

Mayor
• Robert Dandoy

City Manager
• Matt Andrews



Council Members
• Jan Burrell
• Joe Paul
• Bryon Saxton
• Diane Wilson
• Ann Jackson

ROY CITY COUNCIL WORK SESSION (ELECTRONIC)

APRIL 6, 2021 – 4:30 P.M.

No physical meeting location will be available. This meeting will be streamed live on the Roy City YouTube channel. <https://www.youtube.com/channel/UC6zdmDzxdOSW6veb2XpzCNA>

A. Welcome & Roll Call

B. Discussion Items

1. Proposed Mixed Use for Frontrunner Station and Innovation Center Areas

C. Adjournment

In compliance with the Americans with Disabilities Act, persons needing auxiliary communicative aids and services for these meetings should contact the Administration Department at (801) 774-1020 or by email: admin@royutah.org at least 48 hours in advance of the meeting.

Public meetings will be held electronically in accordance with Utah Code Section 52-4-210 et seq., Open and Public Meetings Act. Pursuant to a written determination by the Mayor finding that conducting the meeting with an anchor location presents a substantial risk to the health and safety of those who may be present due to the infectious and potentially dangerous nature of COVID -19 virus appropriate physical distancing in City Council Chambers is not achievable at this time accordingly, the meeting will be held electronically with no anchor location.

Pursuant to Section 52-4-7.8 (1)(e) and (3)(B)(ii) “Electronic Meetings” of the Open and Public Meetings Law, Any Councilmember may participate in the meeting via teleconference, and such electronic means will provide the public body the ability to communicate via the teleconference.

Certificate of Posting


The undersigned, duly appointed City Recorder, does hereby certify that the above notice and agenda was posted in a public place within the Roy City limits on this 1st of April, 2021. A copy was also provided to the Standard Examiner and posted on the Roy City Website and Utah Public Notice Website on the 1st of April, 2021.

Morgan Langholf
City Recorder

Visit the Roy City Web Site @ www.royutah.org
Roy City Council Agenda Information – (801) 774-1020





Date: 6 April 2021
To: City Council members
From: Steve Parkinson – Planning & Zoning Administrator 
Subject: Discussion on Mixed Use Ordinance for FrontRunner Station & Innovation Center areas

On February 11, 2020 the Planning Commission forwarded an ordinance allowing a Mixture of uses for Downtown, FrontRunner Station and the Innovation Center.

During the February 18, 2021 City Council meeting, the Council split the ordinance into two and were going to focus on the Downtown portion first.

On March 2, 2021 Ordinance No 21-3 was approved which created a “Mixed Use” zoning ordinance for Downtown.

Today, we will discuss the rest of that code in terms of the FrontRunner Station area and the Innovation Center area.

The foundation of the Mixed Use code that was approved was the same for all three areas. Thus to revisit the other areas (FrontRunner & Innovation) we only need to look at and amend a few tables.

Those tables are found below, I have also attached a map of the areas to show which properties were originally included:



Amend

10-13-2 Uses

Table 2.1 (I)

Uses	Districts					
	DT-E	DT-W	DT-G	Station Core	Station General	Innovation
Residential & Lodging						
Residential	P	P	P	P	P	U
Hotel & Inn	P	P	P	P	D	P
Residential Care	P	P	P	P	D	U
Civic						
Assembly	P	P	P	P	D	P
Transit station	P	P	P	P	P	P
Hospital & Clinic	P	P	P	P		P
Library/Museum/Post Office (no distribution)	P	P	P	P		P
Police & Fire	C	C	C	C	C	C
School	P	P	P	P	P	P
Retail						
Neighborhood Retail	P	P	P	P	P	P
General Retail	P	P	P			
Outdoor Sales Lot			C			
Service						
Neighborhood Service	P	P	P	P	P	P
General Service	P	P	P			P
Vehicle Service			C	C		
Office & Industrial						
Office	P	P	P	P		P
Craftsman Industrial			P	D		
Infrastructure						
Parking Lot	D	D	D	D	D	D
Parking Structure	D	D	D	D	D	D
Utility & Infrastructure	C	C	C	C	C	C
Open Space	D	D	D	D	D	D
Accessory Uses						
Home Occupation	P	P	P	P	P	P
Outdoor storage of Goods			D			
Parking Lot	P	P	P	P	P	P
Parking Structure	D	D	D	D	D	D

Key

P Permitted

D Permitted with Development Standards

Table 2.1 (I). Uses by District.

U Permitted in Upper Stories Only

C Requires Conditional Use Approval

10-13-3 Building Types

Building Types by District							
Districts							
		DT-E	DT-W	DT-G	Station Core	Station General	Innovation
Building Types	Storefront	P	P	P			P
	General Stoop	P	P	P	P	P	P
	Limited Bay			P			
	Large Format	P	P	P			
	Civic Building	P	P	P	P	P	P
	Row Building				P	P	

P Permitted

Table 3.1 (I). Permitted Building Types by District.

