parking management. Yet each district is unique, requiring different solutions in their specific context. Investments in each mixed-use district should be aimed to deliver multiple experiences across the city, retaining customers throughout the week and providing additional options to attract more. With more housing and active businesses, the tax base in these areas can better contribute to citywide programs.

- A. Reinforce or establish unique identities for Birmingham's mixed-use districts.
- B. Establish different standards for each mixed-use district, diversifying intensity, character, and businesses.
- C. Incentivize housing to increase activity, district tax-base, and to absorb future growth.
- D. Ensure each district has amenities like parks and public art.
- E. Promote private development in underperforming mixed-use districts through public investments in parking and streetscapes.

6. Advance Sustainability Practices

Birmingham's natural resources and beauty are assets to all residents. The City should protect these assets, especially the Rouge River watershed, through sustainable development practices on the part of the City, residents, businesses, and developers.

- A. Repair degraded river banks along the Rouge River and take measures to reduce chemical and other damage from run-off.
- B. Reduce stormwater run-off affecting homes and the watershed with neighborhood-scale treatment and stricter standards for new construction.
- C. Improve recycling availability, investigate composting.
- D. Incentivize sustainable business practices such as compostable containers, recycling, and composting.
- E. Improve municipal practices both in facilities and operations.

SCHEDULE SUMMARY

To position this meeting in terms of future review efforts, below is a snapshot of the Draft 1 Review schedule approved by City Commission in June, 2020. Please note that we have added a 2nd neighborhood plans review meeting during the December, 2020 Planning Board meeting.

Master Plan Review and Adoption, 2020 - 2022		
Action	Meeting(s) / Time Period	Outcome(s)
1st Draft Review: <ul style="list-style-type: none"> Affirm and continue process for completing review of 1st draft 	<ol style="list-style-type: none"> PB study session 6/10/20 CC / PB joint meeting 6/15/20 	<ul style="list-style-type: none"> Broad consensus on Themes and Key Objectives revisions to draft plan by PB and CC

<ul style="list-style-type: none"> • Prepare neighborhood packets for additional public input on neighborhood proposals • Focus on themes and key objectives 	<ol style="list-style-type: none"> 3. PB review meetings 3 – 8 (1 -4 completed), 8/20 through 1/21 6. CC review meeting for direction on proposed revisions to 1st draft per PB recommendation, 2/21 	
Prepare 2nd Draft: <ul style="list-style-type: none"> • Consultant team prepare revisions to draft Master Plan 	<ul style="list-style-type: none"> • 1 month 	<ul style="list-style-type: none"> • 2nd Draft Master Plan, delivered to City approx. 4/1/21
Option - Conduct Additional Public Engagement: <ul style="list-style-type: none"> • Interactive draft 2 website • Neighborhood meetings • Report 	<ul style="list-style-type: none"> • 2 months 	<ul style="list-style-type: none"> • City receives additional public input related to revised draft plan to use in reviewing 2nd draft
2nd Draft Review: <ul style="list-style-type: none"> • Focus on outstanding strategic issues concerning themes and key objectives 	<ol style="list-style-type: none"> 1. PB review meetings 1 - 4 5. CC / PB joint meeting 	<ul style="list-style-type: none"> • City finalize 2nd draft and distribute to adjoining and other entities consistent with MI Planning Enabling Act for their review



MEETING PROCESS

The Planning Board meeting process was approved to be conducted as follows:

- Consultants will briefly summarize the Key Objectives, 10 minutes.
- The Planning Board will welcome public comment on Key Objectives, approximately 30 minutes or so.
- The Planning Board will discuss recommendations concerning the Key Objectives.

Please let us know if you have any questions; thank you.

Regards,

Matthew Lambert

Cc: Jana Ecker, Planning Director
Bob Gibbs, Gibbs Planning Group
Sarah Traxler, McKenna



THE
BIRMINGHAM
PLAN

2040

Mixed-use Districts

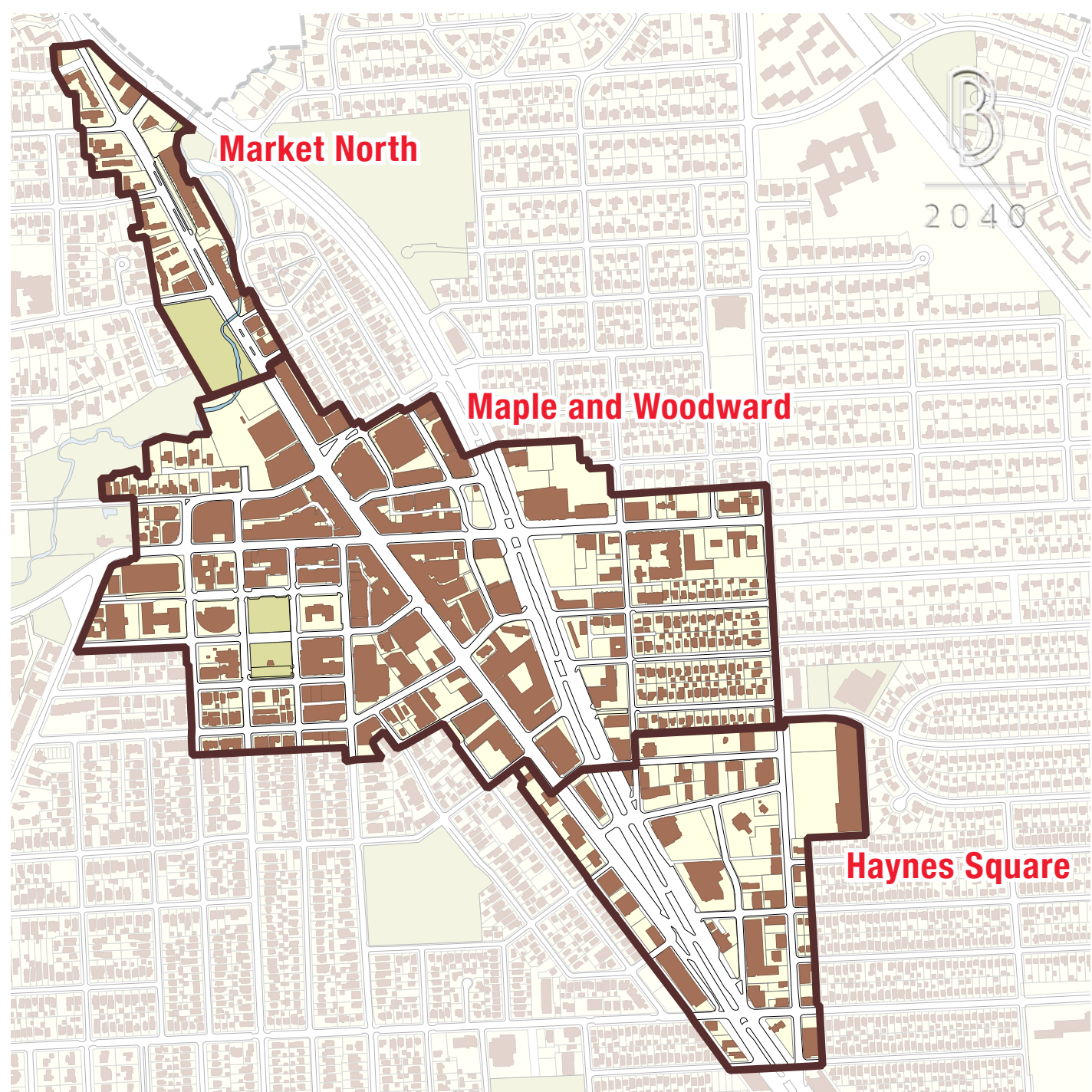
PLANNING BOARD MEETING · OCTOBER 14, 2020

DPZ GIBBS MCKENNA

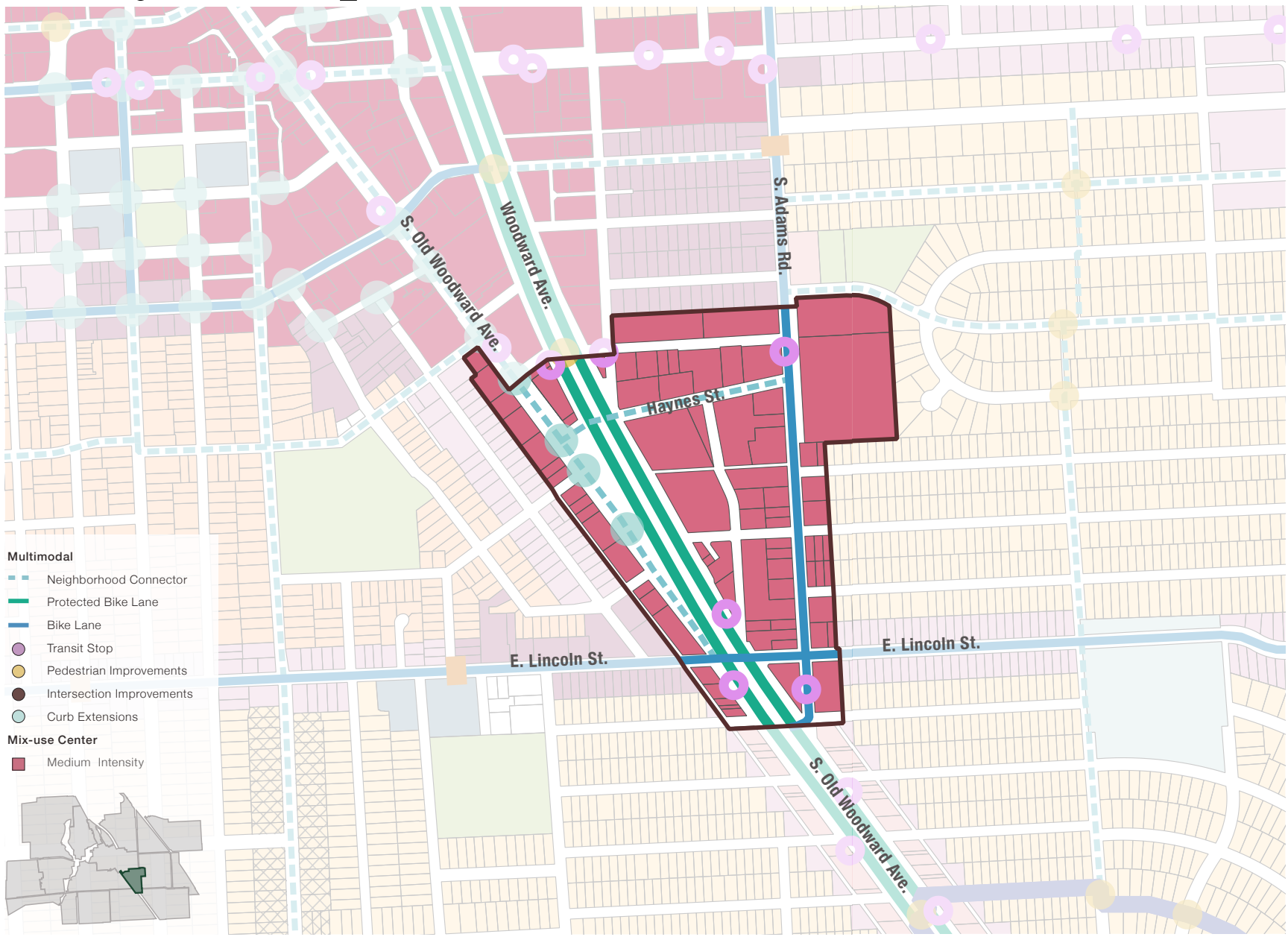
District Differentiation

Goal: To create a clear identity for each mixed-use district in order to drive customer traffic and encourage harmonious businesses in each area. For instance, Market North should remain a lower-scale, quaint main street surrounded by neighborhoods. Maple and Woodward should remain the City's primary shopping district, along with its upscale dining and lodging. And South Old Woodward needs a real identity and sense of place, proposed as Haynes Square for a traffic safety and placemaking solution which will be presented at a subsequent meeting.

1. Release an RFP to brand the City's multiple mixed-use districts, especially concerning signage and wayfinding, but extending also to elements of the streetscape like tree grates, lights, trash and recycling cans, and public art themes like the recent popcorn painting of a utility box on Old Woodward at the theater.
2. Define, sign, and market three distinct Downtown districts: Market North, Maple and Woodward, and Haynes Square.
3. Install business directory and way-finding signage for pedestrians and cyclists throughout all shopping districts, beginning with Maple and Woodward, Market North, and Haynes Square.
4. Install smart and elegant parking wayfinding signage in Downtown.

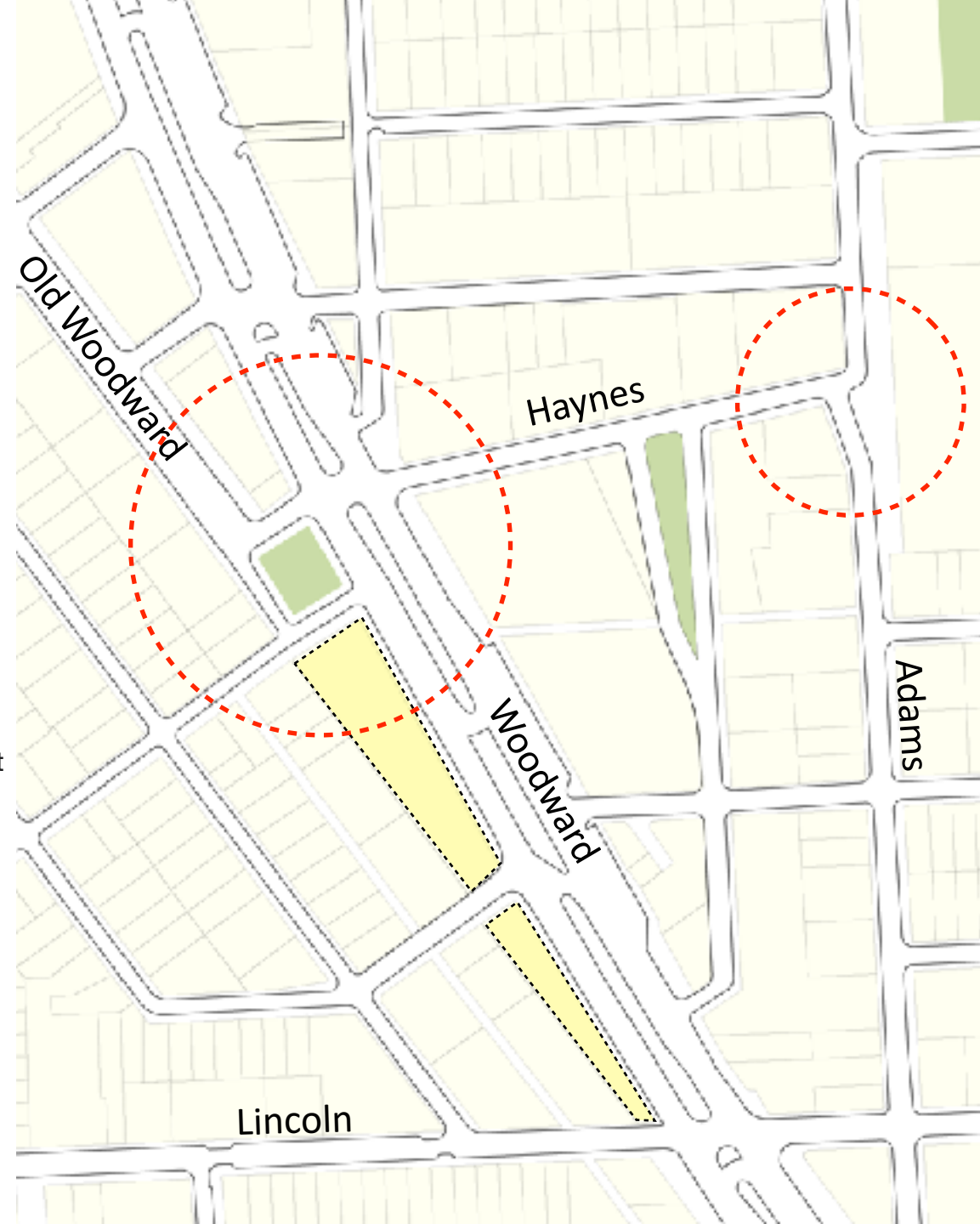


Haynes Square



Opportunities

1. Reconfigure the streets around Haynes Square to create the square and fix the acute intersection between Woodward and Old Woodward.
2. Build the public square with a cafe and trees to block noise from Big Woodward, seating, a kids play area, and other compelling civic features.
3. Purchase properties and implement Worth Park and other public realm enhancements, including civic art, streetscape, traffic calming, and way-finding.
 1. Build Worth Park.
 2. Install enhanced streetscape and landscape improvements along Bowers, Haynes, and Webster.
 3. Develop Worth Street as a shared-use streetscape.
4. Create a parking assessment or incremental tax district as necessary for land purchases and for financing the development of parking structures, as has been done in the past.
5. Build a public parking deck on the east side of Haynes Square, in the Walgreens parking lot as has been negotiated.
6. Create a brand for Haynes Square, reinforced with special signage, landscaping, street furnishings, and building design standards.
7. Install metered, on-street parking along Adams and Lincoln Roads (Lincoln dependent upon the bike lane).
8. Create subdivision and zoning standards to encourage redevelopment of the Adam's Square shopping center, offering significant development capacity in exchange for a public open space and public parking.
9. Implement pedestrian-walkway improvements along Woodward to improve the walkability and Woodward crossings.





South Woodward Gateway

**SOUTHERN GATEWAY
URBAN DESIGN PLAN**

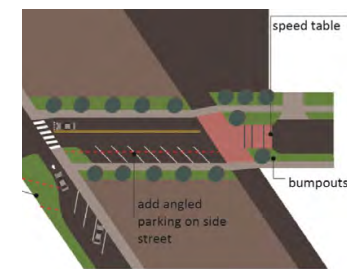
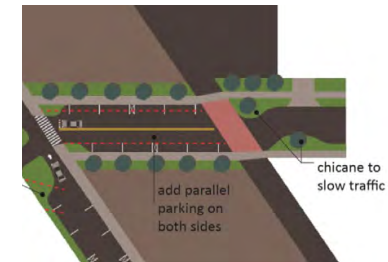
SWG: Alleys

1. Identify an alley segment to use as a pilot project. This segment should have generally underutilized parking and intermittent buildings, like the segment between Humphrey and Bennaville on the east side, or underutilized parking and businesses that may be willing to open rear entrances, like the segment between Bennaville and Chapin on the east side.
2. Pilot a shared-use alley at the selected segment by re-paving the alleyway and working with property owners to infill housing along the triangular parcels and open existing buildings to the alley. Power poles should be relocated underground during the alley development.
3. Incentivize redevelopment through increased zoning capacity and reduced parking requirements.