Amend

10-13-3 3) Store Front Buildings

STORE FRONT		Permitted Districts				
		DT-E	DT-W	DT-G	Station Core	Innovation
(1) Building Siting Refer to Figure 3.3 (1)						
	Multiple Principal Buildings	permitted	permitted	permitted	permitted	permitted
a	Front Property Line Coverage	80% ¹	80% ¹	80% ¹	80% ¹	80% ¹
	Occupation of Corner	required	required	required	required	required
b	Front Build-to Zone	0' to 15' ²	0' to 15' ²	0' to 15' ²	0' to 10'	0' to 10'
c	Corner Build-to Zone	0' to 15' ²	0' to 15' ²	0' to 15' ²	0' to 10'	0' to 10'
d	Minimum Side Yard Setback	0' ³	0' ³	0' ³	5'	5' ³
e	Minimum Rear Yard Setback	0' ³	0' ³	0' ³	5'	5' ³
f	Minimum Lot Width	none	none	none	none	none
	Maximum Lot Width	none	none	none	none	none
g	Parking & Loading Location	rear & side yard ¹			rear & side yard ¹	
(2) Height Refer to Figure 3.3 (2)						
i	Minimum Overall Height	1 story	1 story	1 story	3 Stories	1 Story
k	Maximum Overall Height	60' ⁴	60'	40'	5 Stories ⁵	6 stories ⁵
(3) Uses Refer to Figure 3.3 (2). Refer to 10-13-1 Uses for permitted uses.						
n	Ground Story	retail, service, office			retail, service, office	
	Upper Story	any permitted use			any permitted use	
p	Parking within Building	permitted fully in any basement and in rear of upper floors			permitted fully in any basement and in rear of upper floors	
q	Required Occupied Space	30' deep on all full floors from the front facade			30' deep on all full floors from the front facade	
(4) Street Façade Requirements Refer to Figure 3.3 (3)						
r	Minimum Ground Story Transparency Measured between 2' & 8' above grade	60% front only	60% front only	60% front only	60% front only	60% front only
	Minimum Transparency per each Story	15%	15%	15%	15%	15%
	Blank Wall Limitations	required per floor (refer to 10-13-3 2 d ii)				
t	Front Façade Entrance Type	storefront, arcade			storefront, arcade	
	Principal Entrance Location	front or corner facade			front or corner facade	
u	Required Number of Street Entrances	1 per each 100' of front facade			1 per each 100' of front facade	
	Vertical Façade Divisions	every 40' of façade width			every 40' of façade width	
	Horizontal Façade Divisions	required within 3' of the top of the ground story, and every third story above the ground floor			required within 3' of the top of the ground story, and every third story above the ground floor	
(5) Roof Type Requirements Refer to Figure 3.3 (3)						
	Permitted Roof Types	parapet, pitched, flat			parapet, pitched, flat	
	Tower	permitted			permitted	

Notes

1 – Lots wider than 140' are permitted one double-loaded aisle of parking (maximum width of 72'), located perpendicular to the front property line, which is exempt from front property line coverage.

2 – Building along Riverdale Road, 1900 West and 5600 South are exempt from Front Build-to Zone requirements, and shall follow setback requirements:

- A 15 foot setback is required on all new development along these streets
- All setbacks areas must contain either landscape, trees, patio space, or sidewalk space
- Trees, landscaping and other improvements should be used to mitigate the negative impacts from the heavy fast moving traffic

3 – As required for Site Plan approval, also reference 10-13-3 2 b iv

4 – Building heights on Riverdale Road, 1900 West and 5600 South shall not exceed 60 feet high in the area beginning at the back of the curb and extending 100 feet therefrom. In areas beyond 100 feet building height may be increased up to 80 feet.

5 – Above the third story, the upper stories of any building façade with street frontage shall have a step back from the lower stories that is a minimum of six feet

Amend

10-13-3 4) General Stoop Buildings

GENERAL STOOP		Permitted Districts					
		DT-E	DT-W	DT-G	Station Core	Station General	Innovation
(1) Building Siting Refer to Figure 3.4 (1)							
	Multiple Principal Buildings	permitted	permitted	permitted	permitted	permitted	permitted
a	Front Property Line Coverage	80% ^{1 & 2}	80% ^{1 & 2}	70% ^{1 & 2}	80% ^{1 & 2}	70% ^{1 & 2}	80% ^{1 & 2}
	Occupation of Corner	required	required	required	required	required	required
b	Front Build-to Zone	0' to 15' ³	0' to 15' ³	0' to 15' ³	0' to 15'	0' to 15'	0' to 10'
c	Corner Build-to Zone	0' to 15' ³	0' to 15' ³	0' to 15' ³	0' to 10'	0' to 10'	0' to 5'
d	Minimum Side Yard Setback	0' ⁴	0' ⁴	0' ⁴	5'	5' ⁴	5'
e	Minimum Rear Yard Setback	0' ⁴	0' ⁴	0' ⁴	5'	5' ⁴	5'
f	Minimum Lot Width	none	none	none	none	none	none
	Maximum Lot Width	none	none	none	none	none	none
g	Parking & Loading Location	rear & side yard ²			rear & side yard ²		
(2) Height Refer to Figure 3.4 (2)							
i	Minimum Overall Height	1 story	1 story	1 story	1 story	1 story	1 story
k	Maximum Overall Height	60' ⁵	60'	40'	5 stories ⁶	3 stories ⁶	6 stories ⁶
(3) Uses Refer to Figure 3.4 (2). Refer to 10-13-1 Uses for permitted uses.							
n	Ground Story	Retail, service, office			any permitted use		
	All Upper Stories	any permitted use			any permitted use		
p	Parking within Building	permitted fully in any basement and in rear of upper floors			permitted fully in any basement and in rear of upper floors		
q	Required Occupied Space	30' deep on all full floors from the front facade			30' deep on all full floors from the front facade		
(4) Street Façade Requirements Refer to Figure 9.4 (3)							
r	Minimum Transparency per each Story	15%	15%	15%	15%	15%	15%
	Blank Wall Limitations	required per floor (refer to 10-13-3 2 d ii)					
	Front Façade Entrance Type	stoop, porch, storefront			stoop, porch, storefront		
t	Principal Entrance Location	front or corner facade			front or corner facade		
	Required Number of Street Entrances	1 per each 100' of front facade			1 per each 100' of front facade		
	Vertical Façade Divisions	every 40' of façade width		every 50' of façade width	every 40' of façade width		
	Horizontal Façade Divisions	required within 3' of the top of the visible basement and of the ground story, and every third story above the ground floor			required within 3' of the top of the visible basement and of the ground story, and every third story above the ground floor		
(5) Roof Type Requirements Refer to Figure 2.4 (3)							
v	Permitted Roof Types	parapet, pitched, flat			parapet, flat		
w	Tower	permitted			permitted		

Notes

- 1 – A courtyard covering up to 35% of the front façade is permitted and may contribute to the Front Lot Line Coverage requirements.
- 2 – Lots wider than 140' are permitted one double-loaded aisle of parking (maximum width of 72'), located perpendicular to the front property line, which is exempt from front property line coverage.
- 3 – Building along Riverdale Road, 1900 West and 5600 South are exempt from Front Build-to Zone requirements, and shall follow setback requirements:
 - a. A 15 foot setback is required on all new development along these streets
 - b. All setbacks areas must contain either landscape, trees, patio space, or sidewalk space
 - c. Trees, landscaping and other improvements should be used to mitigate the negative impacts from the heavy fast moving traffic
- 4 – As required for Site Plan approval, also reference 10-13-3 2 b iv
- 5 – Building heights on Riverdale Road, 1900 West and 5600 South shall not exceed 60 feet high in the area beginning at the back of the curb and extending 100 feet therefrom. In areas beyond 100 feet building height may be increased up to 80 feet.
- 6 – Above the third story, the upper stories of any building façade with street frontage shall have a step back from the lower stories that is a minimum of six feet

Amend

10-13-3 7) Civic Buildings

CIVIC	Permitted Districts					
	DT-E	DT-W	DT-G	Station Core	Station General	Innovation
(1) Building Siting Refer to Figure 3.7 (1)						
Multiple Principal Buildings	permitted	permitted	permitted	permitted	permitted	permitted
Front Property Line Coverage	not required	not required	not required	not required	not required	not required
Occupation of Corner	not required	not required	not required	not required	not required	not required
b Front Setback	15' ¹	15' ¹	15' ¹	10'	10'	10'
c Corner Setback	15' ¹	15' ¹	15' ¹	5'	5'	5'
d Minimum Side Yard Setback	5' ²	5' ²	5' ²	5'	5'	5' ²
e Minimum Rear Yard Setback	5' ²	5' ²	5' ²	5'	5'	5' ²
f Minimum Lot Width	50'	50'	50'	50'	50'	50'
f Maximum Lot Width	none	none	none	none	none	none
g Parking & Loading Location	rear	rear	rear & interior side yard ³	rear & interior side yard ³		
(2) Height Refer to Figure 3.4 (2)						
i Minimum Overall Height	1 story	1 story	1 story	1 story	1 story	1 story
k Maximum Overall Height	60' ⁴	60'	40'	3 stories	3 stories	5 stories
(3) Uses Refer to Figure 3.7 (2). Refer to 10-13-1 Uses for permitted uses.						
n All Stories	limited to civic & institutional uses only			limited to civic & institutional uses only		
p Parking within Building	permitted fully in basement and in rear of upper floors.			permitted fully in basement and in rear of upper floors.		
q Required Occupied Space	30' deep on all full floors from the front facade			30' deep on all full floors from the front facade		
(4) Street Façade Requirements Refer to Figure 3.7 (3)						
r Minimum Transparency per each Story	10%	10%	10%	10%	10%	10%
Blank Wall Limitations	not required			not required		
t Front Façade Permitted Entrance Type	arcade, porch, stoop			arcade, stoop		
u Principal Entrance Location per Unit	front or corner facade			front or corner façade		
Required Number of Primary Street Entrances	1 per 100' of facade		1 per 150' of facade	1 per 150' of façade		
Vertical Façade Divisions	not required			not required		
Horizontal Façade Divisions	not required			not required		
(5) Roof Type Requirements Refer to Figure 3.7 (3)						
v Permitted Roof Types	parapet, pitched, flat; other roof types are permitted by Conditional Use			parapet, pitched, flat; other roof types are permitted by Conditional Use		
w Tower	permitted			permitted		

Notes

1 – Building along Riverdale Road, 1900 West and 5600 South are exempt from Front Build-to Zone requirements, and shall follow setback requirements:

- A 15 foot setback is required on all new development along these streets
- All setbacks areas must contain either landscape, trees, patio space, or sidewalk space
- Trees, landscaping and other improvements should be used to mitigate the negative impacts from the heavy fast moving traffic

2 – As required for Site Plan approval, also reference 10-13-3 2 b iv

3 – Lots wider than 140' are permitted one double-loaded aisle of parking (maximum width of 72'), located perpendicular to the property line, which is exempt from front property line coverage.

4 – Building heights on Riverdale Road, 1900 West and 5600 South shall not exceed 60 feet high in the area beginning at the back of the curb and extending 100 feet therefrom. In areas beyond 100 feet building height may be increased up to 80 feet.