SWG: Sleeves

1. Establish a zoning district to allow for and incentivize redevelopment in this format, including:
 1. Requiring storefronts along neighborhood streets.
 2. Requiring parking be located midblock.
 3. Requiring storefronts along the alley.
 4. Requiring housing along the neighborhood-side of the alley, limited to 2 stories
 5. Permitting townhouses and mixed-use housing along the neighborhood-side of the alley
 6. Permitting multi-family housing on the commercial properties
 7. Permitting 2 stories along the alley and 3 or 4 stories between Woodward and 50 feet of the alley
 8. Reducing parking requirement and allowing shared parking.
2. Pilot the Neighborhood Sleeve option where the shared-use alley is implemented by:
 1. Striping on-street parking.
 2. Constructing chicanes on the neighborhood side of the alley.
 3. Revising the streetscape between the alley and Woodward, paving the planter with pervious pavers and providing seating, trash and recycling cans, pedestrian-scaled lighting, bike racks, and other streetscape elements typical in commercial districts.



Rail District: Near-term

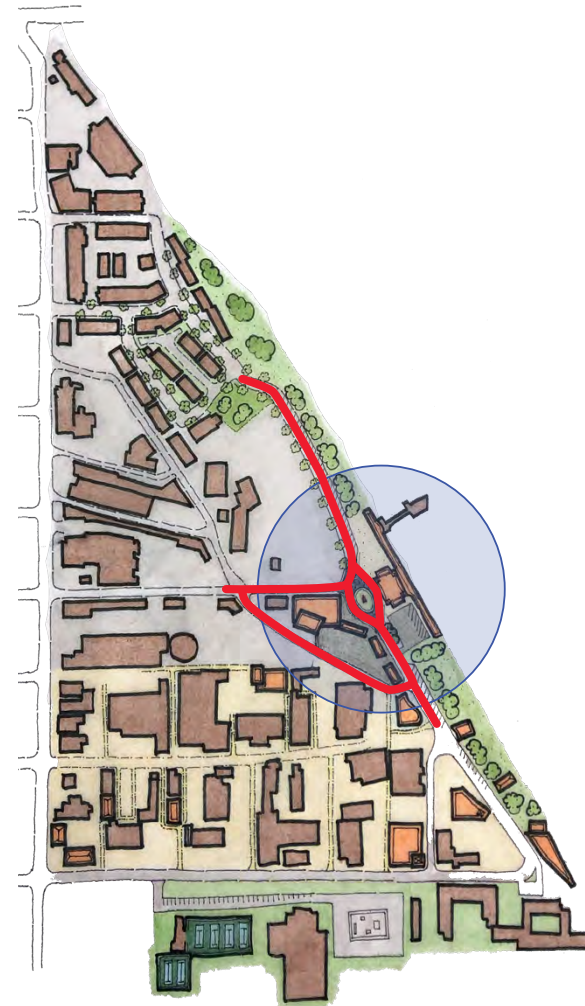
1. Create an overlay district for the Lower Rail District that implements the zoning adjustments discussed above and activates more lenient development review decision making.
2. Construct a shared-use street section along Cole and Commerce Streets.
3. Update the 1999 Eton Road Corridor Plan for the area south of Palmer Street by including:
 1. So long as buildings - existing or new - are one story, eliminate all requirements of Section 5 of the Site Design Guidelines p 41-46. These include but are not limited to:
 1. Eliminating building frontage and sidewalk requirements.
 2. Eliminating parking requirements, except that the on-street parking is determined by the "Immediate Neighbors" in Torry and Kenning.
 3. Eliminating the signage and landscaping requirements.
 4. Eliminating building use and aesthetic requirements.



Rail District: Long-term

1. The MX District should be exempt from LA-01 (e) and (f) as is true in Downtown, or at a minimum planting in the district should only be required within the streetscape and within open area of a property, not based on a minimum number of trees per residential unit as current.
2. The MX District should be carefully analyzed by contracting two or more architects to complete preliminary building designs for mixed-use buildings on existing sites, small and large, with and without on-site parking, attempting to achieve capacity. The architects should be requested to discuss the present challenges and constraints that are faced in the process. While some challenges are intended in the code, others may be unknown without testing.
3. Update the 1999 Eton Road Corridor Plan to include: At the termination of Holland Street, creating a connection to the rail station by purchasing a 30ft wide corridor or easement.
4. Aquire access to the Troy Transit Center from the School District.
5. Aquire access to the Troy Transit Center from remaining property owners through negotiation for purchase or the property or easement, failing which through eminent domain.
6. Pursue development of a public parking structure in the Rail District on a side with adequate access to the Lower Rail District and the future connection to the Troy Transit Center.

Future Potential



- Location of train station addition
- Property blocking connection
- Future road extensions

Existing Conditions



TRAIN STATION ACCESS



DPZ GIBBS MCKENNA

C. Mixed-use Districts

C.4. Haynes Square

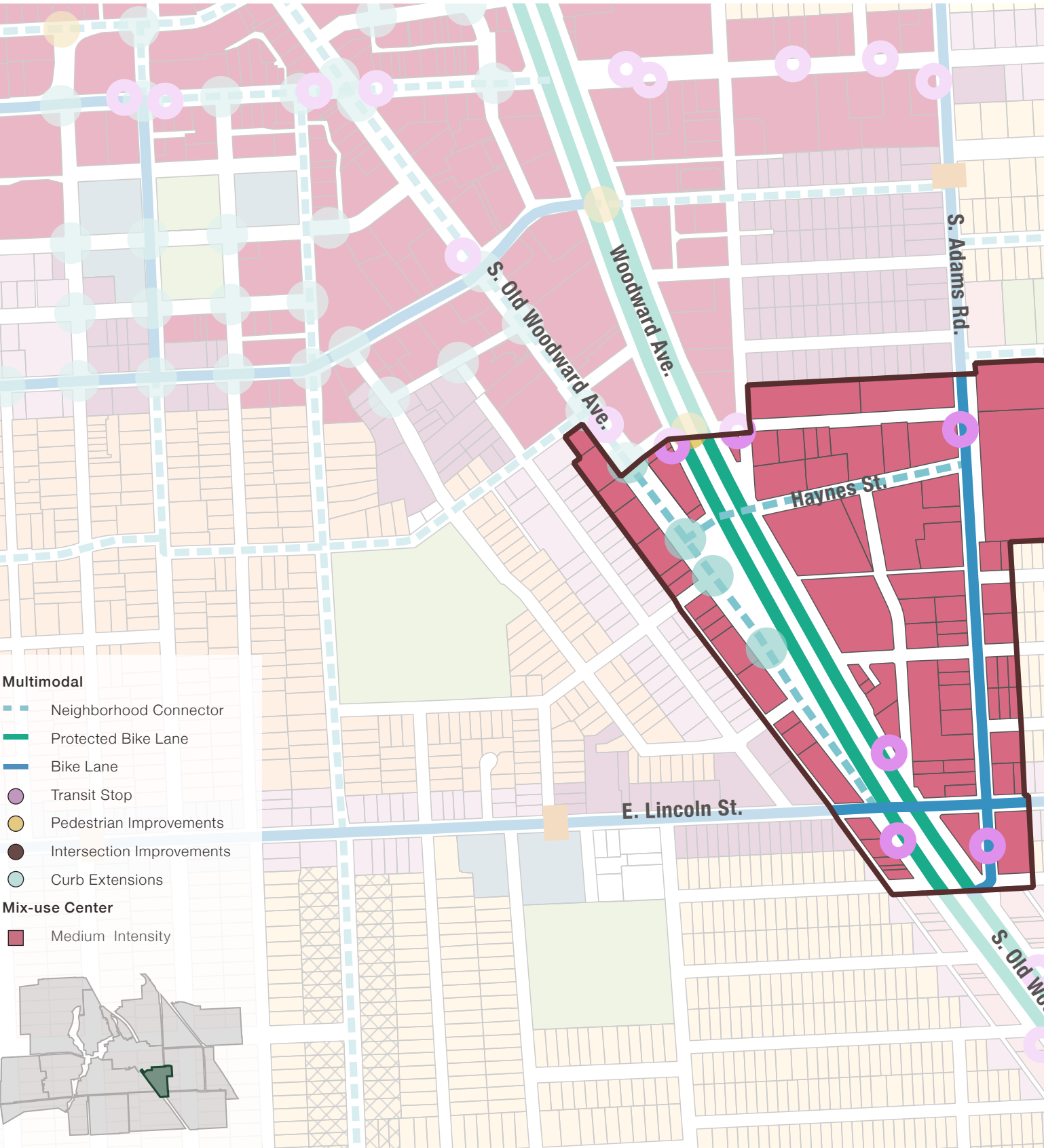


Figure C.4-01. Haynes Square.

C. Mixed-use Districts

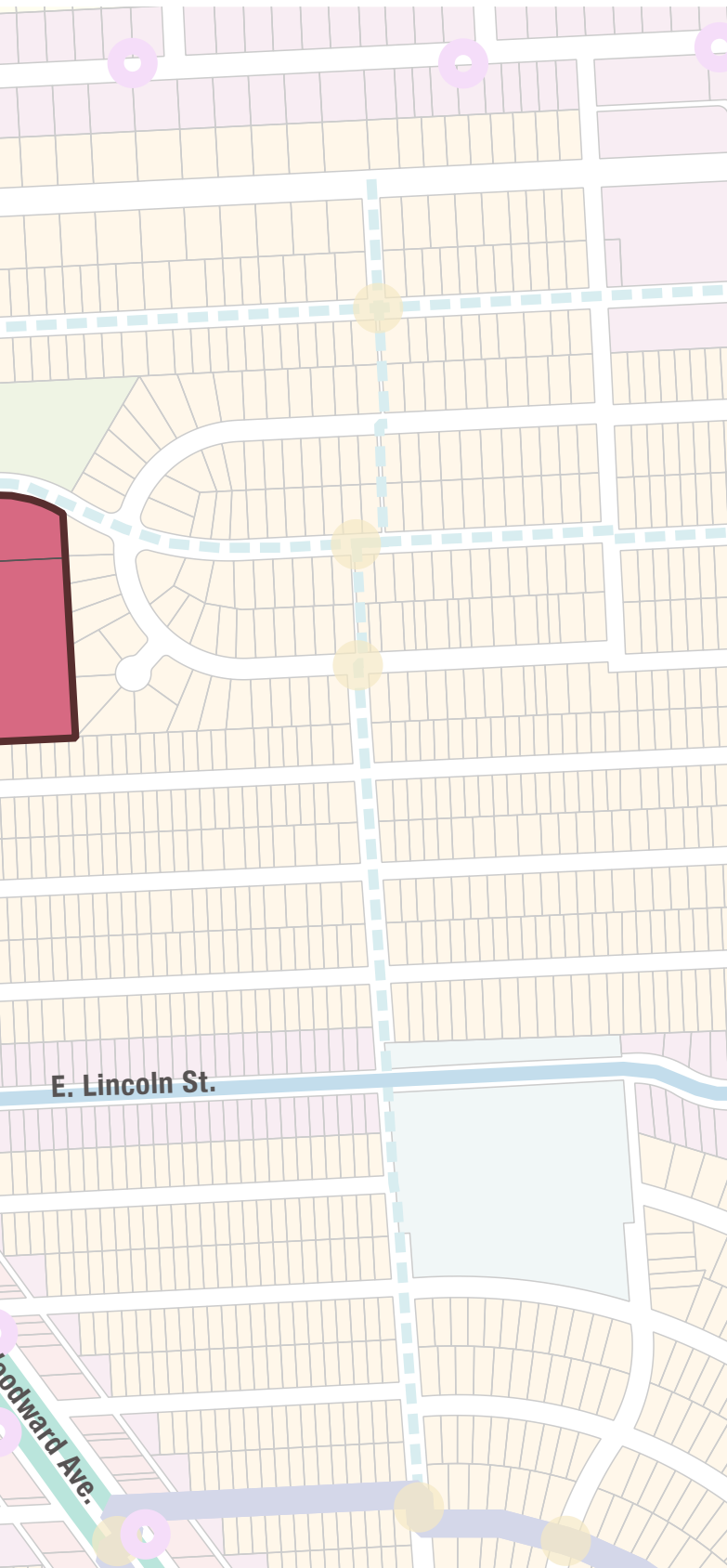
C.4. Haynes Square

Haynes Sqaure

CHARACTER

South of Frank Street, the character of downtown changes, expressed in zoning as well as business success. Birmingham has a need for and room for business diversity, which better serves residents. Rather than consider South Old Woodward an inferior retail district, the area should be combined with the lower Triangle District, spanning big Woodward as Haynes Square. Street reconfigurations would result in a public plaza at south Old Woodward and Haynes Street. This plaza should be the new heart of a district independent from Maple and Woodward. Differentiating this area, and connecting across big Woodward supports a clear distinction in retail and mixed-use. While Maple and Woodward includes a significant presence of offices, Haynes Square may be oriented towards more residentially-based mixed-use. The district encompasses the southern portion of the Triangle District, centered on Worth Park, spanning Woodward. Due to the adjacency of the 555 building and 9-story zoning along Woodward, Haynes Square frames the entry to the greater downtown area and gently feathers down in intensity to the east and west.

This combined district represents Birmingham's greatest opportunity for the development of both extensive middle-income housing—a deficiency that needs to be addressed—and emerging commercial business spaces. Unfortunately, the development envisioned in the 2007 Triangle District master plan has never been realized, even though the region's real estate market has been booming for over a decade. Numerous non-market-based factors, including the lack of public parking structures, an uninspiring public realm, and real estate development restrictions, have contributed to the district's stagnation.



C. Mixed-use Districts

C.4. Haynes Square

Opportunity

OBSERVATIONS

The Haynes Square area lacks the walkability and identity that all other Birmingham commercial districts possess. Having stagnated for decades, the district is the City's greatest opportunity for development, with few existing buildings of value.

DISCUSSION

Haynes Square is within walking distance to Maple and Woodward, on both sides of big Woodward. Nearby are three supermarkets, the Rail District, numerous restaurants, as well as popular businesses in the district. Haynes Square spans both sides of Woodward Avenue, bound by Bowers to the North, Adams to the East, and Lincoln to the South. Its size is similar to the active office and retail

core of Maple and Woodward. To capitalize on its potential, two major investments are required: constructing a parking garage on the east side of Woodward and reconfiguring the intersection between Woodward and Old Woodward.

Neither of the 2007 Triangle District plan's proposed public parking structures nor its proposed parking assessment district have been implemented. Due to the odd lot shapes in the area and high zoned capacity, private development is not going to take the first step to launch the district. To successfully launch Haynes Square, the City needs to invest in a parking garage. Ideally this garage would be suited to meet most of the needs of the district alleviating developers from the burden of parking. With this structure in place, and mixed-use residences able to unbundle

...the district is the City's greatest opportunity for development...



Image C.4-02. The south Old Woodward shopping area is underdeveloped and lacks a clear identity.

C. Mixed-use Districts

C.4. Haynes Square

parking as discussed in the Maple and Woodward section, new housing and businesses are likely to be developed quickly in the surrounding blocks. Due to the district's size and large areas of surface parking, development will bring significant increases in tax revenue. While a smaller garage has been discussed, which may be pursued to whet the appetite of developers, the construction of multiple smaller garages is less efficient in the long run than a higher-capacity structure.

On the west side of Woodward, the southern portion of Old Woodward has historically been considered a poorly performing portion of downtown. Presently the intersection of Old Woodward and Woodward occurs at a very acute angle, and without a signal it requires a dangerous northbound left turn (See Fig. C.4-03). Properties that are located along Old Woodward south of George Street are

zoned for taller buildings, but have not seen redevelopment. While fronting onto the empty grass strip is unattractive, parking issues also make development difficult. Being outside of the downtown parking district, the parking necessary to build in this area makes the prospect impractical with relatively shallow lots that back onto single-family residential. This plan recommends that Old Woodward be reconfigured to alleviate the awkward intersections and provide larger building pads. George St is extended to big Woodward, and Old Woodward removed south of George. Bound by Haynes St, Old Woodward, George St, and big Woodward is a new public square, Haynes Square. South of George St, properties are extended to big Woodward, providing sites that can accommodate buildings and parking. The extended properties should be traded for a public surface parking lot where buildings currently sit along Old Woodward, 70 feet deep measured from the alley, which leaves over 100 feet of property for development, deeper

EXISTING



PROPOSED



Figure C.4-03. Haynes Square reconfiguration.

C. Mixed-use Districts

C.4. Haynes Square



Figure C.4-04. Redevelopment of Haynes Square.

C. Mixed-use Districts

C.4. Haynes Square

New buildings in the area can take advantage of the dynamic and pedestrian-centric streetscape.

than current properties.

The public space of Haynes Square should be configured as a square, similar to Shain Park but about half its size (See Fig. C.4-03). Haynes Square should have a cafe, seating, and restrooms as is recommended for other urban parks. Lined by trees along its edges, the square will provide an attractive entrance to the greater downtown area, flanked by tall, new development east along Woodward and the 555 building to its north.

Through this redevelopment, Haynes St. crosses Woodward to meet Old Woodward at a signal. On the east side of Woodward, Haynes becomes a main street, paired with Worth Street. To support the main street with additional traffic, as Maple and Woodward is supported by Maple's traffic, Adams should be slightly adjusted so that south-bound traffic uses Haynes to access Woodward. This adjustment is detailed in Figure A.2-26 and the section Connecting the City.