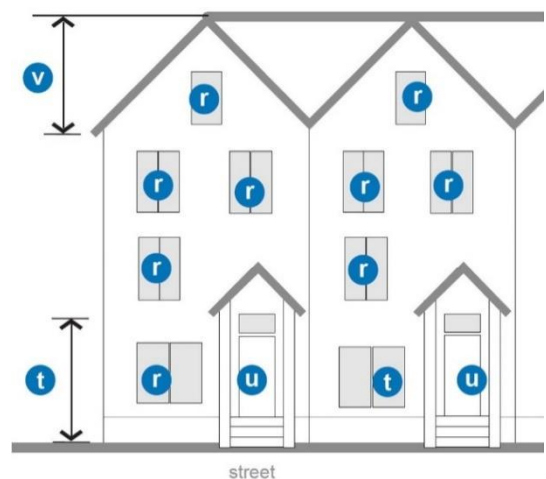
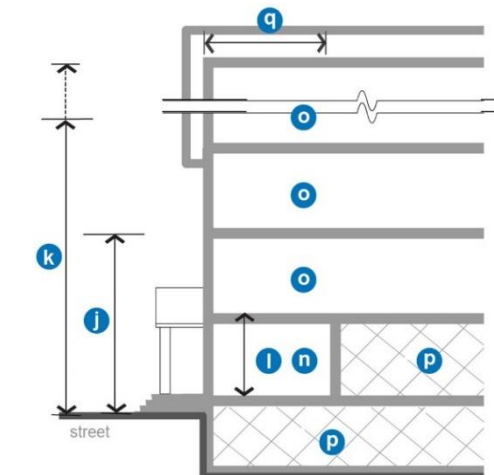
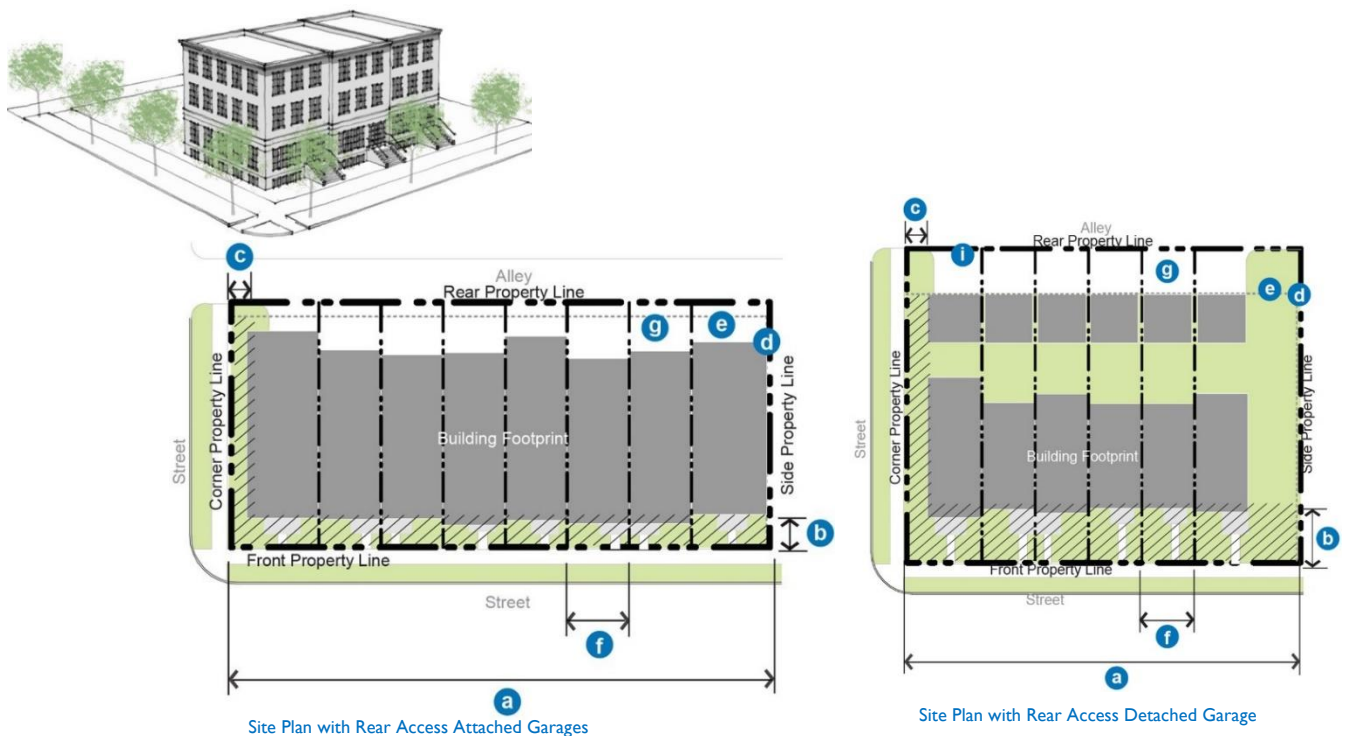
Add

10-13-3 8) Row Building

- a) **Description & Intent.** The Row Building is a building typically comprised of multiple vertical units, each with its own entrance to the street. This Building Type may be organized as townhouses or rowhouses, or it could also incorporate live/work units where uses are permitted.

Parking is required to be located in the rear yard and may be incorporated either into a detached garage or in an attached garaged accessed from the rear of the building. However, when the garage is located within the building, a minimum level of occupied space is required on the front facade to ensure that the street facade is active.

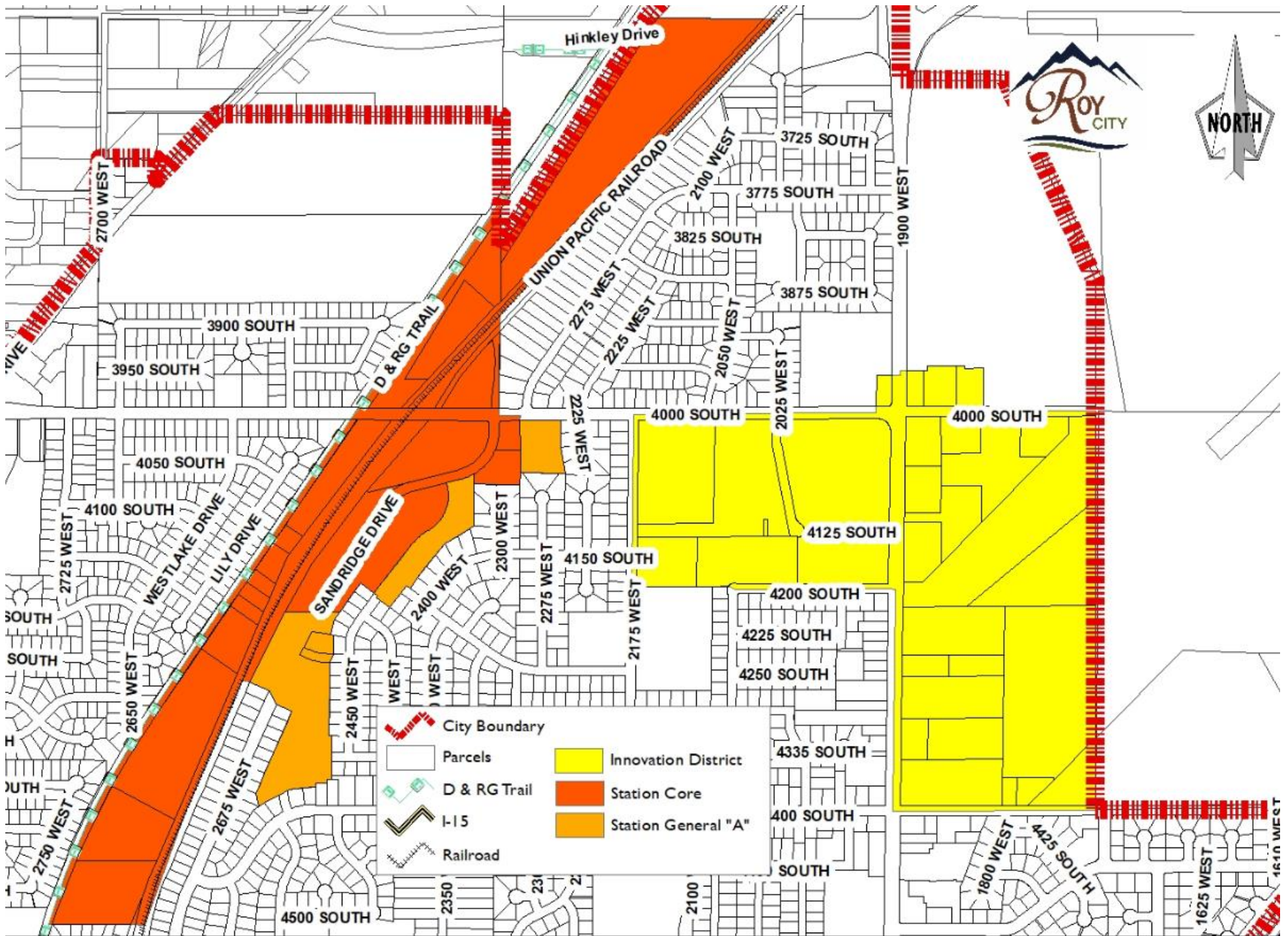
- b) **Regulations.** Regulations for the Row Building type are defined in the adjacent table



ROW BUILDING	Permitted Districts	
	Station Core	Station General
(1) Building Siting Refer to Figure 3.8 (1)		
Multiple Principal Buildings	Permitted ¹	Permitted ¹
a Front Property Line Coverage	65% ²	65% ²
Occupation of Corner	Required	Required
b Front Build-to Zone	0' to 15'	0' to 15'
c Corner Build-to Zone	0' to 15'	0' to 15'
d Minimum Side Yard Setback	0' per unit; 15' between buildings	0' per unit; 15' between buildings
e Minimum Rear Yard Setback	10' ³	10' ³
f Minimum Unit Width	22' per unit	22' per unit
Maximum Building Width	Maximum of 8 units per building	Maximum of 8 units per building
g Parking & Loading Location	Rear yard/facade	Rear yard/facade
(2) Height Refer to Figure 3.8 (2)		
i Minimum Overall Height	2 story	2 story
k Maximum Overall Height	3.5 stories	3.5 stories
(3) Uses Refer to Figure 5.4 (2). Refer to 10-13-4 Uses for permitted uses.		
n Ground Stories	residential, service, office, limited craftsman industrial	residential
o Upper Story	residential only	residential only
p Parking within Building	Permitted fully in basement and in rear of all floors	Permitted fully in basement and in rear of all floors
q Required Occupied Space	30' deep on all floors from the front facade	30' deep on all floors from the front facade
(4) Street Façade Requirements Refer to Figure 3.8 (3)		
Minimum Transparency per each Story	15% ⁴	15%
t Blank Wall Limitations	required per floor (refer to 10-13-3 2 d ii)	
u Front Façade Permitted Entrance Type	stoop, porch, limited storefront ⁵	stoop, porch, limited storefront ⁵
Principal Entrance Location per Unit	front or corner side façade	front or corner side façade
Vertical Façade Divisions	not required	not required
Horizontal Façade Divisions	For buildings over 3 stories, required within 3' of the top of any visible basement or ground story	For buildings over 3 stories, required within 3' of the top of any visible basement or ground story
(5) Roof Type Requirements Refer to Figure 3.8 (3)		
Permitted Roof Types	parapet, pitched, flat	parapet, pitched, flat
Tower	not permitted	not permitted

Notes

- 1 – For the purpose of the Row Building, a building consists of a series of units. When permitted, multiple buildings may be located on a lot with the minimum required space between them. However, each building shall meet all requirements of the Building Type unless otherwise noted.
- 2 – Each building shall meet the front property line coverage requirement, except one of every five units may front a courtyard with a minimum width of 30'. The courtyard shall be defined on three sides by units.
- 3 – As required for Site Plan approval
- 4 – When the storefront entrance type is utilized, the maximum ground story transparency for the unit is 55% as measured between 2' and 8' above grade.
- 5 – The storefront entrance type is permitted only on corners or buildings that are designated for live/work units.