At the intersection of Haynes and Worth Streets, the 2007 Triangle District plan recommends a triangular green called Worth Park. This space provides an important focal center for the east side of Haynes Square. It also provides needed open space for the Torry neighborhood. Like other urban parks discussed in this plan, Worth Park should have ample seating, shade, and areas for children to play, and a cafe with public restrooms. Worth Street, which has few existing buildings facing onto it, should be considered for a shared-use treatment to provide interest and connect with the South Woodward Gateway alley system. New



C. Mixed-use Districts

C.4. Haynes Square

buildings in the area can take advantage of the dynamic and pedestrian-centric streetscape.

A missing piece for decades has been the Adam's Square shopping center, which represents the greatest single redevelopment site in the City. While plans cannot force the owners to redevelop their site, activating Haynes Square will significantly increase the property's value. With an active Haynes Square district adjacent, development pressures may win out. To prepare for this, zoning and subdivision requirements should be considered such that Adam's Square provide open space for the Torry neighborhood and public parking in exchange for development capacity modeled upon the Triangle District Overlay.

RECOMMENDATIONS

1. Create a parking assessment or incremental tax district as necessary for land purchases and for financing the development of parking structures. Decades ago, a similar process was used to build downtown's five structures.
2. Reconfigure the streets around Haynes Square to create the square and fix the acute intersection between Woodward and Old Woodward.
3. Build the public square with a cafe and trees to block noise from Woodward, seating, a kids play area, and other compelling civic features.
4. Purchase properties and implement Worth Park and other public realm enhancements, including civic art, streetscaping, traffic calming, and way-finding.
5. Build a public parking deck on the east side of Haynes Square, in the Walgreens parking lot as has been negotiated.
6. Build Worth Park.
7. Create a brand for the Haynes Square, reinforced with special signage, landscaping, street furnishings, and building design standards.
8. Install enhanced streetscape and landscape improvements along Bowers, Haynes, and Webster.
9. Develop Worth Street as a shared-use streetscape.
10. Improve pedestrian linkages to the surrounding neighborhoods, especially along Adams.
11. Trade developable land and install a public surface parking lot along the south Old Woodward alley.
12. Create a parking district for Haynes Square which allows residences to purchase parking passes in public garages, as discussed for Maple and Woodward.
13. Install metered, on-street parking along Adams and Lincoln Roads.
14. Create subdivision and zoning standards to encourage redevelopment of the Adam's Square shopping center, offering significant development capacity in exchange for a public open space and public parking.
15. Implement pedestrian-walkway improvements along Woodward to improve the walkability to both downtown and the market districts.

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C. Mixed-use Districts

C.5. South Woodward Gateway

South Woodward Gateway

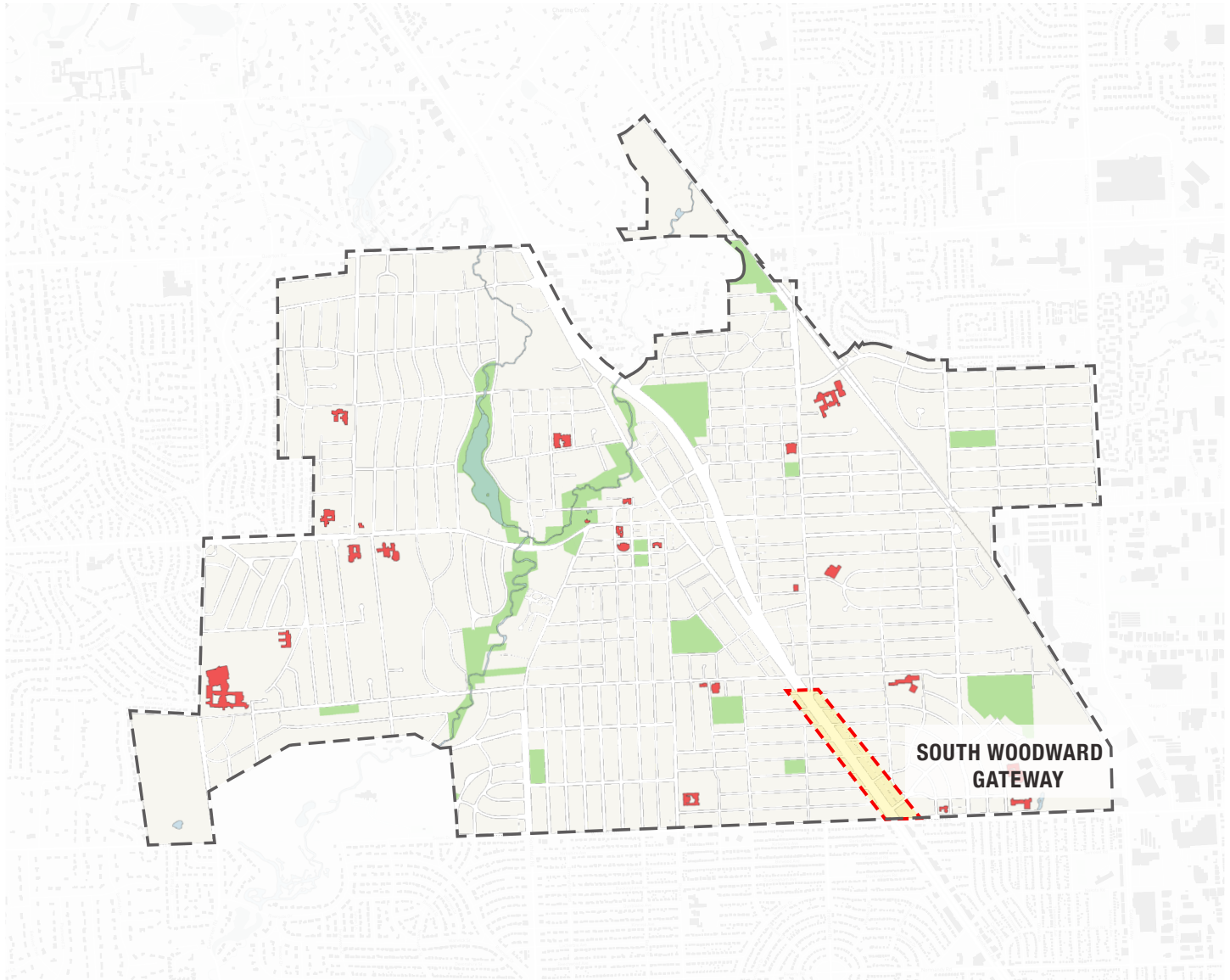


Figure C.5-01. South Woodward Gateway area.

CONDITIONS

The South Woodward Gateway, located along Woodward from 14 Mile to Lincoln, is the most un-sophisticated stretch of retail in the City. The southern portion of Woodward presents a sloppy and tired image of the community, which is otherwise active and successful. Woodward's growth and decades of mis-focused transportation policy has divided Birmingham and eroded the quality of the pedestrian and business environments. The area is not nearly

as bad as Anywhere USA, signage being under a moderate level of control to avoid the most egregious eyesores. This Gateway district is Birmingham's first impression to those travelling from the south. Numerous times the Gateway has been defended, suggesting that the quality of Woodward through Royal Oak is worse. To some extent this is true, Royal Oak has allowed a number of large, pole-mounted signs while Birmingham has not. And in Birmingham there is a roughly 5 foot wide raised section

C. Mixed-use Districts

C.5. South Woodward Gateway

of concrete between Woodward and the angled parking along the front of businesses, consistent for much of the corridor. Yes, the character of Woodward in Birmingham is better than Royal Oak, but it still is not good and not in keeping with the image of Birmingham. However, the area provides lower cost retail space with excellent exposure to traffic, housing most of the national chain merchants in the City. The Gateway is valuable for Birmingham, it just needs a new image. Woodward's conversion to an attractive and grand avenue is now supported by the Department of Transportation, however that future remains distant. In the interim, changes can be made on the side of private development to make this area more attractive and functional. While different from Downtown, the South Woodward area is a gateway to Birmingham and should reflect the community's character.

This Gateway district is Birmingham's first impression to those travelling from the south.

Few built environments are equally unpleasant for drivers, pedestrians, and cyclists alike. Woodward, south of Lincoln, somehow achieves a distinguished failure for all roadway users. For drivers, exiting and entering Woodward to access businesses is confusing and dangerous, and often parking is unclear. For pedestrians the cars move too fast for comfort, there are inconsistent sidewalks, and there is no beauty to hold interest. And for cyclists, there is no place safe to ride.

The low quality of the Gateway is well recognized; in 2013 The Southern Gateway Urban Design Plan was developed. A number of details from this plan should be pursued to identify and improve the Gateway character, adopted into this plan. And some influence the transformative proposals detailed here, like the improved alley treatment and adding Missing Middle housing. The plan recognizes that there are challenges to properties along the corridor and a historic perception that is difficult to change. It also recognizes opportunities to provide retail and events that are not present or allowed in the Downtown retail district. Numerous details from this plan remain important.



Image C.5-02. Typical character of the South Woodward Gateway in Birmingham.

C. Mixed-use Districts

C.5. South Woodward Gateway

With fresh eyes, this plan recognizes the need for more radical transformation. While the 2013 plan focuses heavily on public sector improvements, leaving existing buildings as-is. This plan imagines that with the right incentives and capacity, the area's aged buildings can be redeveloped in a manner that truly changes the Gateway's character. Because we were asked to focus on neighborhoods, this plan looks first at how the Gateway interfaces with the neighborhoods along it and second at the character along Woodward.

This plan imagines that with the right incentives and capacity, the area's aged buildings can be redeveloped in a manner that truly changes the Gateway's character.

From a neighborhood perspective, the Gateway provides some necessary services but it continues to encroach into neighborhoods with parking, increases neighborhood cut-through traffic, and is incredibly inconvenient for pedestrians. Some residents reported that they frequent a number of businesses in the Gateway, which serve as neighborhood destinations. Another resident reported that she walks the alleys for exercise. Despite the advantages of having nearby businesses, parking is a continual problem. Many businesses have purchased neighborhood houses beyond the alley, converting these to surface parking. This condition is most prevalent on the west side, but exists on both sides of Woodward. In many cases, this results in neighborhood houses facing onto open parking lots, and many more sharing a side or rear lot with them. As is recognized in the 2013 plan, the triangular parking lots are incredibly inefficient, even larger ones. In some instances parking spills over onto neighborhood streets as well.



Image C.5-03. Three houses on Bennaville face onto property purchased for parking.

C. Mixed-use Districts

C.5. South Woodward Gateway

Alleys

OBSERVATIONS

Some residents reported that they walk along the alleys of the Gateway rather than along Woodward. The 2013 Southern Gateway Urban Design Plan recommends that alley pavement be improved and made consistent and shared-use to accommodate pedestrians, shoppers, and service vehicles.

DISCUSSION

As identified in the 2013 Southern Gateway Urban Design Plan, the alleys present a key opportunity for transformative improvement. The plan focuses on surface treatment, opportunities for stormwater management, and the need for consistent treatment. It also identifies that alleys are shared between different types of users, recommending shared-use pavement treatment as illustrated in Img.'s C.4-05-08.

In addition to the surface treatment of alleys, they require active uses along their edges to be safe and pleasant. Currently businesses face onto Woodward and use alleys for parking and service. For transformative change, businesses should also face onto alleys, creating true shared-use streets. This dual-sided condition is becoming common in the local area, found at Kroger along Maple, along Big Beaver in Troy, and elsewhere throughout the region. In the alley, businesses should be encouraged to extend outdoor seating and outdoor retail displays into the shared-use alley space (See Img. C.5-04). Some residents expressed excitement at the opportunity for extending seating into alleys and occasional closure to traffic for block parties and events.

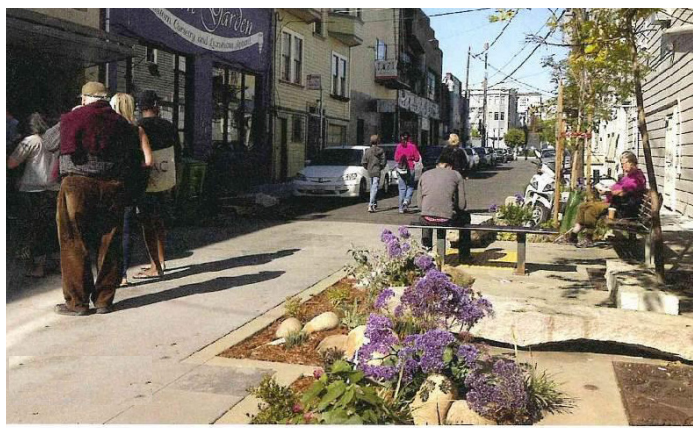
...the alleys present a key opportunity for transformative improvement.



Figure C.5-04. Shared-use alley space.

C. Mixed-use Districts

C.5. South Woodward Gateway



Images C.4-05-08. Shared-use paving examples from the 2013 Southern Gateway Urban Design Plan.



Image C.4-09. Double-fronted retail at Maple and Woodward.

C. Mixed-use Districts

C.5. South Woodward Gateway

In the alley, businesses should be encouraged to extend outdoor seating and outdoor retail displays into the shared-use alley space.

Full alley activation requires that both sides of the alley engage to define its character. Two options are presented in later subsections that propose consolidated parking. With parking addressed more efficiently, the triangular parking lots should revert to residential use. Most of the triangular properties can remain yard space, with shallow townhouses lining the alley and side streets. A muse-type townhouse is also appropriate, which has a flush single car garage entry composed along with the building facade. Common in the United Kingdom, Birmingham has a few townhouse developments that approximate this treatment.

RECOMMENDATIONS

1. Identify an alley segment to use as a pilot project. This segment should have generally underutilized parking and intermittent buildings, like the segment between Humphrey and Bennaville on the east side, or underutilized parking and businesses that may be willing to open rear entrances, like the segment between Bennaville and Chapin on the east side.
2. Pilot a shared-use alley at the selected segment by re-paving the alleyway and working with property owners to infill housing along the triangular parcels and open existing buildings to the alley. Power poles should be relocated underground during the alley development.
3. Incentivize redevelopment through increased zoning capacity and reduced parking requirements.



Image C.5-10. Muse-type housing in London.



Image C.5-11. Muse-type housing in Birmingham.



Figure C.5-12. Muse-type houses along the alley.

C. Mixed-use Districts

C.5. South Woodward Gateway



Figure C.5-13. Neighborhood Sleeve block configuration which consolidates parking mid-block and creates small neighborhood-t

C. Mixed-use Districts

C.5. South Woodward Gateway

Neighborhood Sleeves

OBSERVATIONS

The South Woodward Gateway should provide neighborhood-serving retail and services for nearby houses as well as retail opportunities for travelers along Woodward. Facing buildings onto Woodward creates an awkward parking condition and poor pedestrian experience. And parking access from Woodward is inefficient.

DISCUSSION

Each residential street in the Kenning and Pierce neighborhoods terminates on Woodward. Currently, the last 150 feet or more of each residential street is presented with surface parking, an unattractive alley, and typically a long blank wall along the side of buildings that face Woodward. For the neighborhood this is a poor experience by car, and especially walking. One specific instance illustrates a different potential outcome. The building with a sign “Charleston”, located behind a Dominos Pizza, faces onto Bennaville rather than Woodward (See Img. C.5-14). If this building were a coffee shop, for instance, it might have an outdoor patio to the left side of the image.

If most buildings faced businesses onto the neighborhood streets rather than Woodward, the end of each street would become a small neighborhood main street with a handful of shops. These Neighborhood Sleeves would benefit neighborhood residents and provide a superior pedestrian experience over Woodward. Limited in size, each Neighborhood Sleeve would create minimal traffic, and further they would encourage neighborhood residents to walk or bike for convenient access. The 2013 Southern Gateway Urban Design Plan includes two recommendations

The South Woodward Gateway should provide neighborhood-serving retail and services for nearby houses as well as retail opportunities for travelers along Woodward.



focused nodes along the east-west streets.

C. Mixed-use Districts

C.5. South Woodward Gateway



Image C.5-14. Building on Bennaville with potential for outdoor activation facing onto the neighborhood street.

for reducing traffic speed into neighborhoods. These options, particularly the one with parallel parking, perfectly support Neighborhood Sleeves and the pedestrianized alley. Facing businesses onto the neighborhood streets would also help slow traffic entering neighborhoods by the presence of pedestrians and seating. Additionally, these small neighborhood main street areas would be shielded from most of Woodward's traffic noise.

To achieve Neighborhood Sleeves, parking is consolidated midblock into two efficient parking trays, replacing both the angled-parking and triangle parking lots one for one. The existing buildings are razed and new buildings built to face the neighborhood streets. These new buildings could accommodate housing above. Due to the street geometries, new buildings could also include a mix of larger and smaller spaces. Should development demand be sufficient, a single parking deck would fit in the double tray, allowing for two stories of residential above the ground floor shops.

Along Woodward, the condition is made more attractive by removing the angled parking and consolidating parking access closer to midblock. Building facades line the Woodward sidewalk and the parking area can be made more attractive by low walls and landscaping, as is recommended in the 2013 plan.

Compared to the parking tray alternative, the Neighborhood Sleeve allows for larger footprint buildings and interfaces better with surrounding neighborhoods. However it is less attractive along the shared-use alley which is fronted by parking for a good duration. This can be mitigated by shade structures for seating and landscaping, as the retail buildings retain frontage on the alley as well.

C. Mixed-use Districts

C.5. South Woodward Gateway

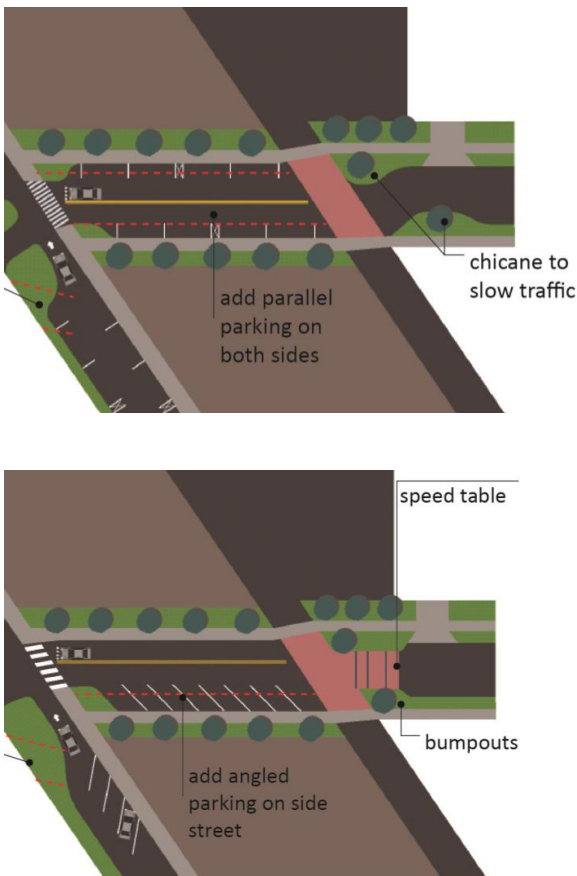


Figure C.5-15. Traffic calming recommendation from the 2013 Southern Gateway Urban Design Plan.