Discussion on the Zoning Ordinance Involving the Front Runner Station and Industrial Park

This paper provides background information for the 6 April 2021 City Council Work Session that will address possible ordinance changes dealing with areas around the Front Runner Station and Industrial Park.

Front Runner Station

- 2019 Senate Bill 34 – Affordable Housing Modification
 - Moderate Income Housing (MIH) element:
 - Municipalities/counties covered: Utah Code has long required municipalities and counties to plan for moderate income housing growth. SB34 requires, by December 1, 2019, the following municipalities and counties to update and adopt the moderate-income housing element of their general plan, and annually report on implementation:
 - all municipalities of the 1st, 2nd, 3rd, and 4th class;
 - cities of the 5th class with a population of 5,000 or more that are located in counties of the 1st, 2nd, and 3rd class;
 - metro townships with a population of 5,000 or more; and
 - all counties must plan and adopt a MIH element including strategies from the ‘menu’ (see below) but only counties of the 1st, 2nd, and 3rd class with an unincorporated population of 5,000 or more must annually report on implementation.
 - In addition to the recommendations required above, municipalities that have a “fixed guideway public transit station” shall include a recommendation to implement either “G” or “H” [not required for counties].
 - (G) encourage higher density or moderate-income residential development near major transit investment corridors
 - (H) eliminate or reduce parking requirements for residential development where a resident is less likely to rely on their own vehicle, e.g. residential development near major transit investment corridors or senior living facilities
- 2019 Roy City General Plan Amendment to Support SB 34 Requirements – Approved November 19, 2019 Roy City Council
 - Roy City Ordinance No. 19-16 an Ordinance Establishing Amendments.
 - Chapter 6 Residential Development
 - Goal 3: To meet existing and future moderate-income (affordable) housing needs of the City.
 - Objective 4: Allow for Higher Density and Mixed Use (F, **G**, **H**, J).
 - Policy B: Encourage higher density or moderate-income residential development near major transit investment corridors;
 - Policy C: Eliminate or reduce parking requirements for residential development where a resident is less likely to rely on the resident's own vehicle, such as residential development near major transit investment corridors or senior living facilities;
 - Policy D: Implement zoning incentives for low to moderate income units in new developments.

- 2020 Senate Bill 150 - Transportation Governance and Funding Amendments
 - Transit Oriented Development (TOD) and UTA
 - Eliminates the cap on the number of TODs UTA can participate in (was 8).
 - Establishes requirements for UTA board of trustees when approving UTA participation in TODs, in addition to the requirements already in law. New requirements:
 - The municipality or unincorporated county where the TOD is located must be in compliance with the requirements to develop and report on their moderate-income housing plan in their general plan (pursuant to SB34 (2019)).
 - The municipality or county where the TOD is located must have developed and adopted a “station area plan” that establishes and preserves a vision for areas within one-half mile of a UTA fixed guideway station, the development of which includes:
 - a) involvement of all relevant stakeholders who have an interest in the station area, including relevant metropolitan planning organizations;
 - b) identification of major infrastructural and policy constraints and a course of action to address those constraints; and
 - c) other criteria as determined by the board of trustees of the relevant public transit district.
 - UTA may request, in writing, that a municipality or county provide UTA with an electronic notice of each land use application received by the municipality/county that may impact the development of a major transit investment corridor. Municipalities/counties that receive a written request shall provide UTA with timely electronic notice of each land use application requested. (Note: this is consistent with the notice requirements municipalities now provide to UDOT.)
- 2021 Senate Bill 217 - Housing and Transit Reinvestment Zone Act
 - The State is poised to make a historic investment in strategic double-tracking of Front Runner to increase capacity, speed, and frequency. The HTRZ is designed to facilitate mixed-use, multi-family and affordable housing development within a 1/3-mile radius of Front Runner stations.
 - Under the bill, a city could propose a development, and zoning, around a Front Runner station that is designed to:
 - promote higher utilization of public transit;
 - increase availability of housing, including affordable housing;
 - conservation of water resources through efficient land use;
 - improving air quality by reducing fuel consumption and motor vehicle trips;
 - encourage transformative mixed-use development and investment in transportation and public transit infrastructure in strategic areas;
 - strategic land use and municipal planning in major transit investment corridors; and
 - increasing access to employment and educational opportunities.
 - The development proposal would be required to

- (i) be mixed-use,
 - (ii) be at least 50 units to the acre, and
 - (iii) have at least 10% affordable housing.
- The city proposal would also identify the costs and revenues needed to make the project "pencil" for public infrastructure costs associated with the increased development. The proposal would be subject to an independent financial assessment.
- The proposal would be submitted to a committee for the HTRZ, made up of the relevant government entities, taxing entities, and transportation agencies. If that committee approved the proposal, then the needed portion of incremental local property tax revenues would be captured to support the development costs.
- The HTRZ proposal is designed to get "all hands-on deck" to help Utah tackle its housing crisis by facilitating development in the key areas around Front Runner stations, capitalizing on the State investment to improve Front Runner.
- UTA's Transit-Oriented Development (TOD) Design Guidelines December 2014
 - UTA Goals for TOD.
 - Unlike most landowners, UTA has several expectations and goals in developing property beyond making a profit. First and foremost, UTA is a public transit provider and while generating the best return possible is clearly an objective, it is only one of the goals that UTA has in relation to its property development activities. This includes
 - Increase ridership.
 - Optimize developable land and support the regional growth vision.
 - Generate revenue.
 - **UTA Connectivity**
 - Multi-modal connectivity is paramount to successful transit-oriented development.
 - The auto-centric development patterns of the past sixty years have led to a number of negative externalities both from a regional and local perspective that have been well documented in TOD literature. Designs that cater only to the automobile often lead to a lack of on-street activity, developments with no sense of place, and areas where almost every trip requires the use of an automobile; neglecting populations like the young, elderly, and disabled who are unable drive. Health issues like rising obesity and diabetes rates as well as air quality issues have also be linked to auto-centric design.
 - Proposed developments must consider connectivity for the following transportation modes:
 - Pedestrians
 - Bicyclists
 - Transit
 - Automobiles
 - **UTA Development Form**
 - Creating a pleasant and inviting public space is a crucial component of TOD.