Figure C.5-16. Facing businesses onto the neighborhood streets to help slow traffic.

RECOMMENDATIONS

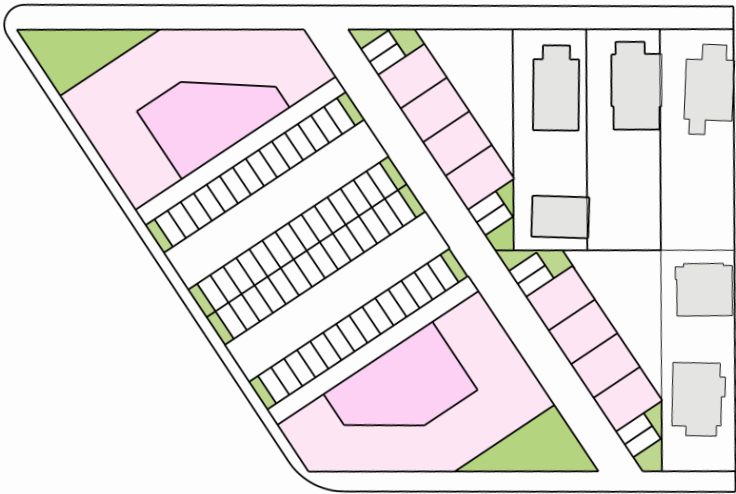
1. Establish a zoning district to allow for and incentivize redevelopment in this format, including:
 - a. Requiring storefronts along neighborhood streets.
 - b. Requiring parking be located midblock.
 - c. Requiring storefronts along the alley.
 - d. Requiring housing along the neighborhood-side of the alley, limited to 2 stories.
 - e. Permitting townhouses and mixed-use housing along the neighborhood-side of the alley.
 - f. Permitting multi-family housing on the commercial properties.
 - g. Permitting 2 stories along the alley and 3 or 4 stories between Woodward and 50 feet of the alley.
 - h. Reducing parking requirements and allowing shared parking.
2. Pilot the Neighborhood Sleeve option where the shared-use alley is implemented by:
 - a. Striping on-street parking.
 - b. Constructing chicanes on the neighborhood side of the alley.
 - c. Revising the streetscape between the alley and Woodward, paving the planter with pervious pavers and providing seating, trash and recycling cans, pedestrian-scaled lighting, bike racks, and other streetscape elements typical in commercial districts.

The following pages include diagrams of the Neighborhood Sleeve condition for sites on either side of Woodward.

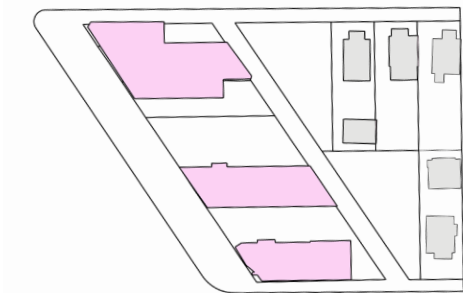
C. Mixed-use Districts

C.5. South Woodward Gateway

FUTURE POTENTIAL



CURRENT CONDITION



CURRENT V. FUTURE CONDITIONS		
	CURRENT	FUTURE
# OF PARKING SPACES	41	64
# OF RESIDENTIAL UNITS	-	18
RESIDENTIAL SF	-	16,523
RETAIL SF	15,177	16,559

- Green
- Residential
- Retail

Figure C.5-17. Neighborhood Sleeve condition on east side of Woodward.

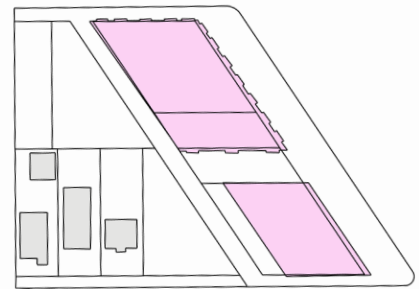
C. Mixed-use Districts

C.5. South Woodward Gateway

FUTURE POTENTIAL



CURRENT CONDITION



CURRENT V. FUTURE CONDITIONS		
	CURRENT	FUTURE
# OF PARKING SPACES	52	52
# OF RESIDENTIAL UNITS	-	21
RESIDENTIAL SF	-	16,815
RETAIL SF	19,852	14,399

- Green
- Residential
- Retail

Figure C.5-18. Neighborhood Sleeve condition on west side of Woodward.

C. Mixed-use Districts

C.5. South Woodward Gateway



Figure C.5-20. Parking Tray alternative consolidates parking along Woodward and focuses buildings towards the alley.

C. Mixed-use Districts

C.5. South Woodward Gateway

Parking Tray Alternative

The Parking Tray option is an alternative to the Neighborhood Sleeve. This option also requires buildings be reconstructed, which remains the only way to achieve real change. New buildings are built to face onto the shared-use alley as their primary facade, providing a secondary entry towards Woodward. This is described in the previous section on Alleys. Allowing second story residential along with the townhouses on the opposite side of the alley provides redevelopment incentive. To present a more attractive face to Woodward, parking is organized in a standard head-in tray, afforded more space as buildings have been relocated towards the alley. The reorganized parking is much more efficient, replacing existing parking one-to-one or substantially increasing parking. Entry to parking is removed from Woodward and relocated to the side streets. While there is a small amount of cross-traffic, turning onto the side streets and then into the parking lots will substantially slow traffic and impact Woodward traffic less during exit and entry. With more space and buildings aligned, the sidewalk along the Woodward-facing side of each business can be wide, consistent, and landscaped. Along Woodward the parking lot can be lined with a consistent low wall and landscaping, as detailed in the 2013 plan. The disadvantage to this option compared to the Neighborhood Sleeve is that it widens the overall space of Woodward rather than providing more enclosure and it does not support structured parking.

The following pages include diagrams of the Parking Tray condition for sites on either side of Woodward.

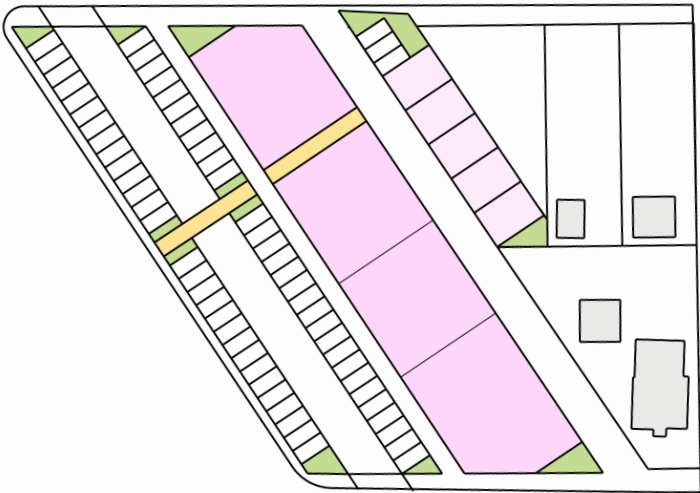
The reorganized parking is much more efficient, replacing existing parking one-to-one or substantially increasing parking.



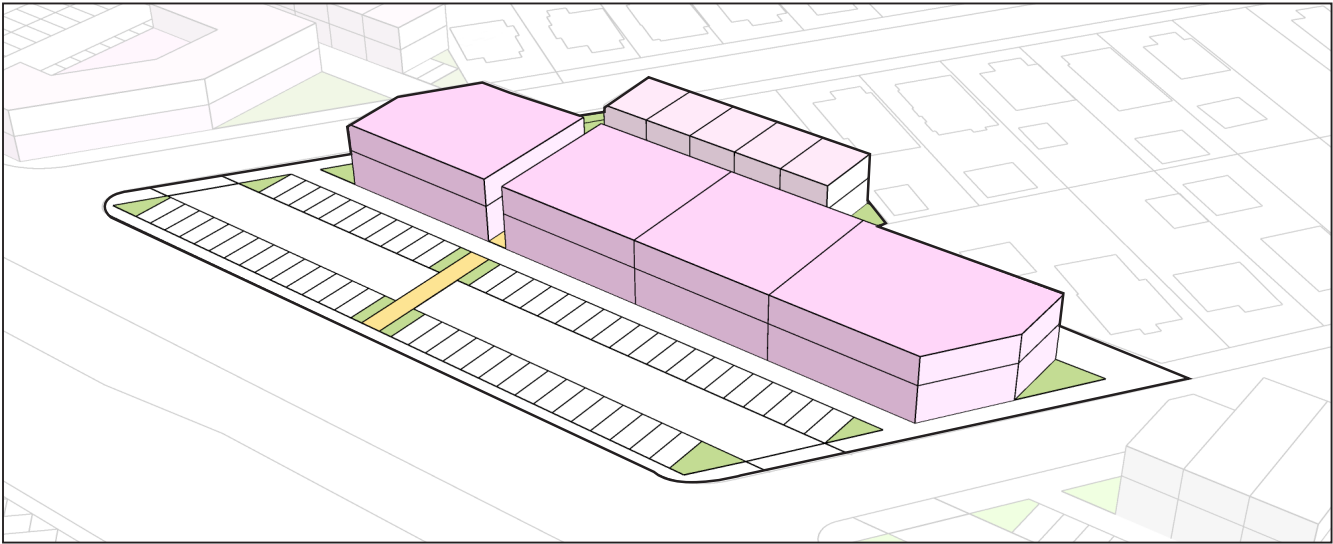
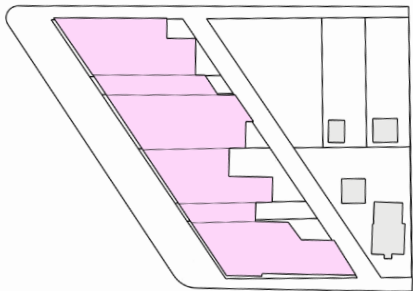
C. Mixed-use Districts

C.5. South Woodward Gateway

FUTURE POTENTIAL



CURRENT CONDITION



CURRENT V. FUTURE CONDITIONS		
	CURRENT	FUTURE
# OF PARKING SPACES	33	62
# OF RESIDENTIAL UNITS	-	21
RESIDENTIAL SF	-	19,629
RETAIL SF	23,155	16,327

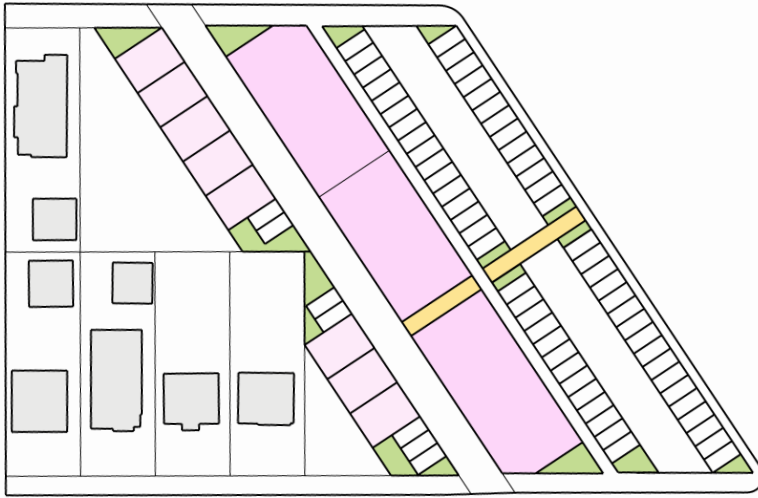
- Pedestrian Path
- Green
- Residential
- Retail

Figure C.5-21. Parking Tray condition on east side of Woodward.

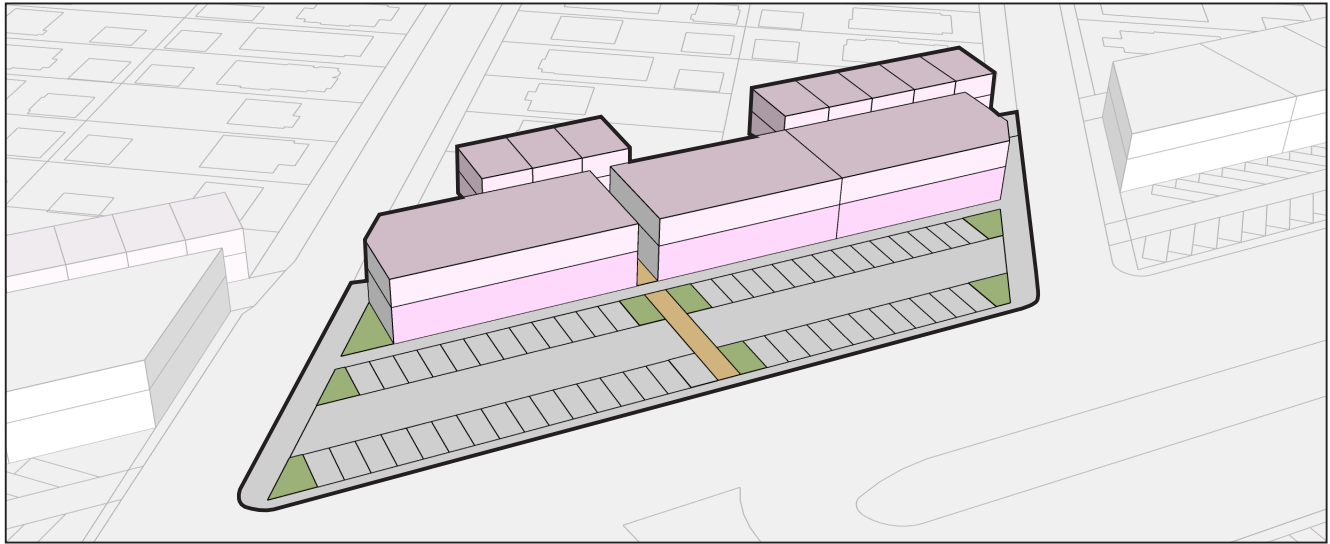
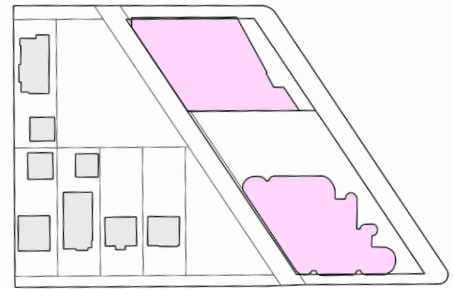
C. Mixed-use Districts

C.5. South Woodward Gateway

FUTURE POTENTIAL



CURRENT CONDITION



CURRENT V. FUTURE CONDITIONS		
	CURRENT	FUTURE
# OF PARKING SPACES	66	66
# OF RESIDENTIAL UNITS	-	20
RESIDENTIAL SF	-	17,090
RETAIL SF	17,278	11,938

- Pedestrian Path
- Green
- Residential
- Retail

Figure C.5-22. Parking Tray condition on west side of Woodward.

C. Mixed-use Districts

C.6. Rail District

Rail District

CONDITION

The Rail District is divided between what has been envisioned for its future and the utility that it currently provides. This is a place of experimentation for Birmingham, and has been for some time. Layers of history expose incremental changes. Traces of former rail spurs from the Grand Trunk Railroad are evident in odd property divisions, fencelines, and paths of unkempt foliage.

The district's northern edge, at the top of South Eton Rd., is capped by Big Rock Chophouse, an upscale restaurant housed in the City's former passenger rail station. From nearby parking lots, the City's Whole Foods market and large scale commercial in Troy is visible just over the tracks, yet inaccessible. Just below this, the District Lofts illustrate a grand future vision that is formal and neat. Along with the adjacent Iron Gate to the south, the area includes

some of the City's most contemporary multi-family offerings. Just east of Iron Gate, also part of the 1999 Eton Road Corridor Plan, is an experiment in live-work units, the area's tallest buildings at the time - 3 stories - that create a tight urban street grid open for future connections on neighboring properties. The Griffin Claw brewery is next southbound on Eton, a substantial micro brewery with an informal brewpub and outdoor beer garden, especially popular with young families. Next to this, tucked far back from Eton is the Robot Garage, a wonderland of toys and classes for creativity in making, from legos to art to robotics. Auto service, a lumber yard, and the City's Public Services Department follow old lines of rail spurs.

The Rail District is a place of experimentation for Birmingham, and has been for some time.



Image C.6-01. Recently constructed District Lofts look to the Rail District's urban future.

C. Mixed-use Districts

C.6. Rail District

The Rail District has no singular character, but overall it feels intimate, which is its charm.

The Lower Rail District, south of Palmer Street, consists of small, mostly single-story warehouse buildings occupied with varied businesses including yoga, co-working, dog daycare, art, dance, videography, auto body shops, and more. These are arranged haphazardly among small parking areas, charming in a way that is certainly not suburban. Only the degraded character of the street and lack of trees detract from the area's charm. The southern end of the district is capped by Kenning Park with the City's Ice Rink and skatepark, along with a new and quite urban senior retirement development.

The Rail District has no singular character, but overall it feels intimate, which is its charm. Other parts of the City are increasing their refinement, and many lament the loss of the City's artistic and entrepreneurial roots. Yet this is alive in the Rail District.

Plans and zoning for the Rail District point to a heavily urbanized future. A 2017 Ad-hoc Committee report for the Rail District estimated the zoned potential that could be built on properties likely to redevelop in the near future could increase intensity 10-fold, albeit unlikely. Due to the significant disparity between the district's long-term future and the functional and desirable near-term conditions, this plan recommends that policies and improvements continue the district's current success while incrementally preparing it for an intensified future condition.



Image C.6-02. Despite being quite near, the large shopping area across the rail line is inaccessible.



Image C.6-03. Live-work townhouses experiment with new business formats and an urban scale well in advance of focused district development.

C. Mixed-use Districts

C.6. Rail District



Images C.6-04-C.6-05. The Robot Garage on South Eton Street.



Image C.6-06-C.6-07. Diverse businesses occupying small, single story warehouse buildings along Cole St.

C. Mixed-use Districts

C.6. Rail District

Near-term Conditions

CONDITION

Many existing buildings within the Lower Rail District, are legally non-conforming, disincentivizing investments in existing buildings and continuation of the present condition.

DISCUSSION

The Lower Rail district is a type of commercial development which is currently emerging nationally. It provides incubator space for businesses at a much lower cost level than the downtown shops. Furthermore, its character is attractive to the younger demographic which is not and has not historically been particularly interested in the downtown corridor. While this area has already been rezoned to four stories it is essential that its continuation not be dis-incentivized. Therefore, everything possible

should be done to support its continued existence and even its extension as one-story buildings embedded in very small parking lots.

The current code applies standards that are appropriate to create pedestrian-oriented streets but as a result is burdensome to existing structures and uses. In the near-term, the Lower Rail District should remain informal and somewhat experimental. This character should be encouraged through zoning, development review, and in the public realm.

The Lower Rail district is a type of commercial development which is currently emerging nationally.



Figure C.6-08. The Lower Rail District.

C. Mixed-use Districts

C.6. Rail District

Zoning need only be slightly adjusted. These adjustments are the type appropriate for an overlay district which applies only to the Lower Rail District. The overlay should allow the following when existing buildings are improved or expanded, or when new single-story buildings are built:

- Parking to remain between buildings and front lot lines if it already exists.
- Buildings to retain their present setback when renovated, expanded, or reconstructed.
- Parking lots of 70 feet wide or less to be exempted from required trees and landscaping.
- Screening not be required except along lot lines facing Eton Street.
- Small footprint towers of 600 square feet or less should be allowed to exceed one story without causing the overall structure to be interpreted as over one story, invalidating the overlay allowances.

Development review should allow the unique nature of the district to continue when single story structures are improved or expanded, or when new single-story buildings are built, including the following:

- The wall cladding may be any material including raw concrete block, corrugated metal, wood, or brick.
- Awnings and canopies of any size may be used.
- Artificial sod should be encouraged over pavement where there is no parking. Other than artificial sod no landscaping should be required.
- Sidewalks should not be required to be added, preferring shared-use street conditions with bollards to define sidewalks.
- Large expanses of walls should be painted with murals.

In the near-term, the Lower Rail District should remain informal and somewhat experimental.

To support the district's current character and prepare for the future, streetscape improvements should be pursued which work for both. While mentioned in a number of existing contexts in other districts, shared-use streets are ideal for implementation along Cole and Commerce Streets, and Lincoln to the East of Eton. For the current condition, shared-use formalizes the condition that has occurred organically over time, and provides greater importance to pedestrians and cyclists. In the long-term condition, it helps to retain the character of the district, with greater use of shared-use streets than other places in the City.

RECOMMENDATIONS

1. Create an Overlay District for the Lower Rail District that implements the zoning adjustments discussed above and activates more lenient development review decision making.
2. Construct a shared-use street section along Cole and Commerce Streets.
3. Update the 1999 Eton Road Corridor Plan of 1999 for the area south of Palmer Street by including the following:
 - So long as the buildings--existing or new--are one story, eliminate all requirements of Section 5 of the Site Design Guidelines p 41-46. of the Eton Road Corridor Plan. These include but are not limited to:
 - ◊ Eliminating building frontage and sidewalk requirements.
 - ◊ Eliminating parking requirements, except as the on-street parking shall be as determined by the "Immediate Neighbors" of the adjacent Torry or Kenning Neighborhoods.
 - ◊ Eliminating the signage and landscaping requirements.
 - ◊ Eliminating building use and aesthetic requirements.

C. Mixed-use Districts

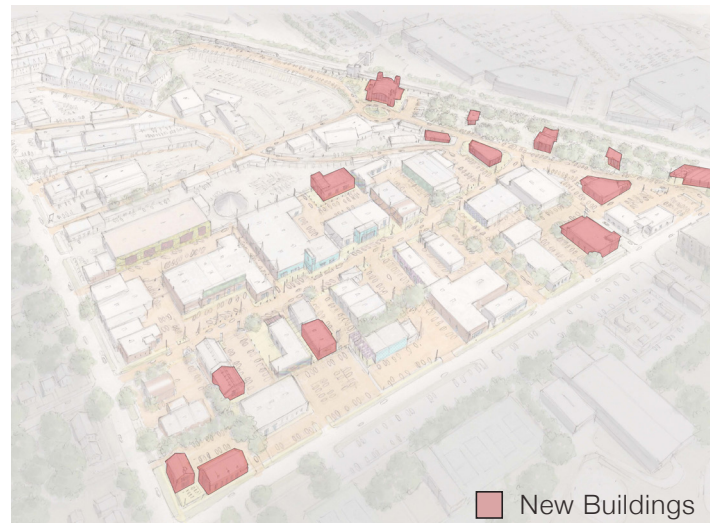
C.6. Rail District



■ New Roads



■ Pedestrian Connections



■ New Buildings



■ Art Graffiti Walls



■ Additions: Towers, Canopies, and Signage

Figures C.6-09 C.6-14. Near-term interventions to extend and improve upon the area's current character.

C. Mixed-use Districts

C.6. Rail District

Long-term Conditions

OBSERVATIONS

From a long-term perspective, local and regional connections are a limiting factor in the Rail District. Like the Triangle District, it requires public investment to incentivize development due to the lotting pattern.

DISCUSSION

Many of the City's district-specific plans have become long-range, with investment remaining focused Downtown. Beyond the clear draw of Downtown's reputation, building there removes a \$50,000 per car obligation from developers, which is \$7,500,000 for 100 apartments or about \$5,625,000 for a 25,000 square foot office building. Both the Triangle and Rail Districts suffer from lot patterns that are generally small and include a number of oddly shaped properties. Redeveloping these properties at a high capacity doesn't easily pencil when parking must be accommodated. On the other hand, Downtown's small properties are being developed, including the recent hotel at Brown and Old Woodward. Like the Triangle District, the Rail District needs public parking capacity and the ability to use that capacity in lieu of providing parking in mixed-use development projects. As opposed to the Triangle District, the City owns property in the rail district. Most notably, the Public Services Department site is well located to provide parking access to Cole Street. Uses on site are necessary for maintenance of the City, and there are few places to relocate those uses. Even remaining on site, the DPS building is approximately the size of a parking structure, and may be part of a redevelopment plan to accommodate both. Additionally, the School District's underutilized bus lot can easily accommodate structured parking. These are options to be weighed in service of unlocking the area's development potential.

Before the district begins to see more intense development, its standards should be revisited. There are a number of ways that the MX standards differ from the Downtown Overlay standard, despite having similar desired physical outcomes. As discussed previously, zoning districts across the City that are similar in their desired outcome

should be consolidated. If not consolidated with Downtown and Triangle District zones, the MX zone should be carefully analyzed. A quick reading of zone standards passes muster, however some details have potential negative consequences. For instance, the zone has tree requirements tied to the number of residential units; because this doesn't account for potentially high lot coverage, the tree requirements could be a barrier to development, disincentivizing new housing in the district.

Connectivity is the most significant limitation to the Rail District. The Grand Trunk Railroad limits all modes of connectivity, with crossings only at Maple and 14 Mile, of which the Maple crossing is in poor condition. Additional rail crossings should be studied, mainly for pedestrian and bicycle movement. A vehicular bridge would be logical at Lincoln, like the Derby bridge, though difficult to achieve due to existing buildings, including the new senior living building on Lincoln. In the further future, with significant development in the Rail District, further connections will be necessary. Every effort should be made to avoid increased car trips from new development, providing extensive pedestrian, bicycle, and transit infrastructure instead.

ACCESS TO THE TRAIN STATION

Over Birmingham's long history, the railroad connection to Detroit has been an important asset. In recent decades, disinvestment in rail and investment in automobiles has reduced the role of rail travel. However, this trend is slowly changing across the country. Into the future, rail's comeback is projected to continue. The City cannot risk being left without a direct connection to passenger rail. Looking forward a few decades, rail access in the Rail District is the most likely economic driver.

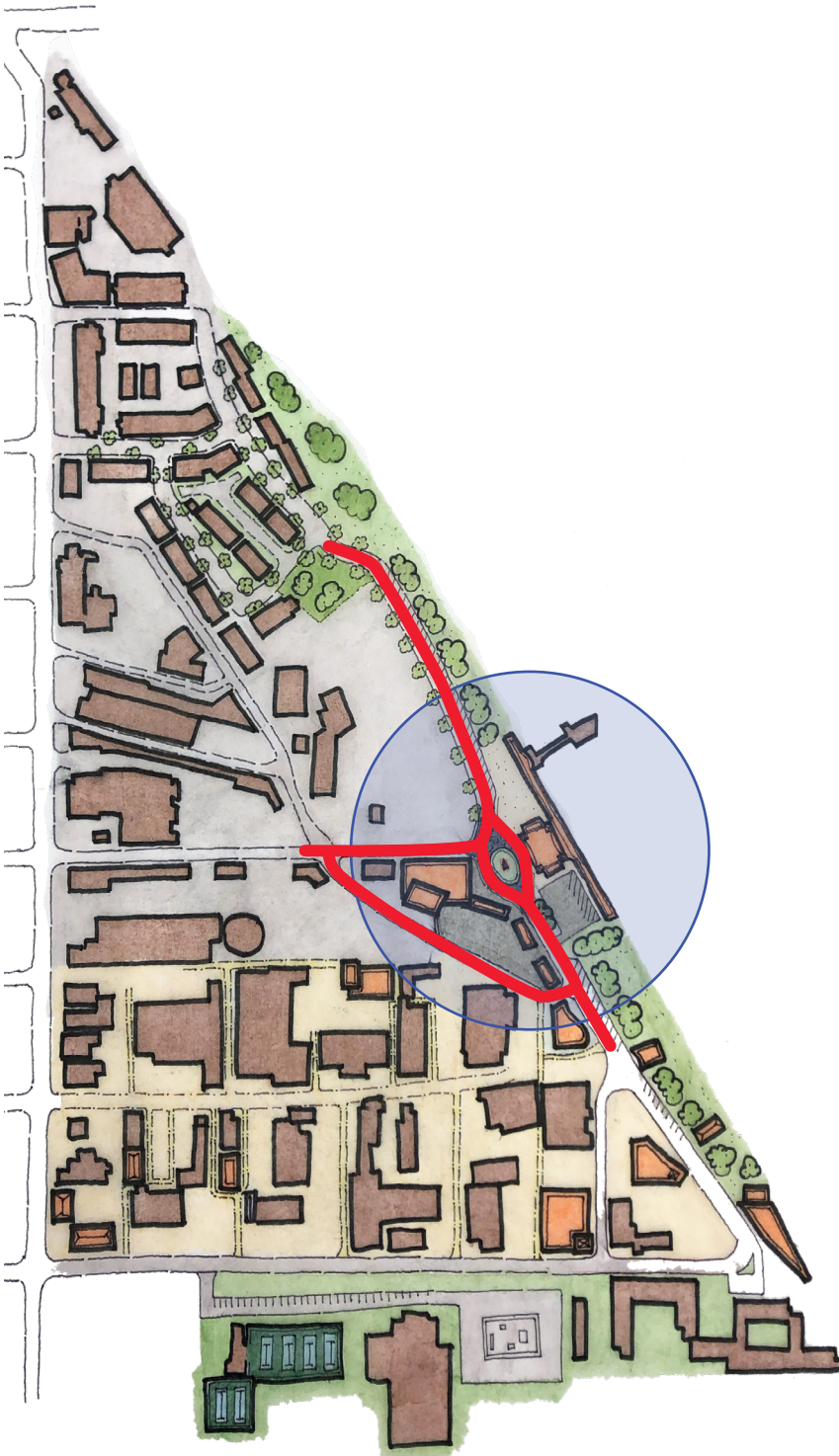
The City has recently attempted to negotiate access to the Troy Transit Center unsuccessfully. While the School District is willing to work with the City, a private land owner is not. The City should make another attempt at connection




Connectivity is the most significant limitation to the Rail District.

C. Mixed-use Districts

C.6. Rail District

Future Potential



-  Location of train station addition
-  Property blocking connection
-  Future road extensions

Existing Conditions



TRAIN STATION ACCESS

The City has recently attempted to negotiate access to the Troy Transit Center unsuccessfully. While the School District is willing to work with the City, a private land owner is not. The City should make another attempt at connection with this land owner. Should they be unwilling to participate, the City should not be afraid to exercise its' power to condemn property. Eminent domain has become a bad word in planning, particularly in a property rights focused place like Michigan. However, the tool is specifically designed for this type of situation, wherein a transportation connection is critical to the City's future success. Property owners are paid fair market value for the property, independently assessed. Ideally an agreement would be reached with the property owner, not requiring condemnation. However, the City has gone down this path before, resulting in City Hall, the Library, and Shain Park.

Figure C.6-15. Existing conditions and future potential of access to Troy Transit Center.

C. Mixed-use Districts

C.6. Rail District

RECOMMENDATIONS

- Zoning should be modified such that the MX District is exempt from LA-01 (E) and (F), as is true in Downtown, or at a minimum that plantings in the MX District are only required within the streetscape and within open areas of the property, but not based on a minimum number of trees per residential unit as currently defined.
- MX District zoning should be carefully analyzed by contracting two or more architects to complete preliminary building designs for mixed-use buildings on existing sites, small and large, with and without on-site parking, attempting to achieve capacity. The architects should be requested to discuss and present challenges and constraints that are faced in the process. While some challenges are part of code design, others may be unknown without testing.
- Update the 1999 Eton Road Corridor Plan for the area south of Palmer Street by including the following:
 - ◊ At the termination of Holland Street, creating a connection to the rail station by purchasing a 30 ft wide corridor or easement.
- Acquire access to the Troy Transit Center from the School District.
- Acquire access to the Troy Transit Center from remaining property owners using through negotiation, failing which through eminent domain.
- Pursue development of a public parking structure in the Rail District on a site with adequate access to the Lower Rail District and the future connection to the Troy Transit Center.

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*Summary of The Birmingham Plan 2040 (Draft) Recommendations
and Dates of Planning Board Review*

**Haynes Square, South Woodward Gateway, Rail District –
October 14, 2020**

Create a parking assessment or incremental tax district as necessary for land purchases and for financing the development of parking structures. Decades ago, a similar process was used to build downtown's five structures.	
Reconfigure the streets around Haynes Square to create the square and fix the acute intersection between Woodward and Old Woodward.	C.4-03
Build the public square with a cafe and trees to block noise from Woodward, seating, a kids play area, and other compelling civic features.	C.4-04
Purchase properties and implement Worth Park and other public realm enhancements, including civic art, streetscaping, traffic calming, and way-finding.	
Build a public parking deck on the east side of Haynes Square, in the Walgreens parking lot as has been negotiated.	
Build Worth Park.	
Create a brand for the Haynes Square, reinforced with special signage, landscaping, street furnishings, and building design standards.	
Install enhanced streetscape and landscape improvements along Bowers, Haynes, and Webster.	
Develop Worth Street as a shared-use streetscape.	
Improve pedestrian linkages to the surrounding neighborhoods, especially along Adams.	
Trade developable land and install a public surface parking lot along the south Old Woodward alley.	
Create a parking district for Haynes Square which allows residences to purchase parking passes in public garages, as discussed for Maple and Woodward.	
Install metered, on-street parking along Adams and Lincoln Roads.	
Create subdivision and zoning standards to encourage redevelopment of the Adam's Square shopping center, offering significant development capacity in exchange for a public open space and public parking.	
Implement pedestrian-walkway improvements along Woodward to improve the walkability to both downtown and the market districts.	
Identify an alley segment to use as a pilot project. This segment should have generally underutilized parking and intermittent buildings, like the segment between Humphrey and Bennaville on the east side, or underutilized parking and businesses that may be willing to open rear entrances, like the segment between Bennaville and Chapin on the east side.	C.5-04
Pilot a shared-use alley at the selected segment by re-paving the alleyway and working with property owners to infill housing along the triangular parcels and open existing buildings to the alley. Power poles should be relocated underground during the alley development.	C.5-04 C.4-05-08

*Summary of The Birmingham Plan 2040 (Draft) Recommendations
and Dates of Planning Board Review*

Incentivize redevelopment through increased zoning capacity and reduced parking requirements	
<p>Establish a zoning district to allow for and incentivize redevelopment in this format, including:</p> <ul style="list-style-type: none"> a. Requiring storefronts along neighborhood streets. b. Requiring parking be located midblock. c. Requiring storefronts along the alley. d. Requiring housing along the neighborhood-side of the alley, limited to 2 stories. e. Permitting townhouses and mixed-use housing along the neighborhood-side of the alley. f. Permitting multi-family housing on the commercial properties. g. Permitting 2 stories along the alley and 3 or 4 stories between Woodward and 50 feet of the alley. h. Reducing parking requirements and allowing shared parking. 	
<p>Pilot the Neighborhood Sleeve option where the shared-use alley is implemented by:</p> <ul style="list-style-type: none"> a. Striping on-street parking. b. Constructing chicanes on the neighborhood side of the alley. c. Revising the streetscape between the alley and Woodward, paving the planter with pervious pavers and providing seating, trash and recycling cans, pedestrian-scaled lighting, bike racks, and other streetscape elements typical in commercial districts. 	<p>C.5-13 C.5-17 C.5-18</p>
Create an Overlay District for the Lower Rail District that implements the zoning adjustments discussed above and activates more lenient development review decision making.	C.6-08
Construct a shared-use street section along Cole and Commerce Streets.	
<p>Update the 1999 Eton Road Corridor Plan of 1999 for the area south of Palmer Street by including the following:</p> <p>So long as the buildings--existing or new--are one story, eliminate all requirements of Section 5 of the Site Design Guidelines p 41-46. of the Eton Road Corridor Plan. These include but are not limited to:</p> <ul style="list-style-type: none"> Eliminating building frontage and sidewalk requirements. Eliminating parking requirements, except as the on-street parking shall be as determined by the "Immediate Neighbors" of the adjacent Torry or Kenning Neighborhoods. Eliminating the signage and landscaping requirements. Eliminating building use and aesthetic requirements. 	
Zoning should be modified such that the MX District is exempt from LA-01 (E) and (F), as is true in Downtown, or at a minimum that plantings in the MX District are only required within the streetscape and within open areas of the property, but not based on a minimum number of trees per residential unit as currently defined.	
MX District zoning should be carefully analyzed by contracting two or more architects to complete preliminary building designs for mixed-use buildings on existing sites, small and large, with and without on-site parking, attempting to achieve capacity. The architects should be requested to discuss and present challenges and constraints that	

*Summary of The Birmingham Plan 2040 (Draft) Recommendations
and Dates of Planning Board Review*

are faced in the process. While some challenges are part of code design, others may be unknown without testing.	
Update the 1999 Eton Road Corridor Plan for the area south of Palmer Street by including the following: ♦ At the termination of Holland Street, creating a connection to the rail station by purchasing a 30 ft wide corridor or easement.	
Acquire access to the Troy Transit Center from the School District.	C.6-15
Acquire access to the Troy Transit Center from remaining property owners using through negotiation, failing which through eminent domain.	C.6-15
Pursue development of a public parking structure in the Rail District on a site with adequate access to the Lower Rail District and the future connection to the Troy Transit Center.	