- While connectivity plays a significant role in creating an excellent pedestrian experience, the form that the development takes is just as important. Good TOD requires pedestrian scale design that creates a sense of place, feels comfortable and safe, limits dead space, and provides visual interest to an area.
- Proposed developments shall address the following aspects of development form:
 - Density (Density calculations are based on developable land and does not include transit critical elements of the site, roads, and open space.)
 - Metropolitan Center
 - Minimum Residential Units per Acre – 50 du/ac
 - Employees per Acre – 100 / ac
 - Urban Center –
 - Minimum Residential Units per Acre – 40 du/ac
 - Employees per Acre – 80 / ac
 - Town Center –
 - Minimum Residential Units per Acre – 30 du/ac
 - Employees per Acre – 60 / ac
 - Station Community – (*Roy Front Runner Station*)
 - Minimum Residential Units per Acre – 25 du/ac
 - Employees per Acre – 40 / ac
 - Parking Ratios
 - Station Community
 - Residential Multi-Family (1 Bedroom) – 1.5 Stalls / Dwelling Unit
 - Residential Multi-Family (2 & 3 Bedrooms) – 2 Stalls / Dwelling Unit
 - Office – 4 Stalls / 1000 square feet
 - Retail – 4 Stalls / 1000 square feet
 - Building orientation and setbacks
 - All street corners shall be occupied by a building unless the corner is set aside for open space.
 - Building setbacks shall be between 0' and 5' feet from the sidewalk on primary streets and 0' to 10' on secondary streets. A larger setback may be allowed if fronting a major corridor with more than 4 lanes of traffic.
 - Side yard setbacks between buildings shall be no more than 5' unless there is a major pedestrian/bicycle connection requiring additional space or vehicular access to parking facilities is needed . Side yard parking may be allowed on secondary/local streets provided that they appropriately screened.
 - Vehicular access (driveways) shall be limited to one between streets, especially on primary streets.
 - Driveways shall be no more than 24' wide and ideally should be sized to accommodate one-way traffic or a yield lane.
 - Principal entrances shall be located on the front or corner façade .

- One on-street entrance shall be provided for every 100' of façade.
- Residential units on a ground floor should have direct access to public rights of way.
- Loading and unloading areas, open storage, refuse areas, and utility appurtenances shall be screened from view from all streets.
- Building heights should be appropriately sized to maintain a human scale.
- Designs should acknowledge the context of the project area and the street right-of-way.
- Building widths should not be out of proportion with building heights. Additionally, consider using a 6' to 12' setback for floors higher than four stories to maintain a pedestrian scale on the street.
- Articulation, fenestration, and transparency
 - A vertical façade division shall be incorporated into the building design every 30' of façade width (as measured along the base of the façade). Elements may include a column, pilaster, or other continuous vertical ornamentation a minimum of 2" in depth.
 - A horizontal façade division shall be incorporated into the building design within 3' of the top of any visible basement or ground story and every fourth floor above the first floor. Elements may include a cornice, belt course, molding, string courses, or other continuous horizontal ornamentation a minimum of 2" in depth.
 - A minimum ground story transparency of 65% (measured between 2' and 8' above grade) for retail and office uses and a minimum ground story transparency of 15% (measured between 2' and 8' above grade) for residential uses.
 - A minimum transparency for all other floors of 15% (measured from floor to floor).
 - No rectangular area greater than 20% of a story's façade may be windowless (measured from floor to floor).
 - No horizontal segment of a story's façade greater than 15' in width may be windowless.
- Location and screening of parking
 - Accommodating parking demand through on-street parking is highly encouraged, especially for retail land uses.
 - Surface parking shall be placed in the rear of buildings.
 - Parking area entrances shall be adequately signed for both pedestrians and automobiles.
 - Limited side yard surface lots may be allowed on non-primary streets if the lot is screened with walls, hedges, or berms. These landscape buffers shall have a minimum depth of 7'. A medium or large shade tree shall be required at least every 40'.
 - Internal pedestrian pathways within the parking area and outside of the parking drive isles shall be provided in surface parking lots with two or more double-loaded isles.
 - Parking structures are highly encouraged and shall be used whenever feasible to accommodate transit parking needs.
 - Above-ground parking structures should be wrapped with retail, residential or office uses to limit the amount of the structure visible from primary and secondary streets.