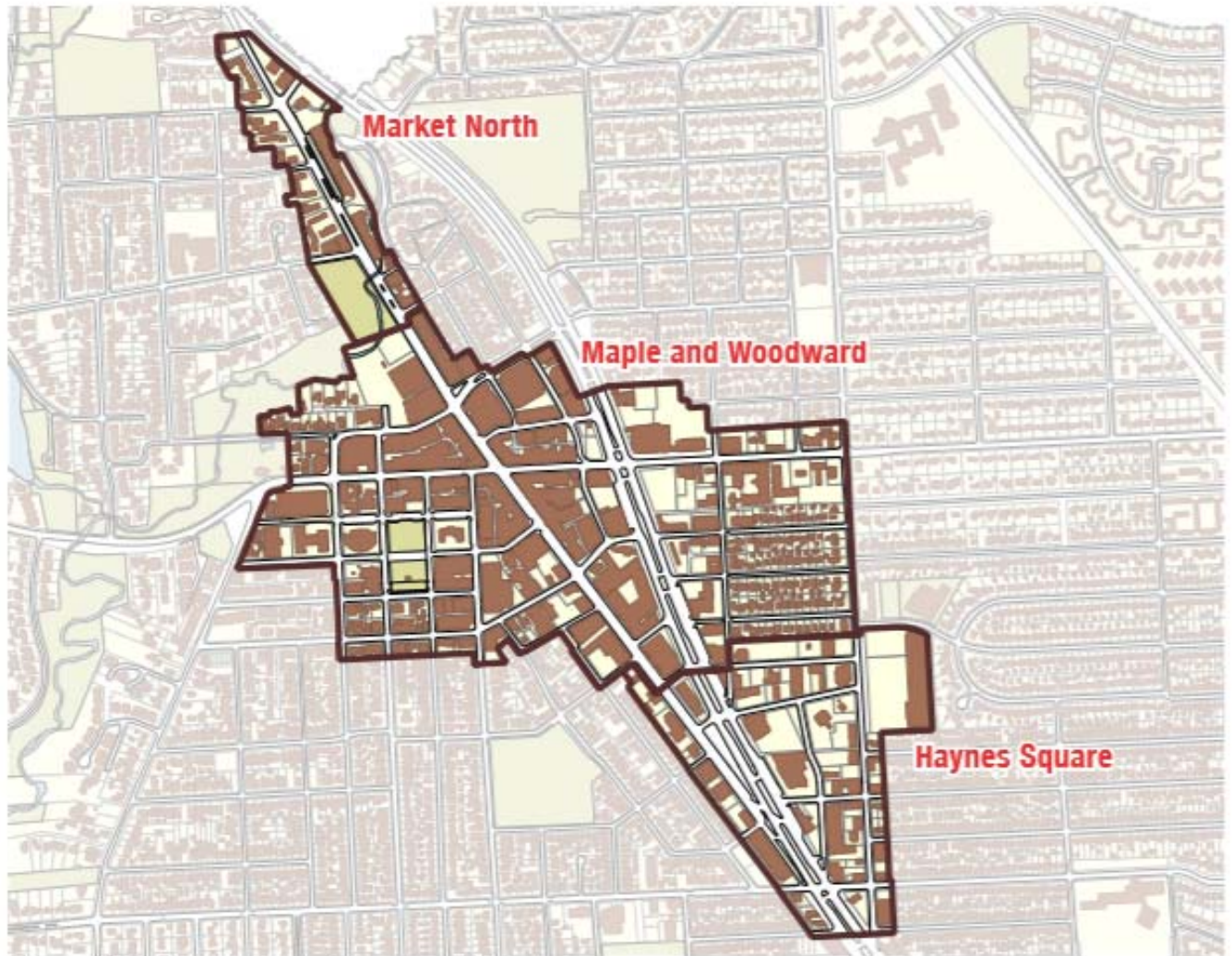


Figure C.1-01. Three districts of Downtown Birmingham.

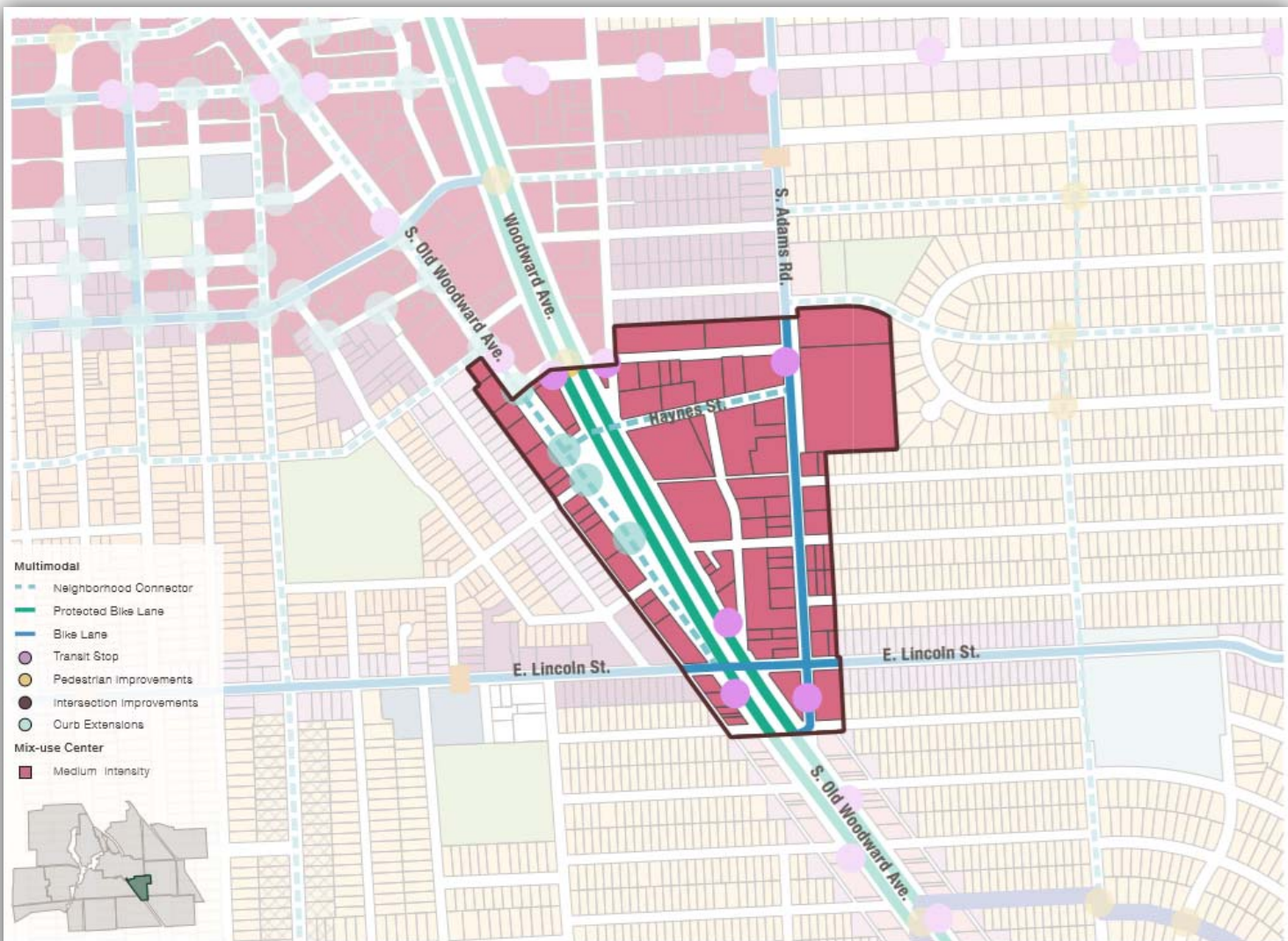


Figure C.4-01. Haynes Square.



Figure C.2-12. Growth capacity West of Woodward on sites with 1 and 2 story buildings.



Figure C.4-04. Redevelopment of Haynes Square.



Figure C.6-04. Shared-use alley space.



Images C.4-05-08. Shared-use paving examples from the 2013 Southern Gateway Urban Design Plan.



Figure C.6-13. Neighborhood Sleeve block configuration which consolidates parking mid-block and creates small neighborhood-focused nodes along the east-west streets.

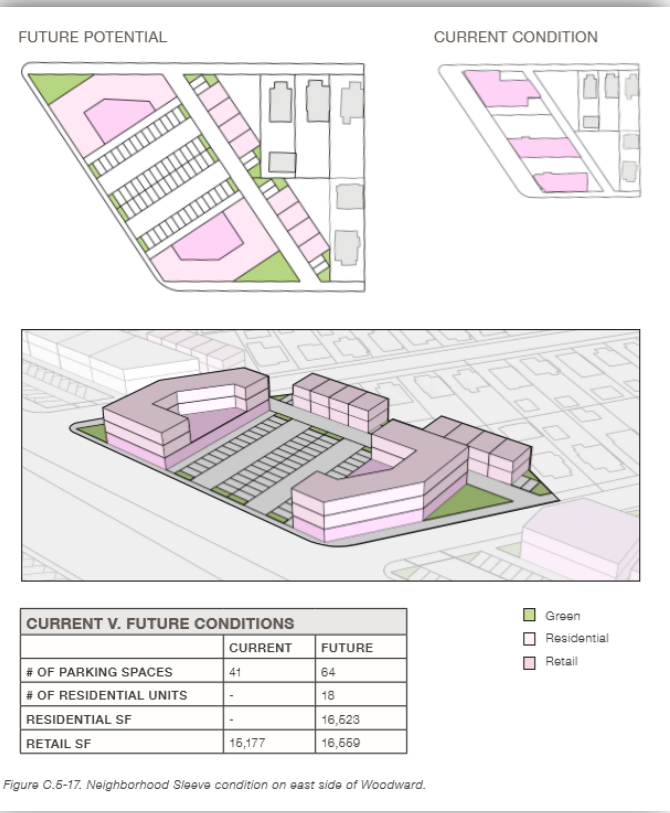
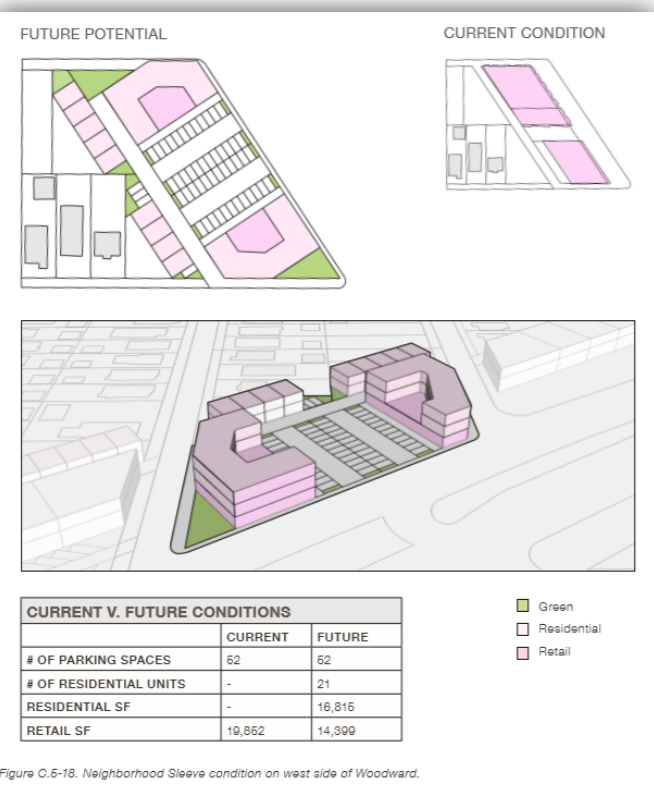







Figure C.6-08. The Lower Rail District.

Future Potential



-  Location of train station addition
-  Property blocking connection
-  Future road extensions

Existing Conditions



Figure C.6-15. Existing conditions and future potential of access to Troy Transit Center.

Administrative Approval Application Planning Division

Form will not be processed until it is completely filled out

1. Applicant

Name: Sam Surnow
Address: 320 Martin Street, Suite 100
Birmingham, mi 48009
Phone Number: 248.877.4000
Fax Number: _____
Email: sam@surnow.com

Property Owner

Name: Chester Street Partners, LLC
Address: 320 Martin Street, Suite 100
Birmingham, mi 48009
Phone Number: 248.877.4000
Fax Number: _____
Email: _____

2. Applicant's Attorney/Contact Person

Name: Kevin Biddison, AIA
Address: 320 Martin Street, Suite 10
Birmingham, mi 48009
Phone Number: 248.554.9500
Fax Number: _____
Email: kb@biddison-ad.com

Project Designer

Name: Kevin Biddison, AIA
Address: 320 Martin Street, Suite 10
Birmingham, mi 48009
Phone Number: 248.554.9500
Fax Number: _____
Email: kb@biddison-ad.com

3. Project Information

Address/Location of Property: 191 North Cester Street

Name of Development: The Jeffrey
Parcel ID #: _____
Current Use: Office-Commercial Building
Area in Acres: 0.40 Acres
Current Zoning: TZ2

Name of Historic District site is in, if any: _____
Date of HDC Approval, if any: _____
Date of Application for Preliminary Site Plan: _____
Date of Preliminary Site Plan Approval: _____
Date of Application for Final Site Plan: _____
Date of Final Site Plan Approval: _____
Date of Revised Final Site Plan Approval: _____

4. Attachments

- Warranty Deed with legal description of property
- Authorization from Owner(s) (if applicant is not owner)
- Completed Checklist
- Material Samples
- Digital Copy of plans
- Two (2) folded copies of plans including an itemized list of all changes for which administrative approval is requested, with the changes marked in color on all elevations

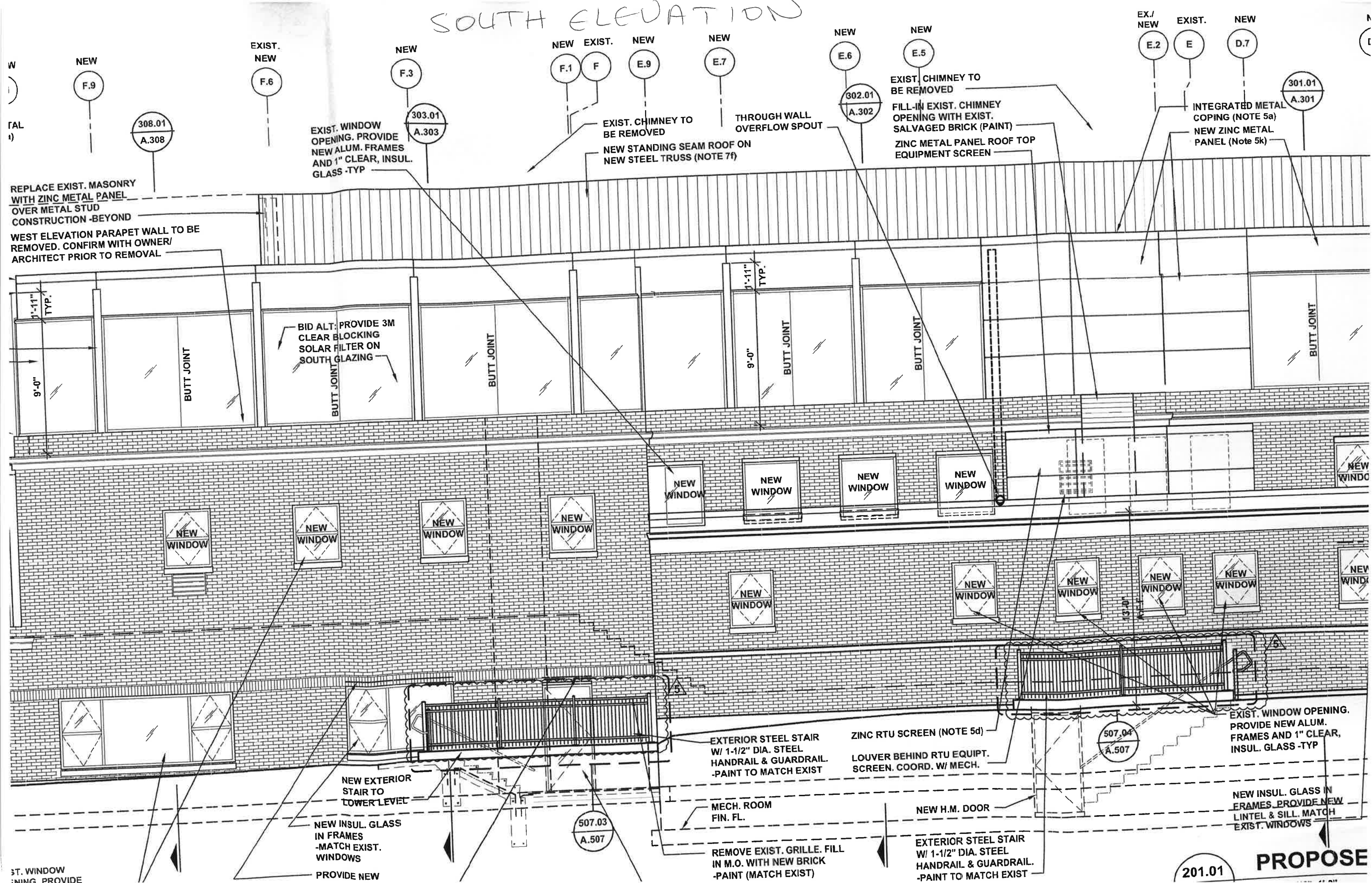
5. Details of the Request for Administrative Approval

The undersigned states the above information is true and correct, and understands that it is the responsibility of the applicant to advise the Planning Division and/or Building Division of any additional changes to the approved site plan.

Signature of Applicant: *Sam Surnow* Date: 09-29-2020

Office Use Only			
Application #: <u>20-0104</u>	Date Received: _____	Fee: _____	
Date of Approval: <u>10/2/2020</u>	Date of Denial: _____	Reviewed by: <u><i>John 3</i></u>	

SOUTH ELEVATION



PROVIDE NEW LIMESTONE SILL
-MATCH EXISTING

202.02
A.202

PROPOSED NORTH ELEVATION

SCALE: 3/16"=1'-0"

EXIST. WINDOW &
FRAMES TO BE REMOVED
- TYP. OF 10

EXIST. ENTRY DOOR
W/ CLEAR TEMP. GLASS
AND SIDELITE IN EXIST.
MASONRY OPENING

NEW ZINC METAL
PANELS

NEW BRICK STAIR
ENCLOSURE -BEYOND
(PAINT. MATCH EXIST)

NEW
0.5

NEW
1.4

NEW
2.5

NEW
3.7

306.01
A.306

NEW
5.2

309.01
A.309

NEW
6.8

NEW
8

ROOF OVERFLOW
SCUPPER

NEW STANDING SEAM
ROOF ON STEEL
TRUSSES. RE-USE EXIST.
T&G WOOD DECKING

ROOF SCUPPER
-BEYOND

ELEVATOR
SHAFT-BEYOND

T.O.MASONRY
@ 37'-0 1/2" A.F.F.

T.O.ZINC METAL PANEL (EAST ADDITION)
@ 36'-7" A.F.F.

T.O.MASONRY @ NORTH STAIR
@ 32'-8 3/4" A.F.F.

@ 31'-10 1/2" A.F.F.

T.O. GLAZING
@ 9'-0" A.F.F.

EXIST. WEST PARAPET
WALL TO REMAIN

EXIST. BRICK - PAINT

SILL ELEVATION
@ 1'-3" A.F.F.
2ND FINISH FLOOR (800.08)
@ 19'-9 1/2"

ROOF OVERFLOW
SCUPPER

BID ALT: PROVIDE 3M
CLEAR BLOCKING
SOLAR FILTER ON
WEST GLAZING

NEW 1ST FLOOR (788.51)
@ 7'-6 1/2"

EXIST. BRICK - PAINT

EXIST. OFFICE FLOOR (782.98) @
@ 1'-10 1/2"

MAIN ENTRY FIN FLOOR (781.10)
@ 0'-0" A.F.F.

INTAKE GRILLE

OWER LEVEL (776.81)
@ 4'-3-1/2" B.F.F.

EW NORTH WALL
FOUNDATION - UNDERPIN
O THE EXIST.

PROVIDE NEW FRAMES
AND INSUL. GLASS ON
EXIST. MASONRY
OPENING. -TYP

EXHAUST HOOD

EXIST. DOOR TO BE
REMOVED AND REPLACED
WITH NEW ANOD. ALUM
ENTRY DOOR W/ CLEAR
TEMP. GLASS

WEST
ELEVATION



CITY OF BIRMINGHAM
Date 09/18/2020 12:06:58 PM
Ref 00172802
Receipt 546391
Amount \$100.00

Administrative Approval Application Planning Division

APPROVED

Form will not be processed until it is completely filled out.

9/18/2020
PAA20-0096

1. Applicant

Name: MasTec Network Solutions, LLC
Address: 1351E Irving Park Road, Itasca, IL 60143
Phone Number: 630-634-4366
Fax Number: 630-250-1353
Email Address: jeffrey.harp@mastec.com

2. Property Owner

Name: TOWER OWNER: Crown Castle USA
Address: 9045 River Road, Suite 425, Indianapolis, IN 46240
Phone Number: 724-743-6362
Fax Number: 724-416-4100
Email Address: hubert.lykins@crowncastle.com

3. Applicant's Attorney/Contact Person

Name: Jeffrey Harp, Construction Manager
Address: 1351E Irving Park Road, Itasca, IL 60143
Phone Number: 765-401-1480
Fax Number: 630-250-1353
Email Address: jeffrey.harp@mastec.com

4. Project Designer/Developer

Name: Jordan Shelley (Tower Engineering Professionals)
Address: 326 Tryon Road Raleigh, NC 27603
Phone Number: 919-661-6351
Fax Number: 919-661-6350
Email Address: CMRP@TEPGROUP.NET

5. Project Information

Address/Location of Property: 300th Strathmore, Birmingham, MI 48009
Name of Development: N/A
Parcel ID#: 08-19-24-451-001
Current Use: Cell Tower Site
Area in Acres: 28.77
Current Zoning: ME

Name of Historic District if any: N/A
Date of HDC Approval, if any: N/A
Date of Application for Preliminary Site Plan:
Date of Preliminary Site Plan Approval:
Date of Application for Final Site Plan:
Date of Final Site Plan Approval:
Date of Revised Final Site Plan Approval:

6. Required Attachments

- Warranty Deed with legal description of property
- Authorization from Owner(s) (if applicant is not owner)
- Completed Checklist
- Material Samples
- Specification sheets for all proposed materials, fixtures, and/or mechanical equipment
- One (1) digital copy of plans
- Two (2) folded copies of plans including an itemized list of all changes for which administrative approval is requested, with the changes marked in color on all elevations
- Photographs of existing conditions on the site where changes are proposed

7. Details of the Request for Administrative Approval

Requesting building permit to perform tower modifications to existing cell tower located at 300 Strathmore in Birmingham, MI. We will be bolting steel flat plate reinforcing to the sides of the existing monopole shaft, and installing core drilled anchor rods into the existing foundation. Structure height and footprint will not be changed.

The undersigned states the above information is true and correct, and understands that it is the responsibility of the applicant to advise the Planning Division and/or Building Division of any additional changes to the approved site plan.

Signature of Applicant: Jeff Harp

Digitally signed by Jeff Harp
Date: 2020.09.06 07:58:49 -05:00

Date: 9-6-20

Office Use Only

Application #: PAA20-0096

Date Received: 9/17/2020

Fee: \$100.00

Date of Approval: 9/18/2020

Date of Denial: N/A

Reviewed By:



CONSENT OF PROPERTY OWNER

I, Michael Gasser on behalf of Crown Castle USA Inc., OF THE STATE OF Indiana AND
(Name of Property Owner)
COUNTY OF Marion STATE THE FOLLOWING:

1. That I am the owner of real estate located at (Wireless Tower Facility) 300 Strathmore in Birmingham, MI;
(Address of Affected Property)
2. That I have read and examined the Application for Administrative Approval made to the City of
MasTec Network Solutions, LLC by Jeffrey Harp (Construction Manager)
Birmingham by: _____;
(Name of Applicant)
3. That I have no objections to, and consent to the request(s) described in the Application made to the City of
Birmingham.

By providing your e-mail to the City, you agree to receive news notifications from the City. If you do not wish to receive these messages, you may unsubscribe at any time.

Name of Owner (Printed): Michael J Gasser

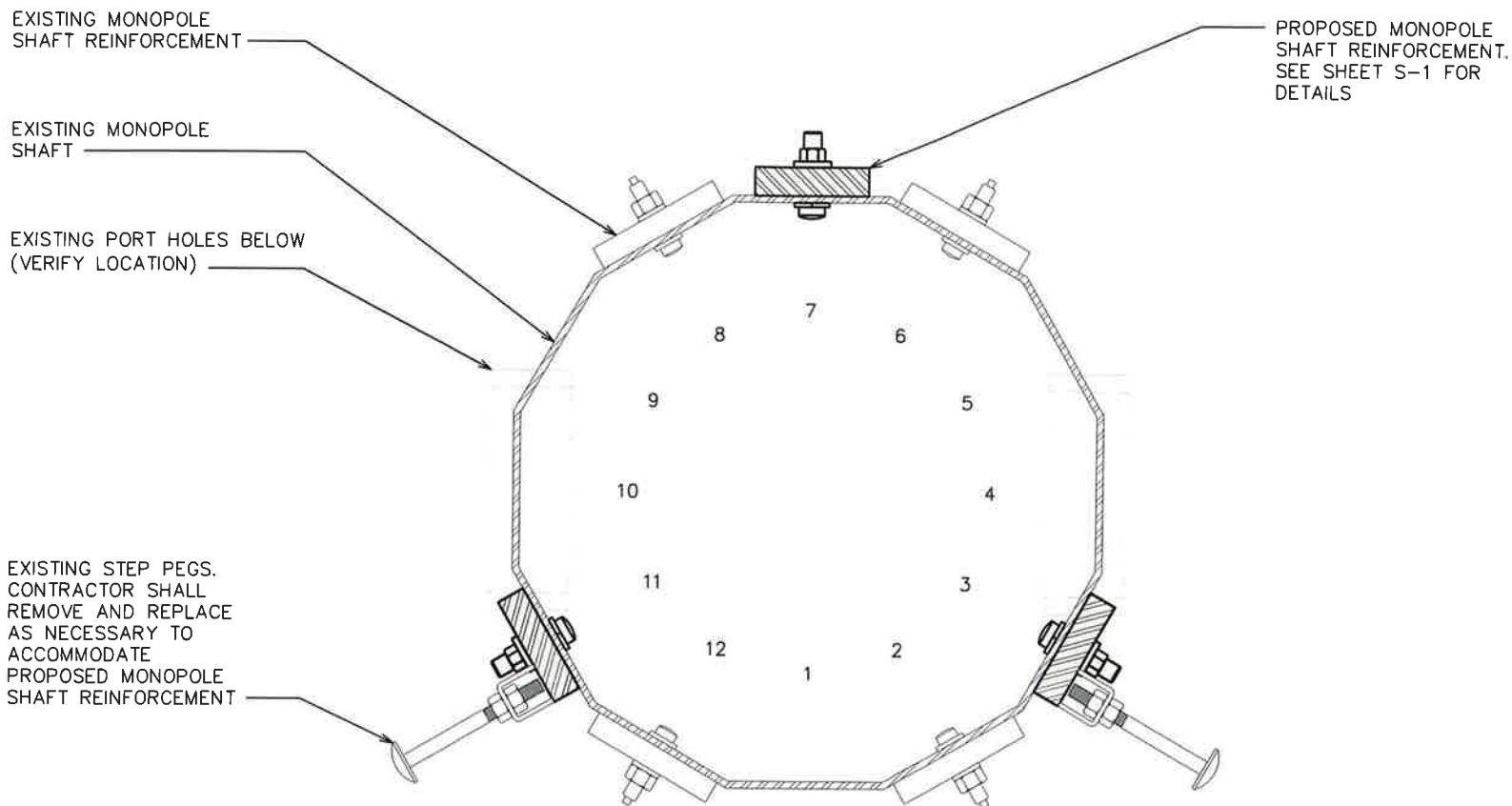
Signature of Owner: Michael J Gasser Digitally signed by Michael J Gasser
Date: 2020.09.09 09:01:36 -04'00' Date: 09/09/2020

THE TOWER SAFETY CLIMB WAS ASSUMED TO BE LOCATED OFF FLAT 1. FIELD VERIFY CLIMBING SYSTEM LOCATION PRIOR TO INSTALLATION. CONTACT TOWER OWNER AND ENGINEER OF RECORD SHOULD ANY DISCREPANCIES ARISE. CONTRACTOR TO REMOVE AND REPLACE CLIMBING SYSTEM USING ONLY APPROVED OPTIONS FROM CROWN CASTLE APPROVED REINFORCEMENT CATALOG AS NECESSARY TO INSTALL PROPOSED REINFORCEMENT. IF CLIMBING SYSTEM IS REQUIRED TO BE ATTACHED TO PROPOSED REINFORCEMENT, IT SHALL BE DONE PRIOR TO GALVANIZATION.



ATTENTION

THE TOWER SAFETY CLIMB WAS ASSUMED TO BE LOCATED OFF FLAT 1. FIELD VERIFY CLIMBING SYSTEM LOCATION PRIOR TO INSTALLATION. CONTACT TOWER OWNER AND ENGINEER OF RECORD SHOULD ANY DISCREPANCIES ARISE. CONTRACTOR TO REMOVE AND REPLACE CLIMBING SYSTEM USING ONLY APPROVED OPTIONS FROM CROWN CASTLE APPROVED REINFORCEMENT CATALOG AS NECESSARY TO INSTALL PROPOSED REINFORCEMENT. IF CLIMBING SYSTEM IS REQUIRED TO BE ATTACHED TO PROPOSED REINFORCEMENT, IT SHALL BE DONE PRIOR TO GALVANIZATION.



PLANS PREPARED FOR:

CROWN CASTLE

2000 CORPORATE DRIVE
CANONSBURG, PA 15317

PROJECT INFORMATION:

BIRMINGHAM 3
BU #: 840273

300 STRATHMORE
BIRMINGHAM, MI 48009
(OAKLAND COUNTY)

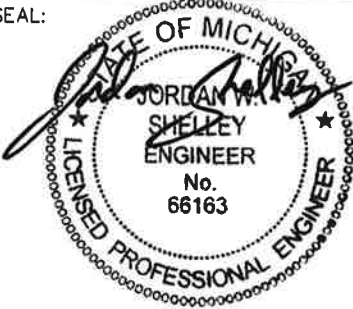
PLANS PREPARED BY:



TOWER ENGINEERING PROFESSIONALS

326 TRYON ROAD
RALEIGH, NC 27603
OFFICE: (919) 661-6351
www.tepgroup.net

SEAL:



June 12, 2020

0	06-12-20	MODIFICATION DRAWINGS
REV	DATE	ISSUED FOR:

DRAWN BY: TLI CHECKED BY: RKE

SHEET TITLE:

SECTION DETAILS

SHEET NUMBER:	REVISION:
S-3	0
	TEP#: 54605.421719

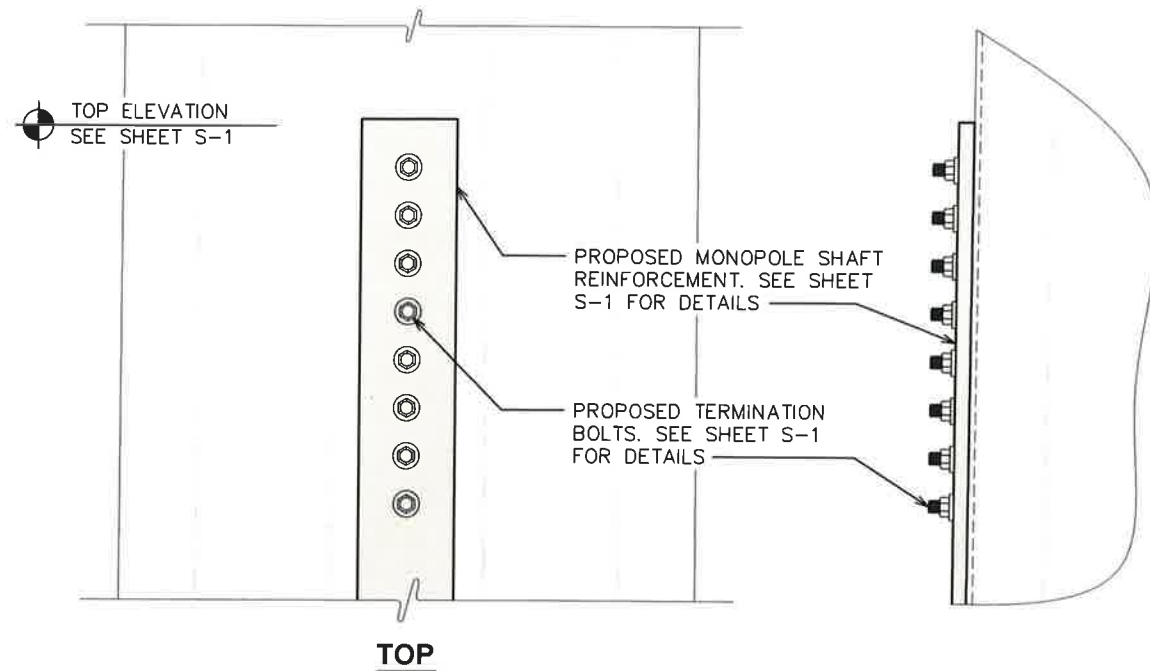
SECTION

SCALE: 1 1/2" = 1'-0"

B

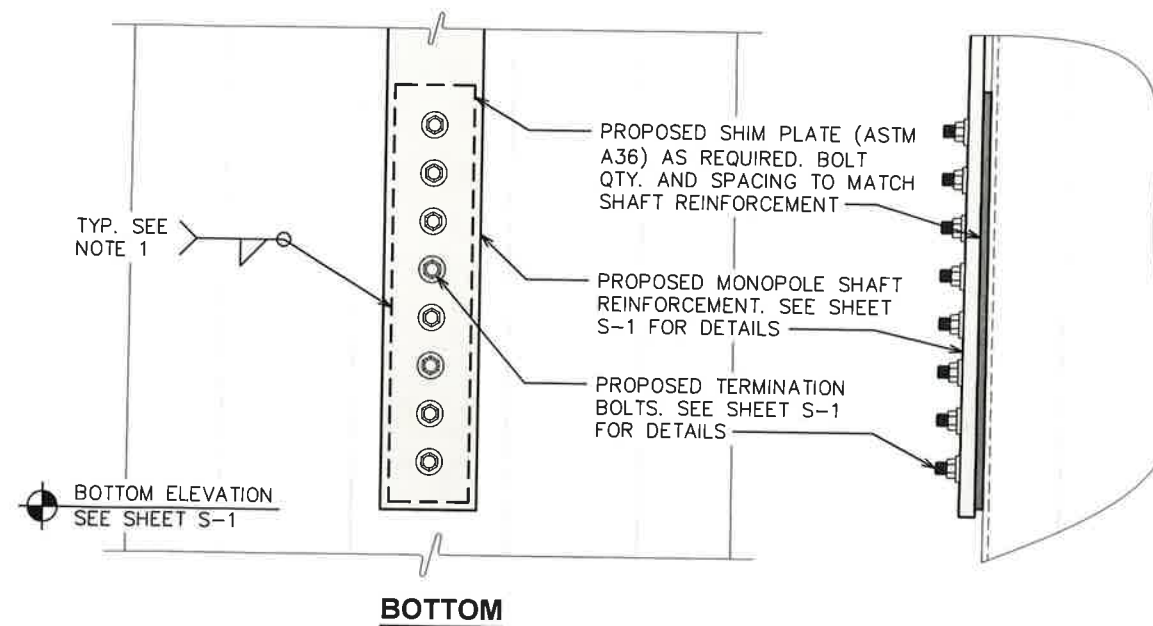


SCALE IN FEET



NOTE:

1. SHIMS GREATER THAN $\frac{1}{4}$ " IN THICKNESS LOCATED WITHIN THE TERMINATION LENGTH OF THE SHAFT REINFORCEMENT PLATE SHALL BE WELDED TO THE SHAFT REINFORCEMENT PLATE.