- Where parking structures are visible, they shall be designed to conceal the view of all parked vehicles and ramps.
- Streetscapes
 - All plans shall clearly indicate the street type.
 - Primary Arterials Street
 - Minimum 5' wide sidewalk
 - Shade trees spaced at mature diameter for the species. While particular species of trees are not suggested in this document, street trees shall provide a minimum clear trunk height of 8'.
 - Pedestrian scale lighting that is a maximum of 16' in height and spaced approximately 60' apart. Light shall direct downward to the sidewalk.
 - A planting or street furnishing zone (4' maximum).
 - For intersections with more than 2 travel lanes a 12' median shall be required.
 - On street parking is not required but is highly encouraged.
 - Primary Local Street
 - Minimum 10' wide sidewalk.
 - Shade trees spaced at mature diameter for the species. While particular species of trees are not suggested in this document, street trees shall provide a minimum clear trunk height of 8'.
 - Pedestrian scale lighting that is a maximum of 16' in height and spaced approximately 60' apart. Light shall direct downward to the sidewalk.
 - A planting or street furnishing zone (4' maximum).
 - Minimum 8' wide on-street parking.
 - An appropriate bikeway facility (this may be located off of the street if using a multi-use pathway).
 - No more than 4 travel lanes with a width no wider than 12' each.
 - For intersections with more than 2 travel lanes a 12' median shall be required.
 - Secondary/Local Streets
 - Minimum 5' wide sidewalk (10' if on a major pedestrian corridor).
 - Shade trees spaced at mature diameter for the species. While particular species of trees are not suggested in this document, street trees shall provide a minimum clear trunk height of 8'.
 - Pedestrian scale lighting that is a maximum of 16' in height and spaced approximately 60' apart. Light shall direct downward to the sidewalk.
 - No more than 2 travel lanes with a width no wider than 11' each.
 - On street parking is not required but is highly encouraged
- UTA owns 17.7 acres at the Roy Front Runner Station
- Roy City's current ordinance allows around 12 Residential Units per Acre

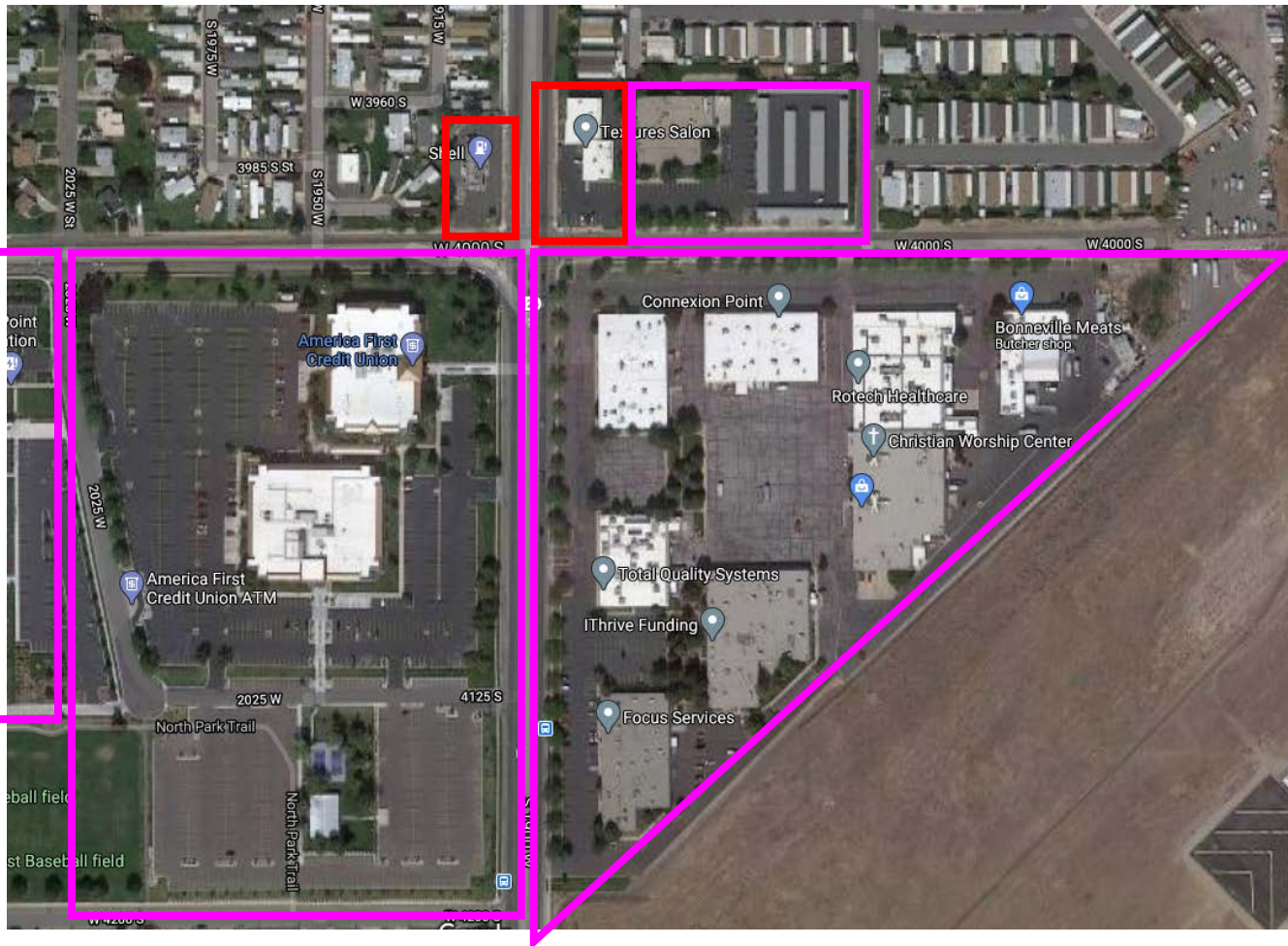


Industrial Park

**Zoned Community
Commercial (CC)**

**Zoned
Business Park (BP)**

**Includes
Elementary
School and
Library**



FAA Advisory Circular

- Subject: A Model Zoning Ordinance to Limit Height of Objects Around Airports - AC 150/5190-4A - Section IV Airport Zone Height Limitations
 - No structure shall be erected, altered, or maintained, and no tree shall be allowed to grow in any zone created.
 - 8 - Transitional Zones - Slope seven (7) feet for each foot upward beginning at the sides of and at the same elevation as the primary surface and the approach surface and extending to a height of 150 feet above the airport elevation.



Distance and Building Heights Based on FAA AC 150/5190-4A Section IV Airport Zone Height Limitations

- Information shows the estimated distance the building sits from the runway and the estimated maximum building height allowed.