TOP TERMINATION DETAILS

SCALE: N.T.S.

3A

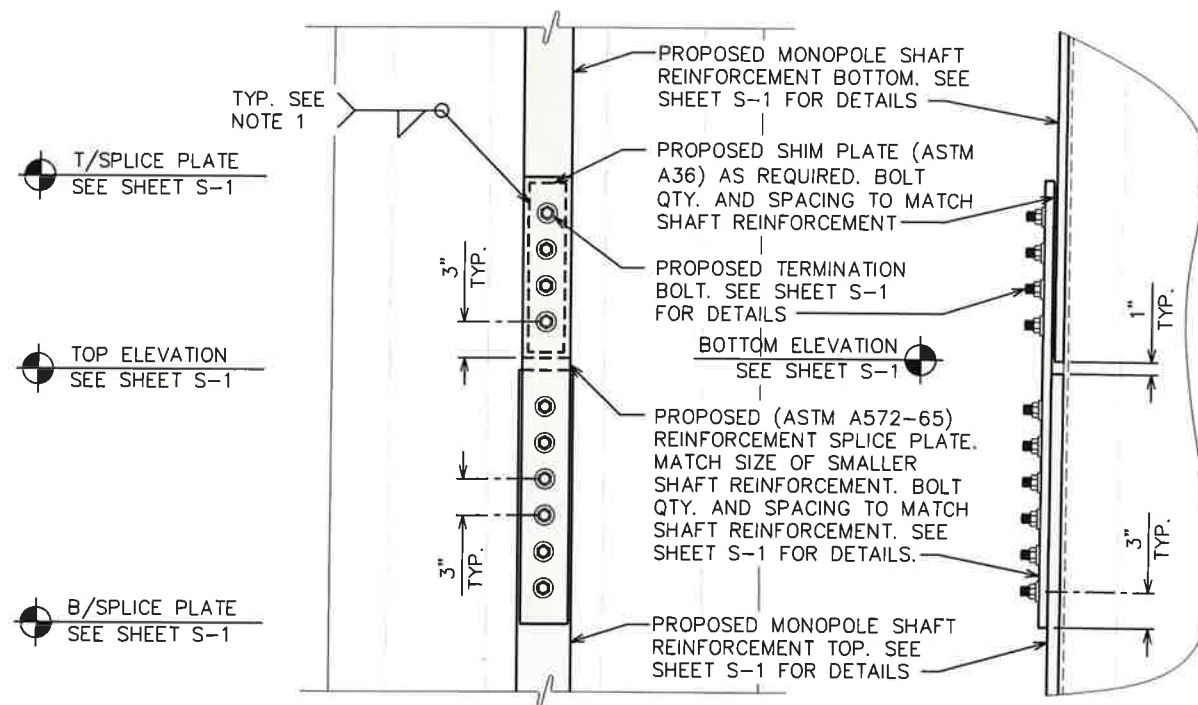
BOTTOM TERMINATION DETAILS

SCALE: N.T.S.

3B

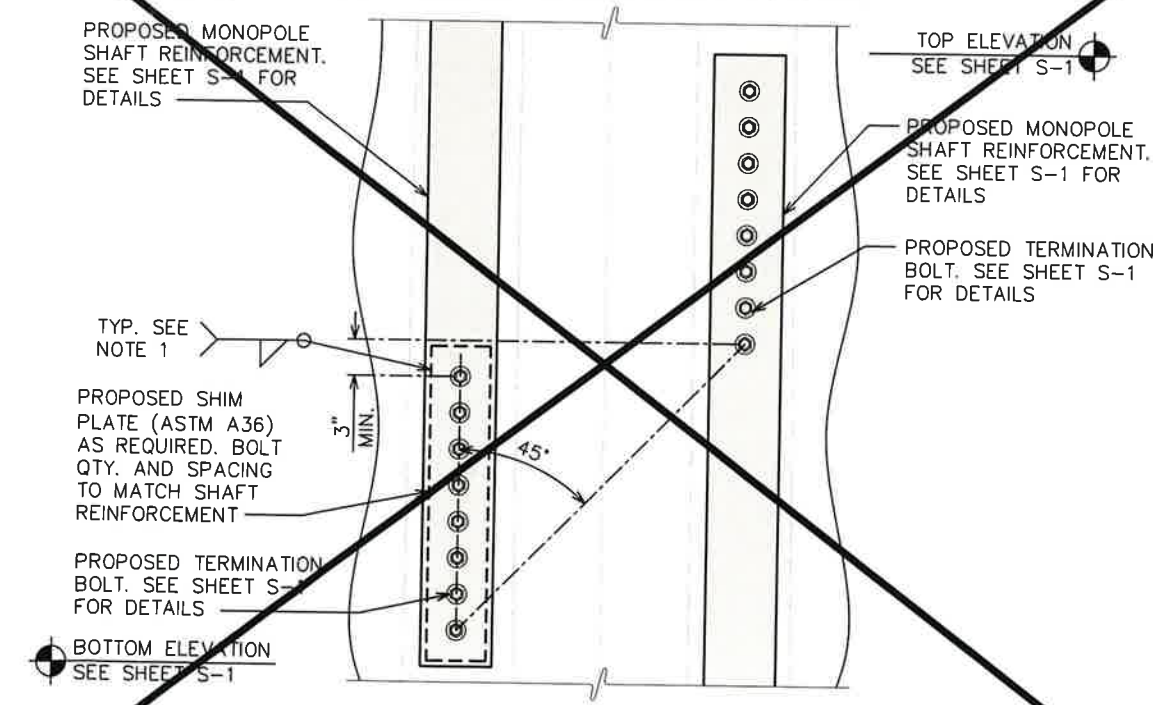
NOTE:

1. SHIMS GREATER THAN $\frac{1}{4}$ " IN THICKNESS LOCATED BETWEEN THE SHAFT REINFORCEMENT PLATE AND THE REINFORCEMENT SPLICE PLATE SHALL BE WELDED TO THE SHAFT REINFORCEMENT PLATE.



NOTE:

1. SHIMS GREATER THAN $\frac{1}{4}$ " IN THICKNESS LOCATED WITHIN THE TERMINATION LENGTH OF THE SHAFT REINFORCEMENT PLATE SHALL BE WELDED TO THE SHAFT REINFORCEMENT PLATE



REINFORCEMENT SPLICE DETAILS

SCALE: N.T.S.

4

OVERLAP SPLICE DETAILS

SCALE: N.T.S.

5

PLANS PREPARED FOR:

CROWN CASTLE

2000 CORPORATE DRIVE
CANONSBURG, PA 15317

PROJECT INFORMATION:

BIRMINGHAM 3 BU #: 840273

300 STRATHMORE
BIRMINGHAM, MI 48009
(OAKLAND COUNTY)

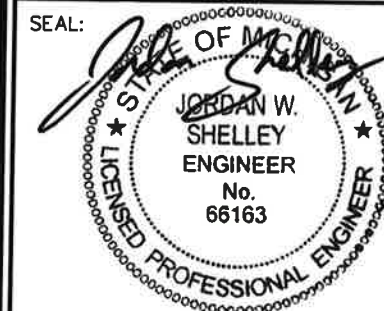
PLANS PREPARED BY:



TOWER ENGINEERING PROFESSIONALS

326 TRYON ROAD
RALEIGH, NC 27603
OFFICE: (919) 661-6351
www.tepgroup.net

SEAL:



June 12, 2020

0	06-12-20	MODIFICATION DRAWINGS
REV	DATE	ISSUED FOR:

DRAWN BY: TLI CHECKED BY: RKE

SHEET TITLE:

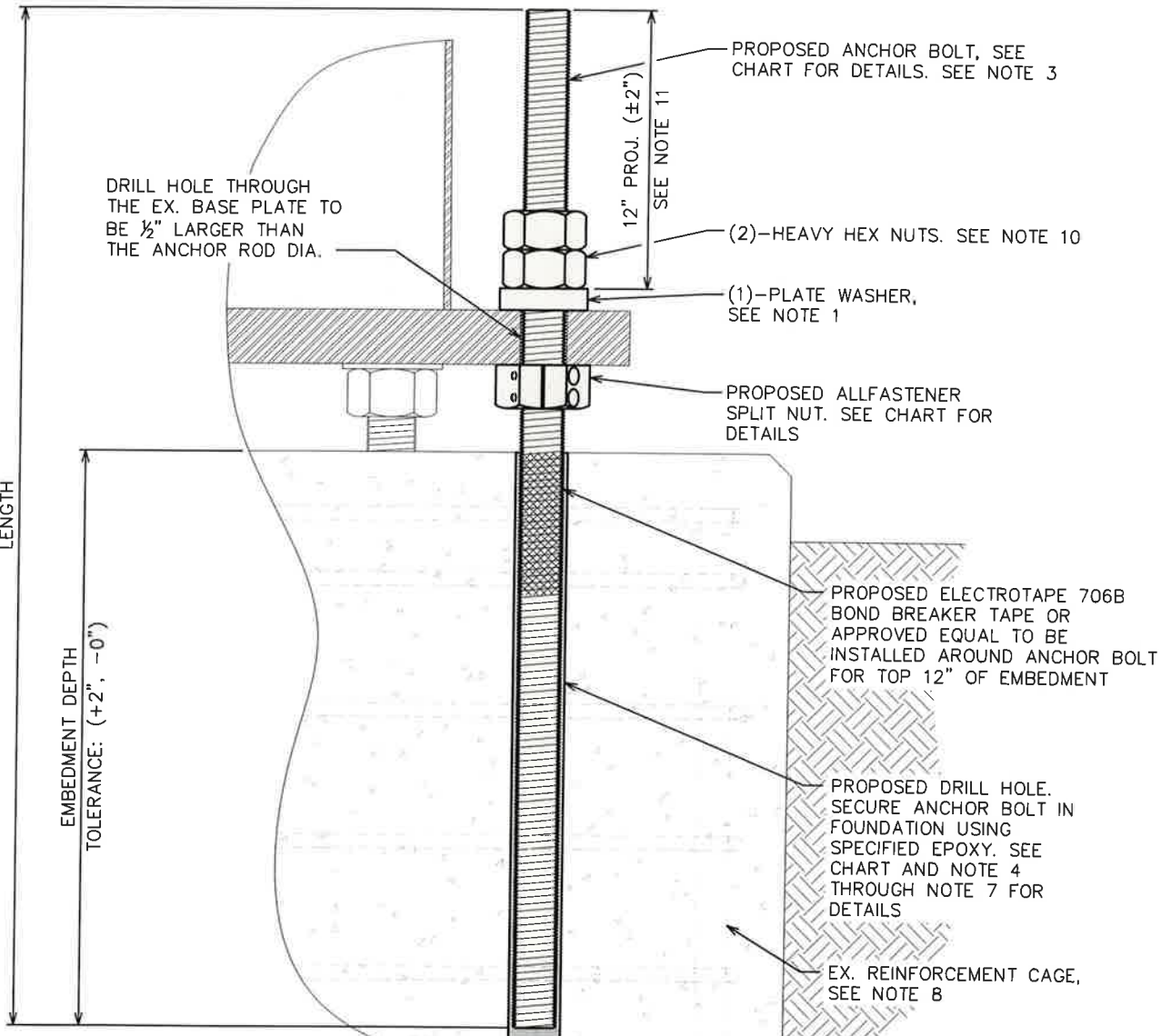
TYP. SHAFT REINFORCEMENT DETAILS

SHEET NUMBER: REVISION:

S-4

0

TEP#: 54605.421719



ANCHOR BOLT REINFORCEMENT

SCALE: N.T.S.

C

NOTES

1. PLATE WASHER SHALL FULLY BEAR ON THE BASE PLATE.
2. REFERENCE CED-CAT-10300 (CURRENT VERSION) FOR ANCHOR ROD DIMENSIONS.
3. RODS SHALL BE GALVANIZED FROM THE TOP OF THE PROJECTION TO 15" BELOW THE SURFACE OF THE CONCRETE, AT A MINIMUM.
4. CORED HOLES SHALL BE MECHANICALLY ROUGHENED USING A CARBIDE HOLE ROUGHENER OR EQUIVALENT. BRUSHING WITH A NYLON OR WIRE BRUSH SHALL BE USED IN THE PROCESS OF HOLE CLEANING, BUT DOES NOT SATISFY THE HOLE ROUGHENING REQUIREMENT.
5. FOLLOW EPOXY MANUFACTURER'S RECOMMENDATIONS FOR HOLE CLEANING.
6. ALL HOLES SHALL BE DRY PRIOR TO PLACING EPOXY.
7. FOLLOW EPOXY MANUFACTURER'S RECOMMENDATIONS REGARDING HANDLING OF THREADED ROD AND EPOXY, AS WELL AS ALL INSTALLATION INSTRUCTIONS AND REQUIREMENTS.
8. TAKE ALL MEASUREMENTS NECESSARY TO AVOID DAMAGING EXISTING REINFORCING BARS DURING CORING OPERATIONS. NOTIFY EOR IMMEDIATELY IF EXISTING REINFORCING BARS ARE ENCOUNTERED AND INTERFERE WITH PLACEMENT OF NEW ANCHORS. MINOR ADJUSTMENT TO PROPOSED LOCATION OF NEW ANCHORS MAY BE REQUIRED.
9. IF BASE PLATE GROUT REPAIR IS REQUIRED FOR ANCHOR ROD INSTALLATION, SEE ENG-STD-10323: BASE PLATE GROUT, FOR PROCEDURES AND RECOMMENDED MANUFACTURERS. CONTRACTOR SHALL DETERMINE THE QUANTITY REQUIRED.
10. ONCE ALL RESIN AND GROUT HAVE CURED, NEW ANCHOR ROD REINFORCING SHALL BE TARGET TENSIONED TO THE VALUE LISTED IN THE TABLE ON THIS SHEET. SEE CED-PRC-10119: PULL-OUT TESTING POST-INSTALLED ANCHOR RODS, FOR SPECIFICATIONS. AFTER ANCHOR BOLT LOAD TESTING IS COMPLETE, INSTALL FIRST NUT TO SNUG TIGHT PLUS 1/4 TURN BEFORE INSTALLING SECOND NUT.
11. CONTRACTOR SHALL VERIFY THAT A PULL TEST IS ABLE TO BE PERFORMED USING THE ANCHOR ROD PROJECTION SHOWN.
12. WHEN COMPLETED WITH EPOXY INSTALLATION, THE TOP OF THE EPOXY SHALL BE EQUAL TO OR HIGHER THAN THE TOP OF THE FOUNDATION, SUCH THAT WATER IS NOT ABLE TO COLLECT IN THE ANNULAR AREA AROUND THE EXPOSED PORTION OF THE ANCHOR ROD.

ANCHOR BOLT

DESCRIPTION	MEASUREMENT
PART NO.	CCI-AR-0225
ANCHOR BOLT DIA.	2 1/4"
TOTAL QUANTITY	4
MATERIAL	ASTM A193 GR. B7
DRILL HOLE DIAMETER	2 1/2"
TARGET TENSION	190 KIPS
EPOXY	ALLFASTENERS AF35LVE HILTI HIT-RE 500 V3
EMBEDMENT	6'-0" 6'-6"
LENGTH	7'-6" 8'-0"

ACCESSORIES

DESCRIPTION	MEASUREMENT
HEAVY HEX NUT	2 1/4"
MATERIAL	ASTM A563
TOTAL QUANTITY	8
DESCRIPTION	MEASUREMENT
ALLFASTENER SPLIT NUT	2SPN214M
MATERIAL	ASTM A563
TOTAL QUANTITY	4
DESCRIPTION	MEASUREMENT
PLATE WASHER	1/2" x 4 1/4" O.D. (2 3/8" I.D.)
MATERIAL	ASTM A572-50
TOTAL QUANTITY	4

PLANS PREPARED FOR:

CROWN CASTLE

2000 CORPORATE DRIVE
CANONSBURG, PA 15317

PROJECT INFORMATION:

BIRMINGHAM 3
BU #: 840273

300 STRATHMORE
BIRMINGHAM, MI 48009
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PLANS PREPARED BY:



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RALEIGH, NC 27603
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SEAL:



June 12, 2020

O	06-12-20	MODIFICATION DRAWINGS
REV	DATE	ISSUED FOR:
DRAWN BY:	TLI	CHECKED BY: RKE

SHEET TITLE:
ANCHOR BOLT
REINFORCEMENT
DETAILS

SHEET NUMBER:	REVISION:
S-5	0
TEP#:	54605.421719